

Project:

Station Yard
Twickenham
TW1 4LJ

Date:

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Report:

Design & Access Statement

Revision:

P0

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INTRODUCTION

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INTEGRATION

Services Consultant



Transport Consultant



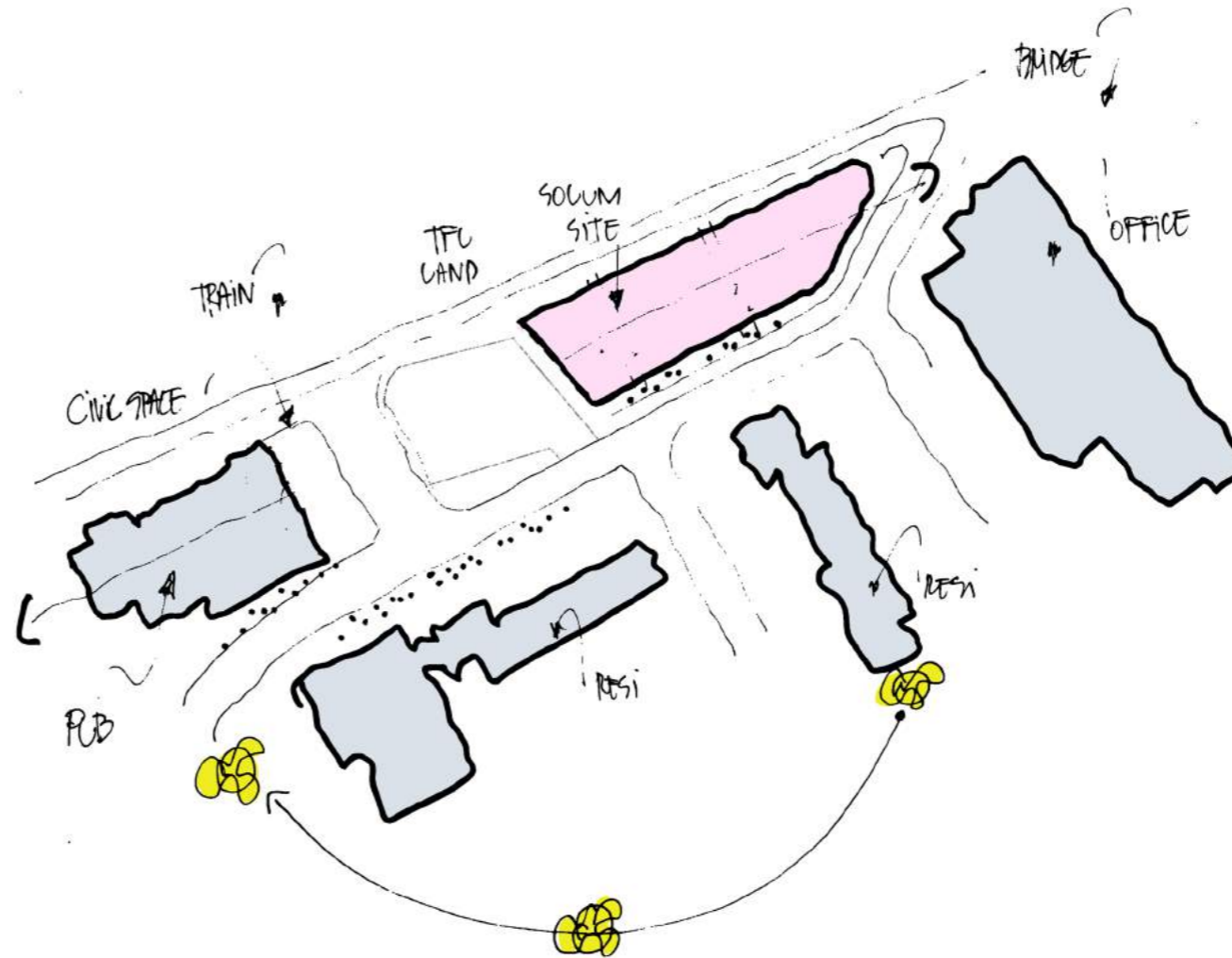
Rights of Light Consultant



Landscape Architect

Studio Archetype FoundationCGI

Visuals and Verified Views



This Design and Access Statement sets out exciting regeneration proposals for an under-used area of brown-field land, which is allocated for residential development within Twickenham town centre. The proposed scheme is on land controlled by Solum Regeneration (Twickenham) LLP at Station Yard, a few minutes walk from Twickenham Station.

The developer is proposing a residential building that is deferential and proportionate in scale to Bridge House and the wider station context that includes the Twickenham Gateway and Regal House to east, whilst also addressing the scale of the Building of Townscape Merit, the Albany Pub & housing to the south and west of the site.

TFL was contacted regarding the extent of their operational requirements for the 'TFL land' site adjacent, in an effort to determine if a combined site development may be possible. However, it has been determined that there is an ongoing operational requirement for event day and emergency parking on the site, restricting its development.

In lieu of an agreement regarding the adjacent TFL land, this report focuses on the 'Solum site' development.

The proposals have been reviewed by Richmond Council in two pre-applications, which took place in December 2018 and 29 May 2019. A pre-application advice letter from Richmond Council was received in February 2019, and a further one in June 2019. A Design Review Panel was conducted on 12 July 2019 and a Public Consultation was held on 16 July 2019.

