

3.3

FORM, SCALE + APPEARANCE

The 3D visualisation on the right provides a direct comparison between the existing and proposed condition at the rear of the site.

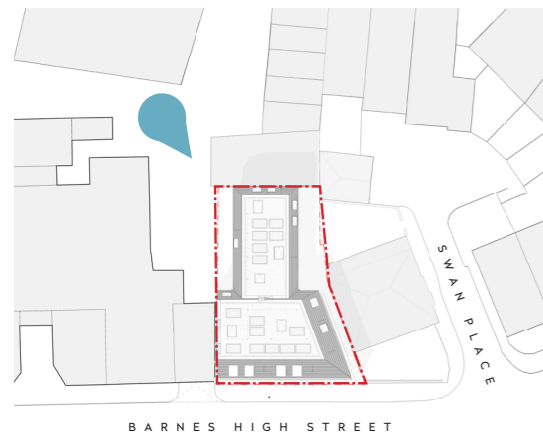
This view has been chosen as it represents the 'worse case' condition to allow an assessment of the development.

At pre-application stage the local planning authority highlighted that they were concerned over the volume and mass at this location. For this reason the proposal has been revised to show a lower lying mass. The units on the second floor have been reduced in size allowing the overall volume to reduce.

The proposed materiality of render and slates ensures that the development better relates to the immediate adjacent neighbours in Rose House (pink render and slate roof) and Seaforth Lodge (yellow/pale render with dark accent detailing).



KEY PLAN



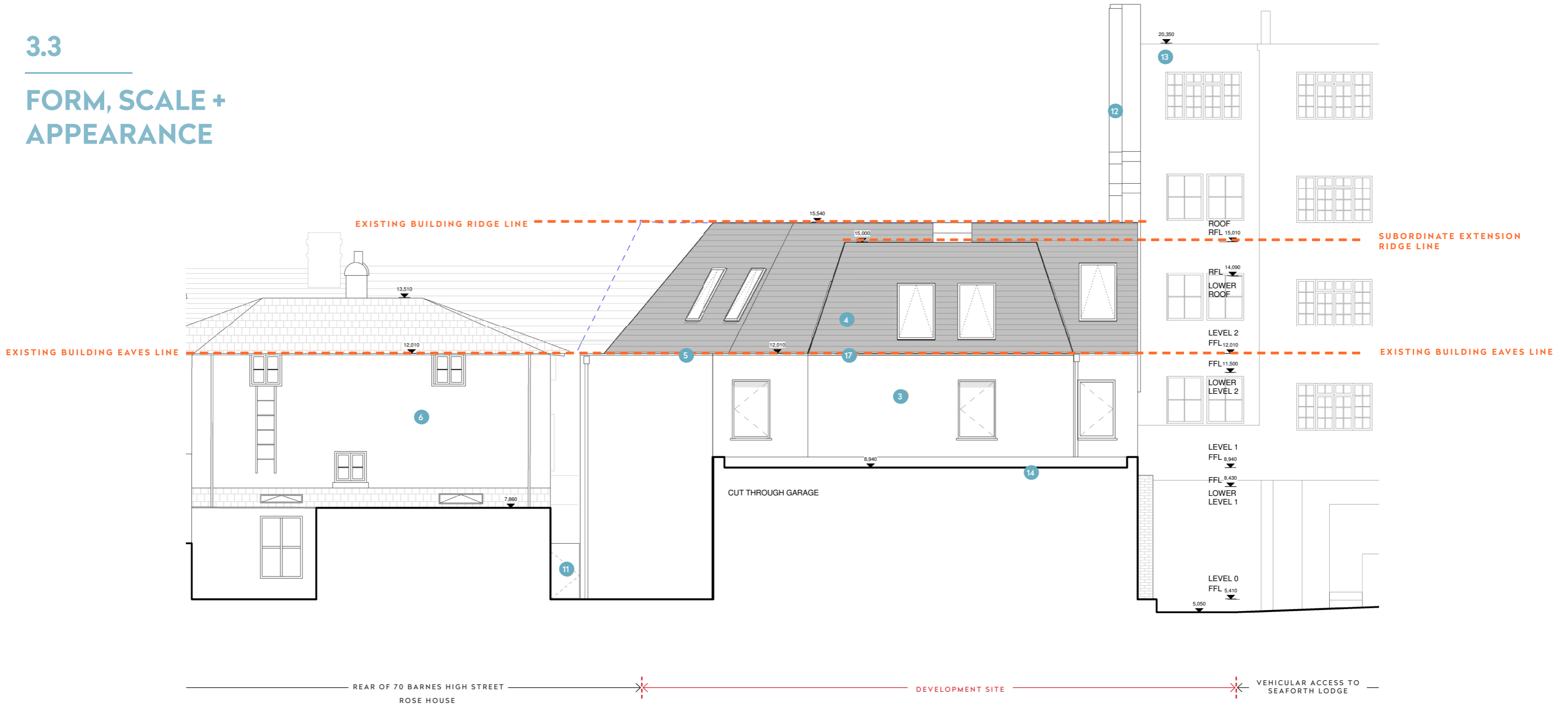
CGI visualisation of the existing building looking south east from the rear of Seaforth Lodge