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01. PLANNING POLICY STATEMENT

Principle of Redevelopment

1. This section sets out the key planning considerations relevant to the development of the site.
2. The site lies within the urban area and is not subject to any environmental or other designations that would restrict the principle of its redevelopment. It represents an under used, brown-field site that can contribute to regeneration and meeting key planning policy objectives. The site is within the town centre of Twickenham which is one of the two principal locations for growth in the Local Plan (2018) spatial strategy and a location where the majority of higher density housing will be targeted. The site itself is allocated for development in the Action Area Plan (2013).
3. Hence the principle of development is supported by the Richmond Borough Local Plan, the London Plan and NPPF (2018).
4. There is also an expectation under the London Plan that development on such brown-field sites within town centres and within areas of good transport accessibility, that development densities will be optimised in order to achieve sustainable development and maximise their contribution to housing supply within London.
5. Paragraph 3.1.37 of the new Local Plan makes clear that Twickenham as a centre, with its accessible locations and established range of services is, alongside Richmond, the most sustainable option for development in the Borough. It supports major developments and notes that there is some potential for 'tall' buildings and higher densities close to Twickenham train station.
6. It goes on to note that increased densities of housing, including some 'taller' buildings as well as the provision of smaller residential dwelling sizes, are appropriate in these centres, to an extent that is compatible with the established character and environmental and heritage constraints.
7. Therefore it is clear a new realisation of the centre of Twickenham should wherever possible, accommodate higher density housing development to fulfil the spatial and quantitative requirements of the Plan.
8. Paragraph 3.1.28 of the Local Plan also notes the housing target of 315 dwellings per annum to be provided for the period of 2015-2025 will be expected to be exceeded by the Mayor. The LP Spatial Strategy seeks to do so by optimising the use of sites, particularly in centres with good public transport accessibility and mixed use redevelopments.

Site Area

9. The site area reflects the full extent of Solum Regeneration (Twickenham) LLP's control and is not the subject of any known constraint in bringing forward development. It is therefore, deliverable in the short term.
10. The design of development on the site should be driven by good design principles and the requirements of good functional operation. While the site allocation includes the adjacent TFL land, this is in separate ownership and does not form part of the site. There is an ongoing operational requirement for coach/bus stopping and turning and that their specific requirements limit the availability of the land and options for its potential development.
11. Hence the approach adopted in this case is to explore a number of theoretical design options for the TFL land, as part of a wider master-plan that respects the AAP aspiration for an enhanced setting for The Albany. This ensures the short term development of the Solum Regeneration (Twickenham) LLP site does not prejudice any future TFL land development, should its operational requirement change.

Land Uses

12. The policy requirement noted within the AAP allocation for the site is for residential development. The predominant land use proposed is therefore, acceptable in principle.

Affordable Housing

13. In accordance with pre-application discussions, the scheme does not at this stage distinguish apartments in terms of tenure. Discussions are underway with the Council's Housing team and with suitable Registered Providers to determine the best approach to affordable housing that may help optimise any contribution made and determine the nature and scale of provision which would then feed into the viability appraisal and any s106 commitments.

Master-planning

14. The site and adjoining TFL land represents a void in the townscape and public realm that detracts from the urban form which undermines the character and appearance of the area. It reduces the sense of this being a safe and well considered public space and leaves a gap within the urban form and frontage provided by the street pattern within the area.

15. Hence the proposals aim to complete the townscape form by proposing an urban block that frames Station Yard and offers a new end point to views northwards along Station Approach and eastwards along the northern section of Queens Road and Station Road.
16. The proposals also ensure that when viewed from London Road, the current void is appropriately framed by a building that responds positively to the context of The Exchange and Bridge House. This is also followed through in views from the north, around Brewery Lane and the south along Railway Approach, Station Yard and Queens Road.

Design – bulk, scale, massing and form

17. The proposals set out in the accompanying design statement have adopted a form that reflects the location of the site and its relationship with the surrounding built form. They have had regard for policy LP2 in relation to building heights and are based on an assessment of prevailing character, which in this case spans the taller, larger footprints of the buildings fronting London Road. The site's context is in large part determined by Bridge House, The Exchange and the emerging buildings at Twickenham Station. The relative heights are shown later in this report and range from 26m AOD to 29.17m AOD for Bridge House, 32-35m AOD at the Station and 40m AOD for Regal House opposite.
18. The site also has to relate well to the residential apartment buildings and homes to the south and west, fronting Station Approach, Station Yard and Queens Road, where the majority of buildings are 3 storey pitched roof design. It therefore needs to form an intermediary role that provides for a stepping down towards the western end of the site.
19. The AAP suggests a height range 3-4 storeys above ground floor level to relate well to the residential scale to the west and a height that takes its cue from Bridge House to the east, which stands at around 29.17m AOD. The proposed scheme has a roof level of 28.80m AOD, respecting the existing bridge house datum.
20. Paragraph 4.2.2 of the Local Plan notes that Twickenham town centre and the area around the station is an area where taller buildings would be appropriate, subject to respecting local character. This document sets out the preliminary studies and shows how the building sits within a context of similar forms and scales and hence meets this requirement.
21. The studies set out in this document show how this has responded to the urban grain, massing and form and why the proposed form is appropriate and is the best use of the site. Preliminary assessments have been made of overshadowing, overlooking, micro-climate and

townscape in determining the most appropriate form, consistent with the form of neighbouring buildings.

Design - Visual Appearance

22. The design language is still at an early stage and will be developed following the initial pre-application discussions.

Heritage

23. The site itself is not within a conservation area and does not include any heritage buildings but the Queens Road Conservation Area (number 47) lies to the west of the site, including the Albany Public House, which is a building of townscape merit, and extends southwards away from the site. Hence regard has been had to LP3 and the impact on heritage assets.

24. Hence the principal interaction with the conservation area is in the immediate vicinity of The Albany, a building of local significance. The major part of the conservation extends south and is a relatively enclosed and contained character that would not be impacted by the development.

25. The pre-application proposals maintain the setting of the Albany and a sense of separation between this and the proposals.

26. Although outside of the control of Solum Regeneration (Twickenham) LLP, the pre-application proposals also show a range of potential public realm enhancements to the area in front of the pub.

27. The views identified within this Design and Access Statement offer a modelled perspective of the impact on local views and views within the view analysis offer perspectives from within the conservation area. These show the scale and form respect the conservation area and protect and enhance its character and setting. There are no significant views of the site further south within the conservation area where the development is screened by the buildings framing Queen's Road and the adjoining streets.

28. Verified views have also been provided as part of the application.

Residential Mix

29. The residential mix is proposed to be predominantly studios & 1 bedroom apartments, which reflects the likely demographic of occupiers given the town centre location, close to public transport

connections and the station, the highly accessible PTAL rating of 4/5 and consequent absence of residential car parking provision. While policy supports the provision of family accommodation over much of the Borough, it recognises that there are circumstances such as this where a smaller unit mix is more appropriate. This is specifically recognised for the centre of Twickenham in the new Local Plan.

Amenity

30. The amenity standards of the residential apartments will comply with Local Plan policy LP35. All are designed to meet national space standards and will have access to good quality private amenity space, oriented to benefit from natural light and avoid issues of privacy or overlooking.

31. A range of environmental and technical studies have been undertaken to assess the proposals and ensure they meet the relevant standards in terms of: noise, micro-climate, flood risk and drainage, air quality and other matters.

Transport

Access

32. Access and servicing for the site would continue to be via Mary's Terrace.

33. The western end of Mary's Terrace is closed at present with controlled barriers which allow for emergency access but not through traffic. This principle would be maintained.

Parking

34. The initial pre-application discussion indicated a preference for car free development, given the site's central location within the town centre and in an area of high public transport accessibility. Hence no parking provision is made for the residential apartments, other than for the provision of 2 x disabled parking spaces.

Servicing and Refuse

35. Servicing is to be undertaken at ground level from the service area and separate recyclable and non-recyclable bins are provided for within the bin store.

Cycle parking

36. Cycle parking is provided in accordance with London Plan standards, accessed from the rear of the building.

Construction Environmental Management Plan

37. The application is accompanied by a construction environmental management plan (CEMP) that defines the main construction stages and each of the methods to be adopted to control construction and related impacts on the environment and amenity.

Conclusion

38. The proposals set out within this application submission represent a proportionate and positive response to the constraints and opportunities of the site.

39. The application is important to progress the delivery of the key project outcomes:

- The development of an allocated site that is part of the regeneration aims for Twickenham;
- The provision of new residential accommodation to address the need for housing in Richmond and London;
- The development of a built form, spaces and public realm that positively contribute to the enhancement of townscape;
- The protection and enhancement of the setting to heritage assets within the area;
- A development that is inclusive and integrated;
- The creation of a sustainable, energy efficient and environmentally sound development that tackles the effects of climate change.

40. The proposals are for a predominantly residential development for which there is a clear and rational basis in both policy and in the characteristics of the area.

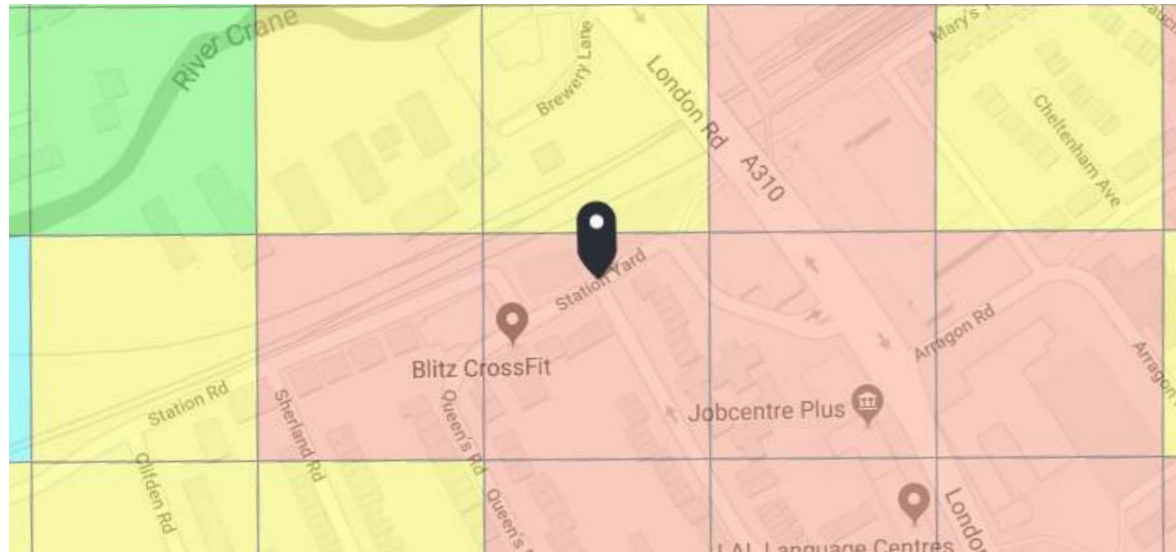
41. The proposals for the site will assist regeneration, deliver a significant qualitative and quantitative improvement in the contribution the site makes to the vitality of Twickenham.

42. Solum Regeneration (Twickenham) LLP wish to engage positively to progress the concepts set out within this submission to progress these proposals and deliver these objectives.