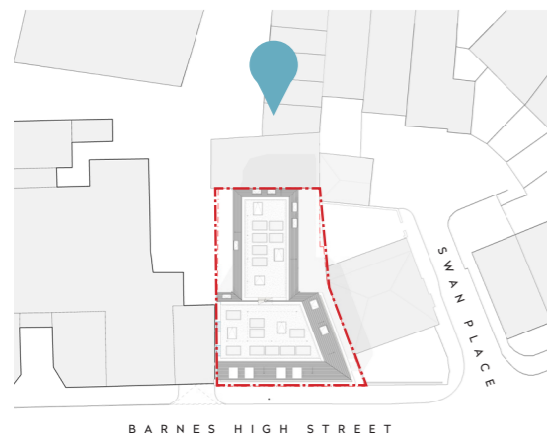
CGI visualisation of proposed scheme looking south east from the rear of Seaforth Lodge

3.3

FORM, SCALE + APPEARANCE



KEY PLAN



PROPOSED NORTH ELEVATION

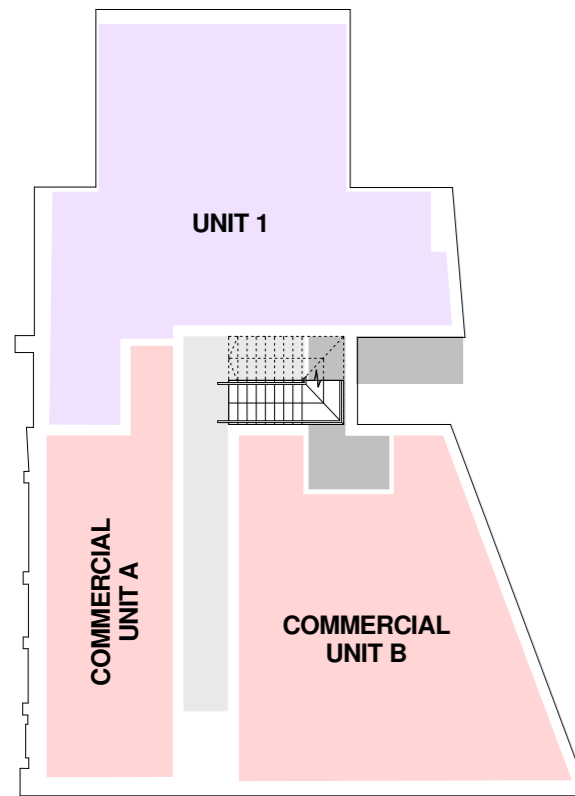
The north elevation highlights the 'transition' nature of the site between the 3 storey listed Rose House and the 5 storey Seaforth House.

The North elevation looks out over a single storey garage. Windows are proposed on the first and second floor facing out. These windows are over the 20m facing distance between habitable rooms and therefore do not require any further obscuring.