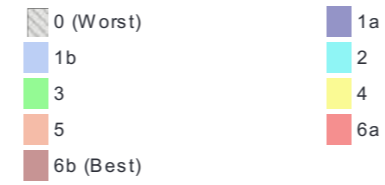
02. THE SITE

THE SITE

Planning Assessment



Map key- PTAL



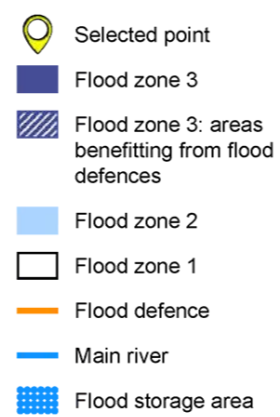
PTAL Map

The site has a Public Transport Accessibility Level of 4/5 (output for Base Year)
 The site is a few minutes walk from Twickenham Station



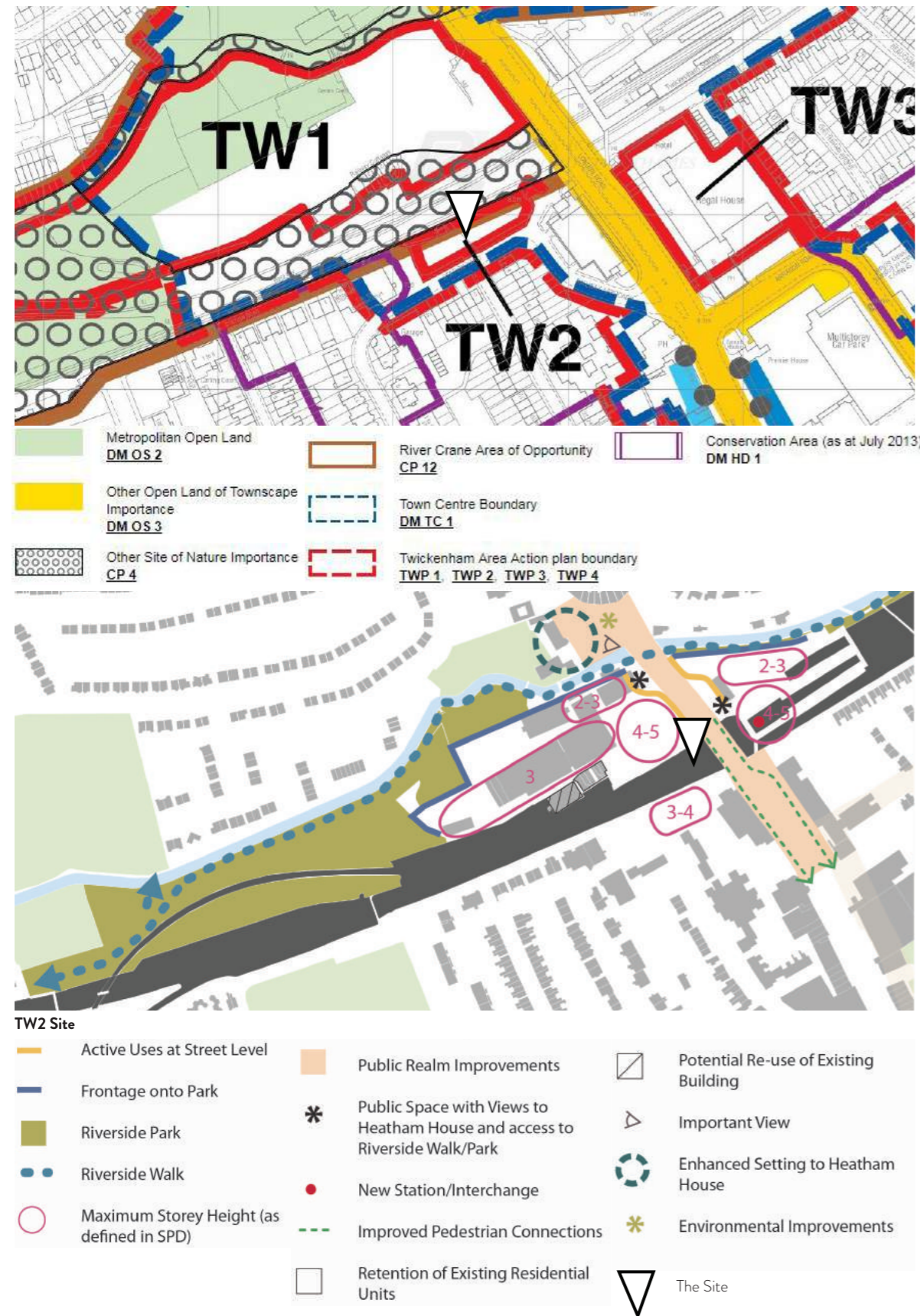
CPZ Map

The site is in a Controlled Parking Zone
 It belongs to Zone D, Central Twickenham



Flood Risk Map

The site is in Flood Zone 1



Local Plan - Proposal Maps July 2015

The Site is affected by:

- Proposal Site TW2 Station Yard
- Town Centre Boundary DM TC 1
- Twickenham Area Action plan boundary TWP1, TWP2, TWP3, TWP4
- Adjacent to the Conservation Area boundary.
- BTM Albany pub facing Solum Site.
- Twickenham Area Action Plan

7.2 Northern Approach

7.2.6 Proposal Site TW2 Station Yard

7.2.6.1 Aims - Subject to adequate re-provision of the bus stands, to redevelop the site with a residential scheme up to 3-4 storeys from ground level. Part of the site to be developed as a public car park provided it is required to replace other parking lost from the town centre. Buildings should be of appropriate height and should integrate well with the surrounding residential area and enhance the setting of the Albany PH, with improved public realm and landscaping.

7.2.6.2 Uses - Residential development up to 3-4 storeys from ground level, landscaping, and public car parking if required and feasible.

Design Guidance

7.2.6.3 The Core Strategy and Development Management Plan (superseded by the Local Plan in 2018), Twickenham Station and Surroundings SPD and Design Quality SPD will all apply. In addition, the following guidance should also be followed:

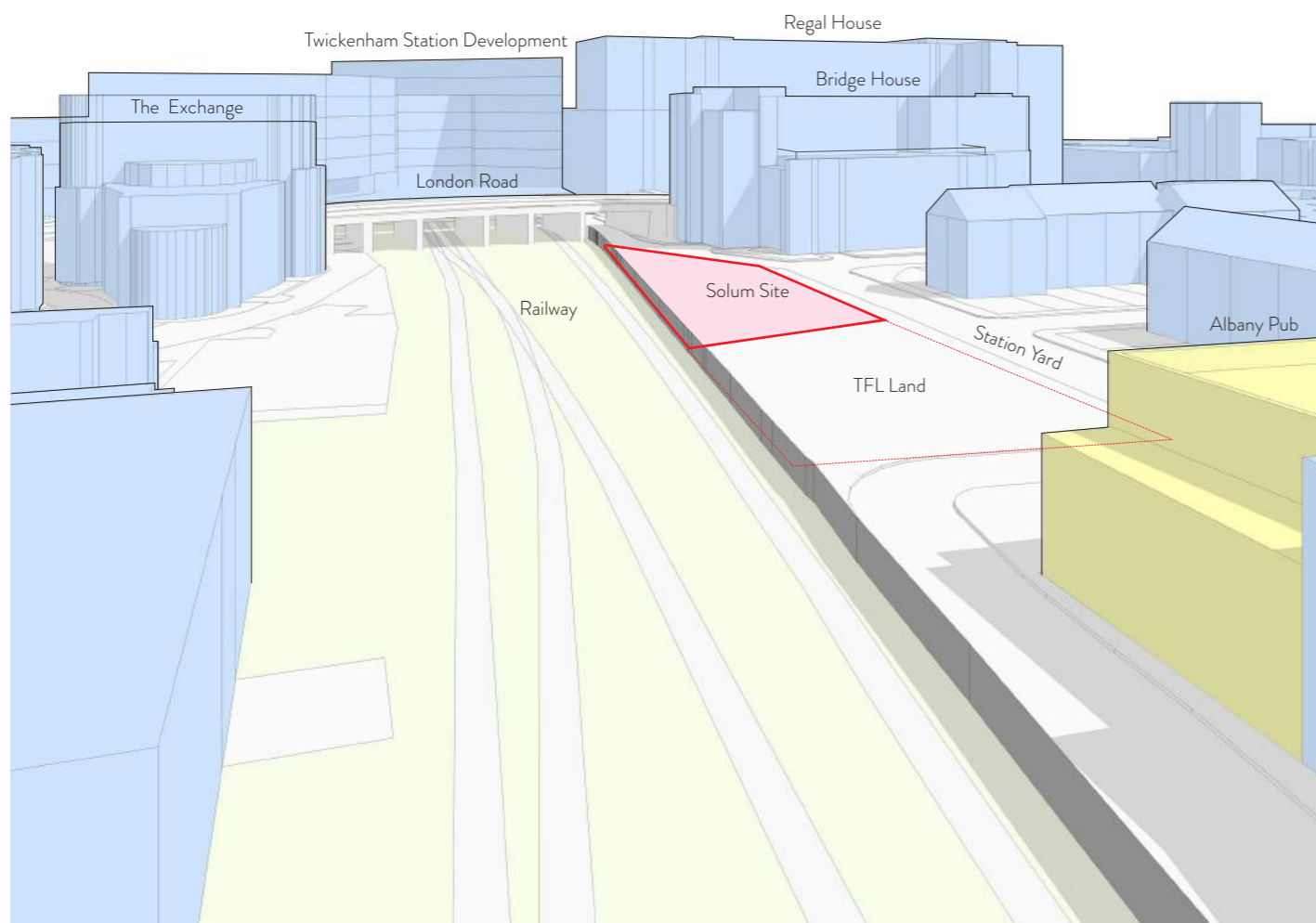
- Height/massing should form an intermediary between that of Bridge House and surrounding development;
- Maximum height of 3-4 storeys from ground floor level to ensure development is compatible in scale to the surrounding residential area, including the Queens Road conservation area;
- Designed to integrate well with the surrounding residential area including the Queens Road conservation area;
- Excellent design to provide a positive frontage towards the Albany Public House (a Building of Townscape Merit) and its setting;
- The opportunity should be taken to provide a small area of landscaped space to improve the area closest to the Albany PH;
- Relocation of bus stands;
- High quality tree planting and other landscaping to improve the quality of the environment;
- Public car parking will only be provided to replace any parking lost to the town centre as a result of regeneration; and will be subject to a traffic assessment.