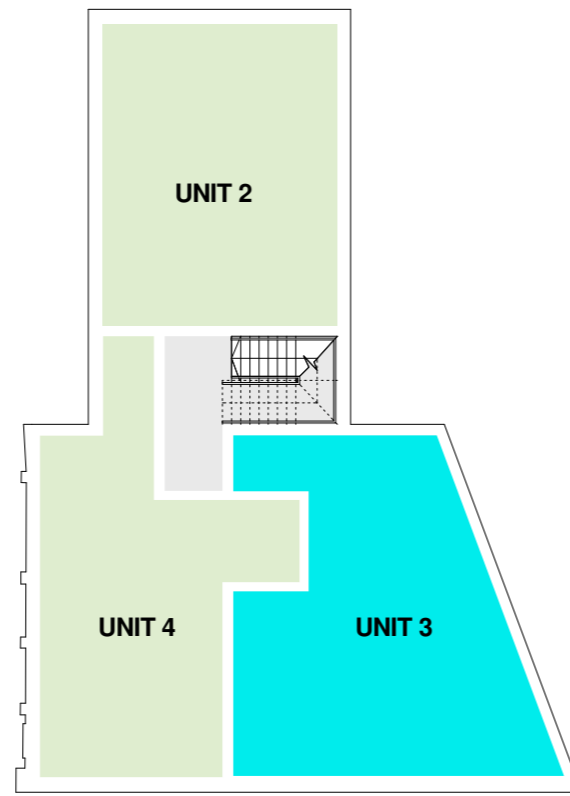
The window proportions are consistent with the proportions proposed on the South Elevation and local context.

KEY

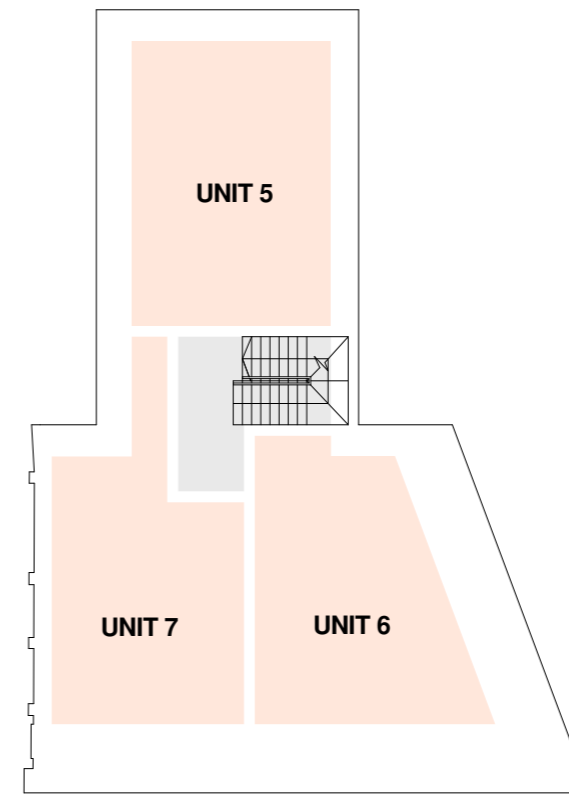
- 1 New aluminium dormer windows to second floor residential units
- 2 New aluminium window
- 3 Rendered finish (colour TBC)
- 4 Slate tiled roof to match existing
- 5 Parapet height to match neighbouring properties
- 6 Grade II listed Rose Cottage
- 7 Pink rendered finish
- 8 New residential entrance to units 1-7
- 9 Dark red brickwork of Seaforth Lodge
- 10 Existing signage to commercial units at ground floor
- 11 Access gate to shared bin and bike store
- 12 Existing traditional brick chimney stack to be retained
- 13 6 storey neighbouring property, Seaforth Lodge
- 14 Single storey garage to the rear of the site
- 15 Canopy to commercial units at ground floor
- 16 New boundary wall
- 17 Metal coping to parapet wall
- 18 Soldier course brick detail
- 19 New aluminium roof lights to residential units