THE SITE

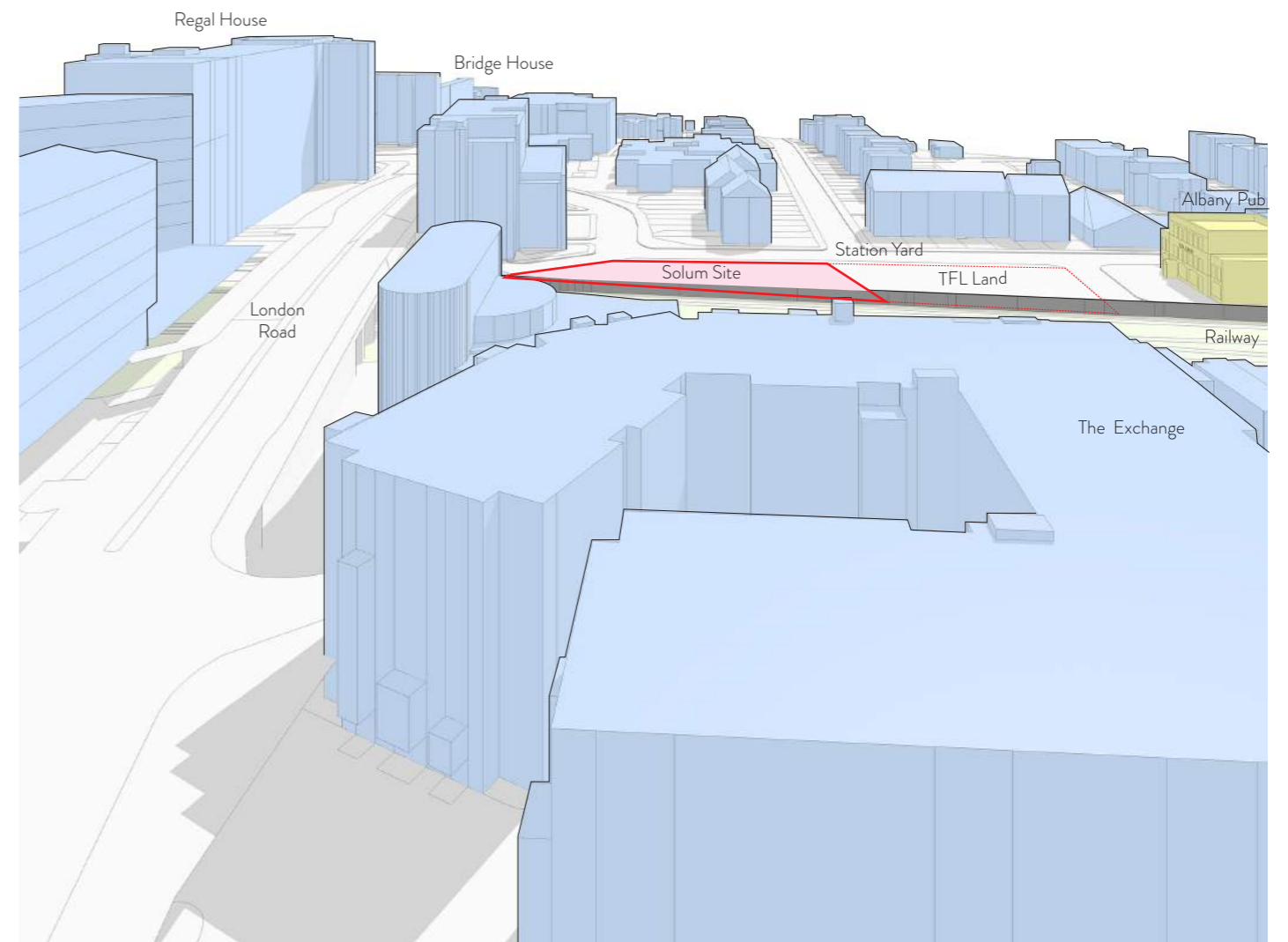
Site Location



Site Location



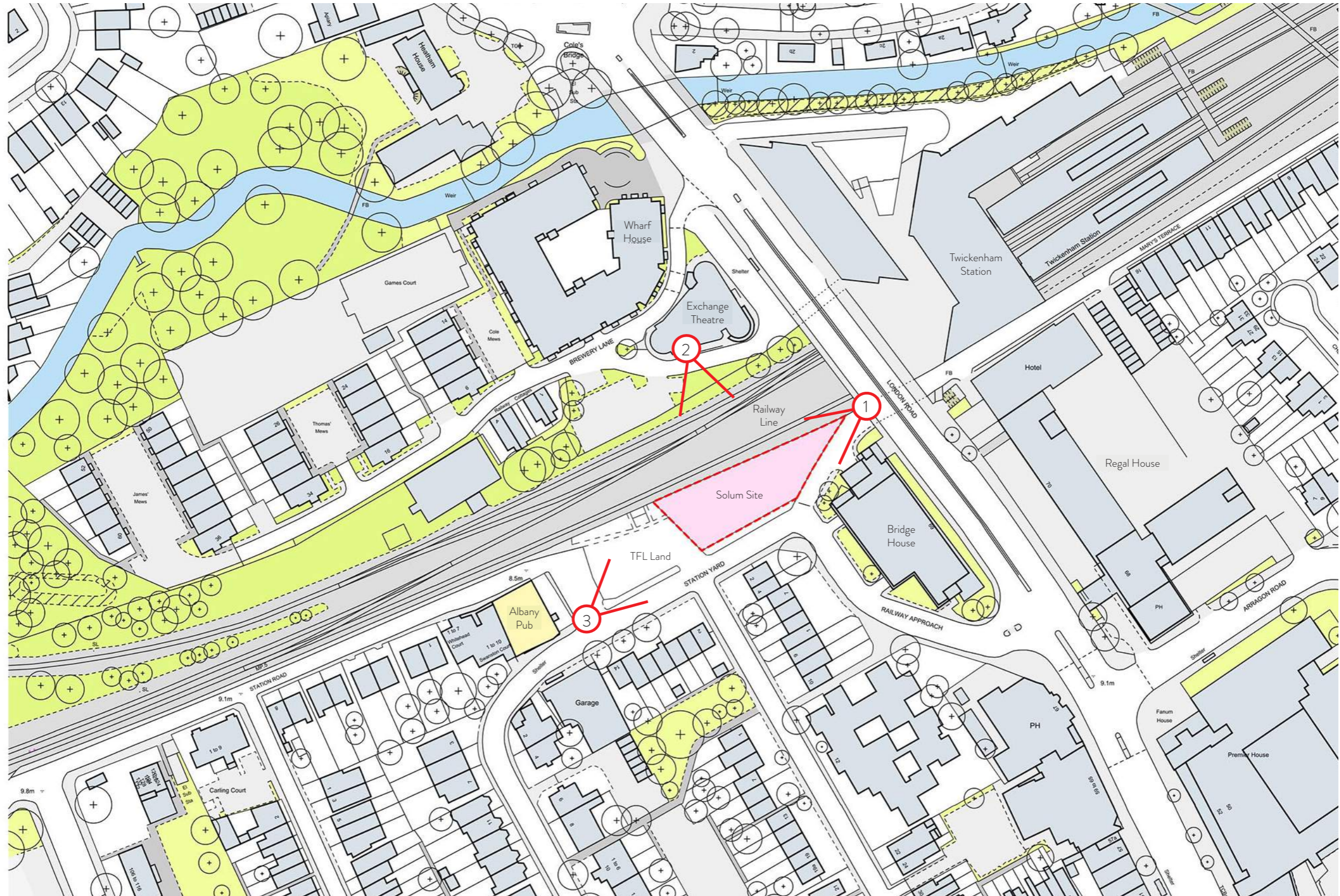
Aerial view 1 of site & context from west