LEVEL 0



LEVEL 1



LEVEL 2

3.4

SUMMARY + MIX

All units meet or exceed the nationally described internal space standards. The proposed development includes the following:

Commercial (A2)

• Commercial Unit A :	35m ²
• Commercial Unit B :	65m ²
Total Commercial GIA:	100m²

Residential (C3)

• Unit 1 (2B 4P) :	75m ²
• Unit 2 (1B 2P) :	52m ²
• Unit 3 (2B 3P) :	62m ²
• Unit 4 (1B 2P) :	51m ²
• Unit 5 (Studio) :	42m ²
• Unit 6 (Studio) :	39m ²
• Unit 7 (Studio) :	39m ²
• Shared Circulation:	46m ²
• Cleaners Store :	2m ²
• Internal Bicycle Store :	3m ²
• Non-Net (partitions etc):	17m ²
Total Residential GIA:	423m²

Policy DMHO4 states that development should generally provide smaller size accommodation within areas located near town centres. Therefore the proposed housing mix shows a larger proportion of studio and 1 bedroom units.

The existing three residential units do not have private amenity space due to site constraints. The site is located approximately 100m from Barnes Common which is one of the largest areas of common land in London with 122.4 acres (49.55 hectares) of protected open land. Its facilities include a full-size football pitch and a nature trail.

As the proposal is mainly focused on smaller units and are therefore less likely to be occupied by families as highlighted in the formal pre-application response. With this in mind, coupled with the close proximity of generous and usable common land, only the largest unit (the one most likely to be occupied by a family) has any proposed external amenity space. This area is divided into two terraces of 6m² and 9m² (15m² in total).

4.0

ACCESS

Pedestrian Access

Residential access is via a shared entrance lobby located on Barnes High Street. The site can be accessed directly from the street. Unit 1 is accessed from the ground with a shared communal entrance for the flats on the first and second floor. The main residential entrance is set back from the street by 2m to provide a covered entrance

Both commercial units are accessed via Barnes High Street, as per the existing condition.

Inclusive Access

As the proposal is a refurbishment and extension, it has been designed to comply with the Building Regulation Requirement Part M4 (1) Category 1 : Visitable Dwelling. As a lift is not viable or reasonable achieved, a suitable general access, communal stair based on Building Regulation Part K has been proposed. The layout and design of all internal spaces has been considered and specific considerations has be given to the following areas:

- Door widths
- Corridors
- Stairs and balustrades
- WC's

Although, due to existing level changes across the site, step-free access is not achievable, where possible the scheme has been designed to the optional Building Regulation Requirement Part M4 (2) Category 2 : Accessible and Adaptable Dwellings.

Transport

The site has a PTAL (Public Transport Accessibility Level) of 3, meaning that the site is considered moderate in terms of public transport. The site is 15 metres walk from the nearest bus stop and an average walking time of 4 minutes to Barnes Bridge and 12 minute walk to Barnes National Railway Station.

Additionally, the site is located on Barnes High Street, which is predominately characterised by retail units, providing many necessary local amenities.

Refuse / Waste Management

The residential units currently use refuse sacks that are left on Barnes High Street for collection on Monday mornings. Sacks must be left on the street side between the hours of Sunday 8pm - Monday 6am. Following conversations with the local planning authority, as the proposal includes less than 8 residential units, the proposed strategy matches the existing condition.

The commercial units currently leave their refuse sacks on street and are collected at an alternative time to the residential units. It is proposed that the existing situation is continued.

Parking Provision

The site does not provide any car parking provision for the new units on site. The existing 3 residential units currently receive parking permits allowing parking in the vicinity of the site. It is proposed that these be retained for 3 of the units. No further permit applications are proposed. Therefore, there is no further potential for off-site parking.