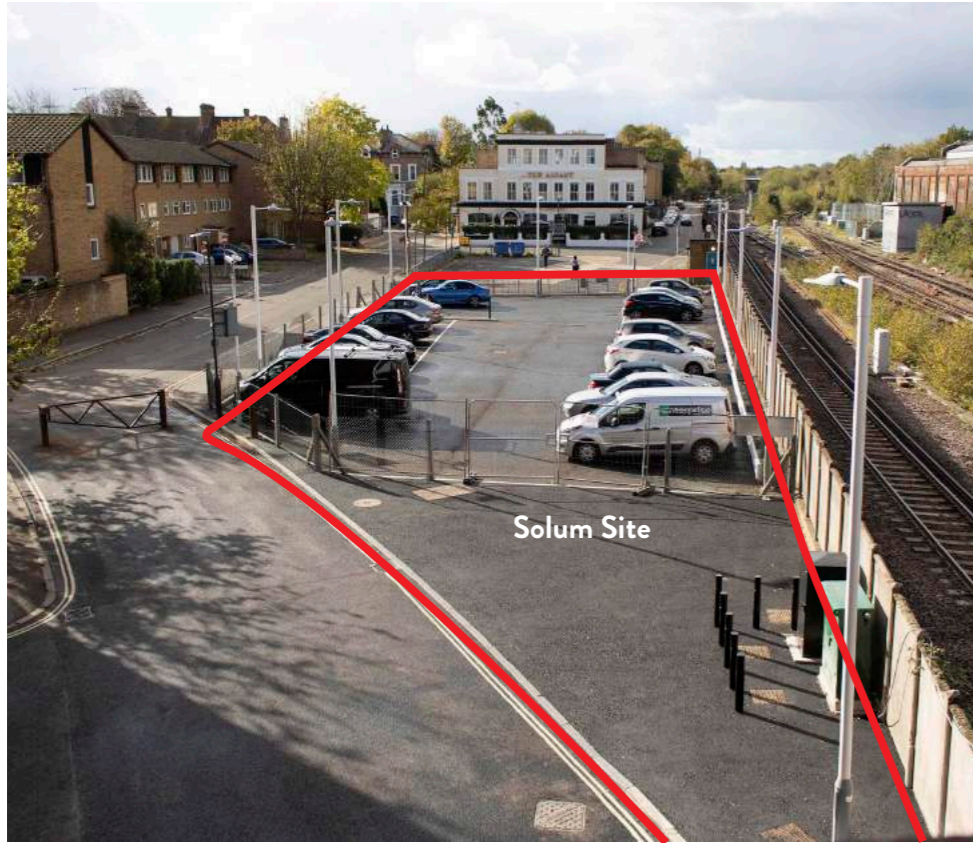
Aerial view 2 of site & context from north