In accordance with the London Plan suggested that all developments should provide dedicated storage space for cycles at the following levels:

- 1 per 1 bedroom dwelling; or
- 2 per 2, 3 or more bedroom dwellings

This would result in a total of 9 cycle spaces, currently there are 10 provided: 9 in a communal secure store and 1 in a private store located in Unit 1's private terrace. The stores are located on the key plan to the right.

Emergency Access

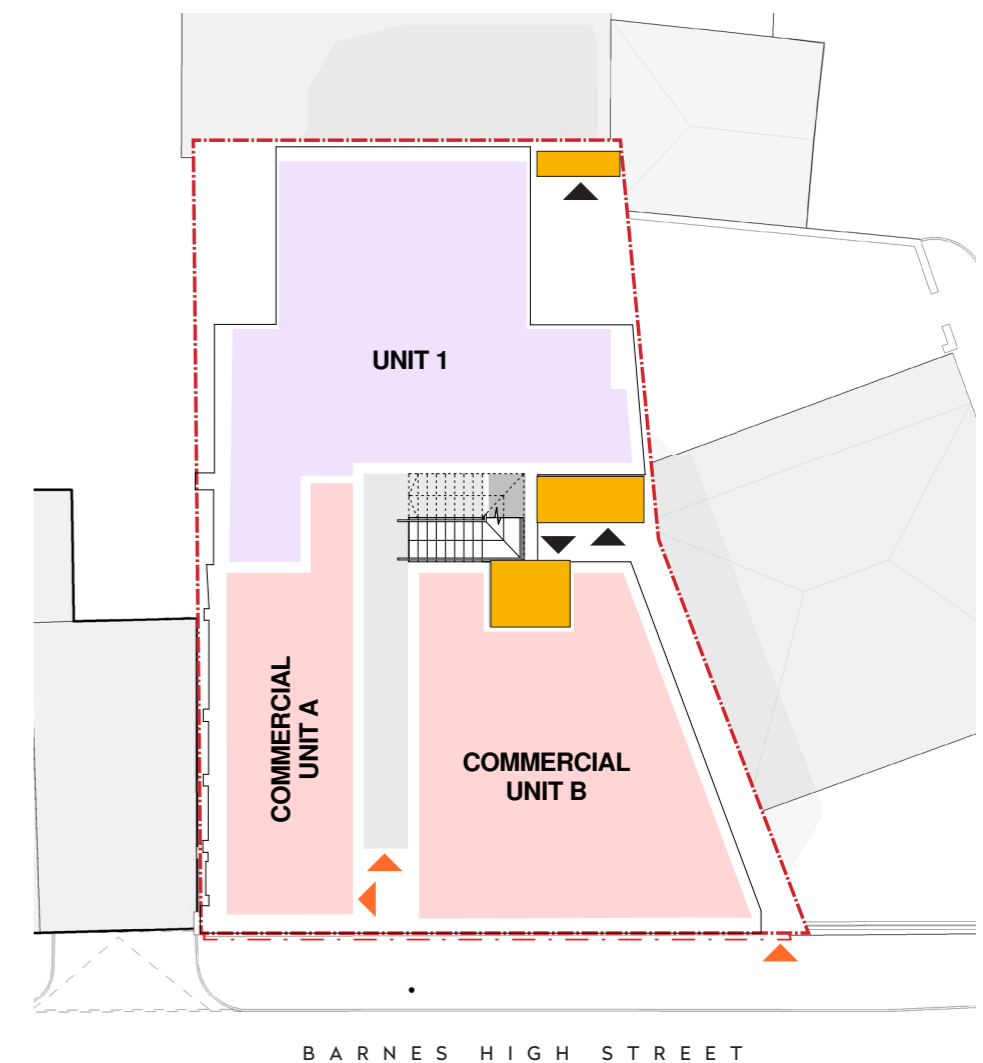
To occur via Barnes High Street

Roof Access

To occur via the roof access hatch in the communal stair core. The roof has a man-safe system to ensure safety of those accessing for maintenance purposes.



KEY PLAN : PROPOSED GROUND FLOOR



KEY

- Pedestrian access points
- Access to bicycle stores
- Bicycle store locations