THE SITE

Location Plan



Site Photos



View 1 from London Road Bridge



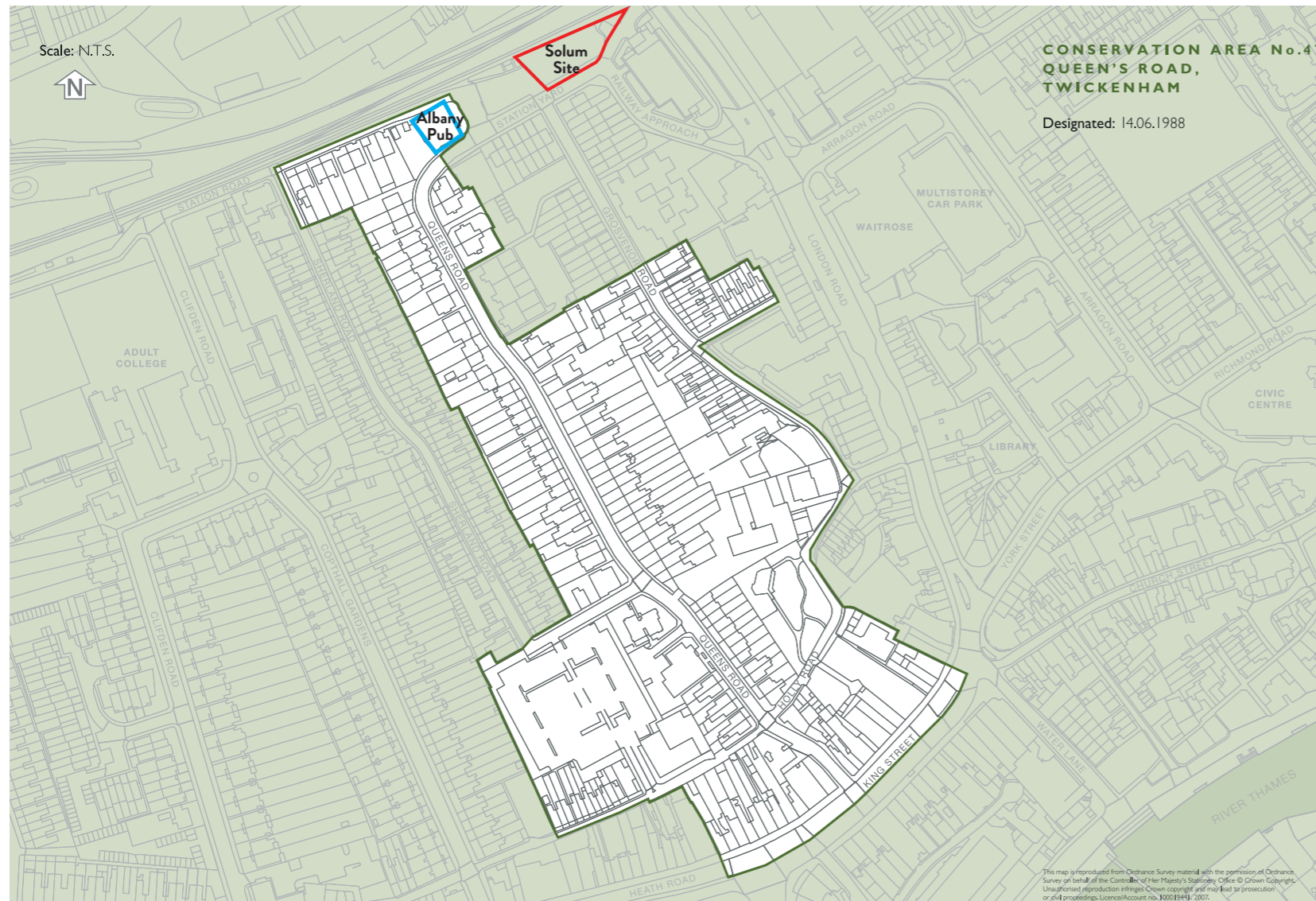
View 2 from the Exchange



View 3 from Station Road

THE SITE

Site History



Queens Road (Twickenham) Conservation Area 47

Buildings of Townscape Merit: Albany Public House

Character: The Albany public house, an imposing three storey stucco building on an island site, acts as a focus to the north end of the conservation area.

Address: 1 Queens Road, TW11 0LX

Ward: Teddington

Listed Date: 20/09/1988

Ref. Val.: 88/00409/BTM



Planning Precedents

92/1464/FUL - Old Station Forecourt, Railway Approach, Twickenham for Change of Use of land for public car park. Decision issued: 13/01/1993

95/0286/FUL - Old Station Forecourt, Railway Approach, Twickenham for Change of Use of land for storage of cars and commercial vehicles. Decision issued: 22/06/1995



1761
Historic Settlement Patterns



1896



1957

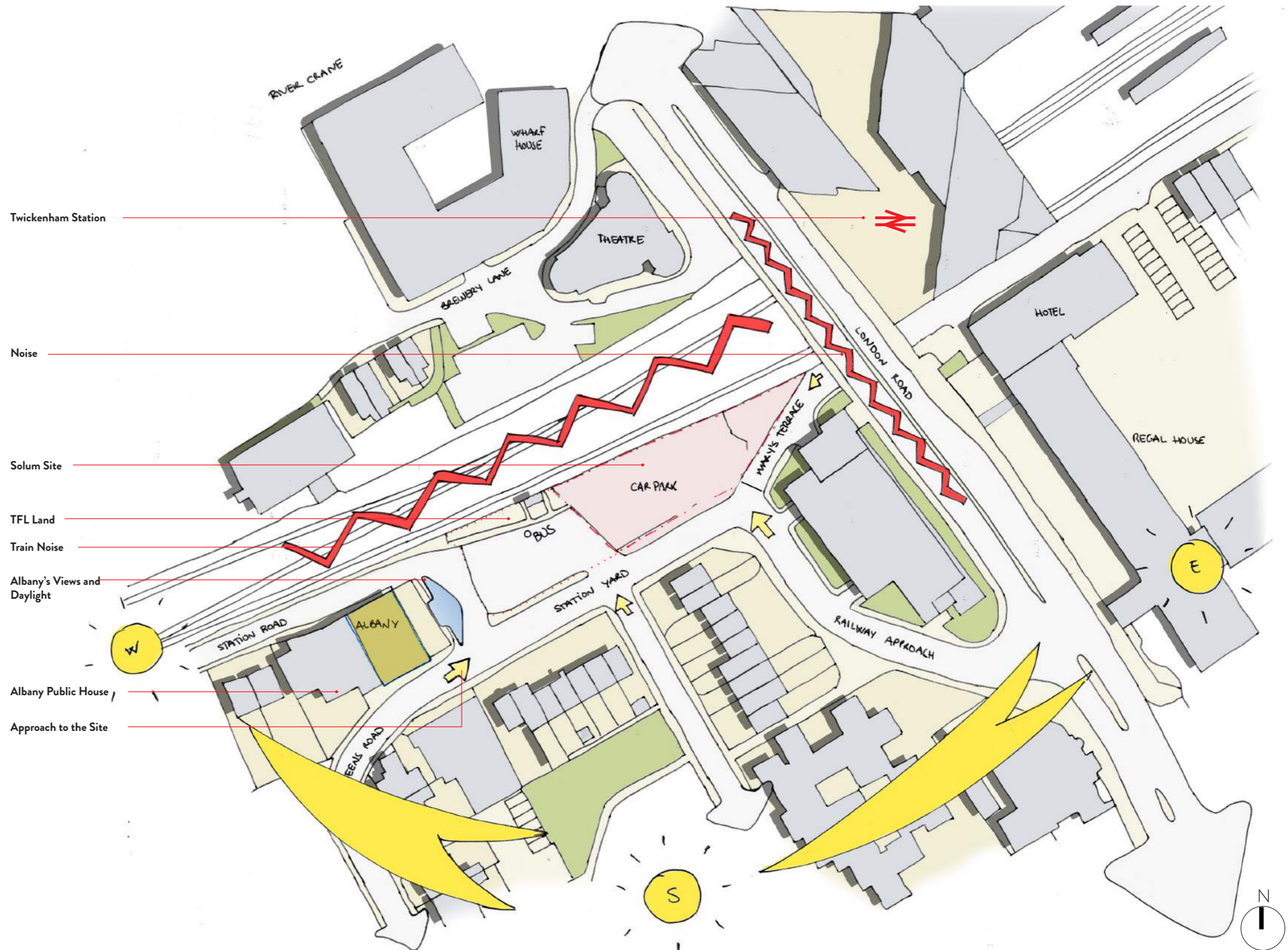
Analysis

Site Constraints

- Noise from railway and London Road
- Services - Thames Water sewer under the site
- Bus stop and manoeuvring opposite to the site to be accommodated
- Narrow site with one long side facing north
- Requirement to maintain 13 spaces for railway use











Site Opportunities

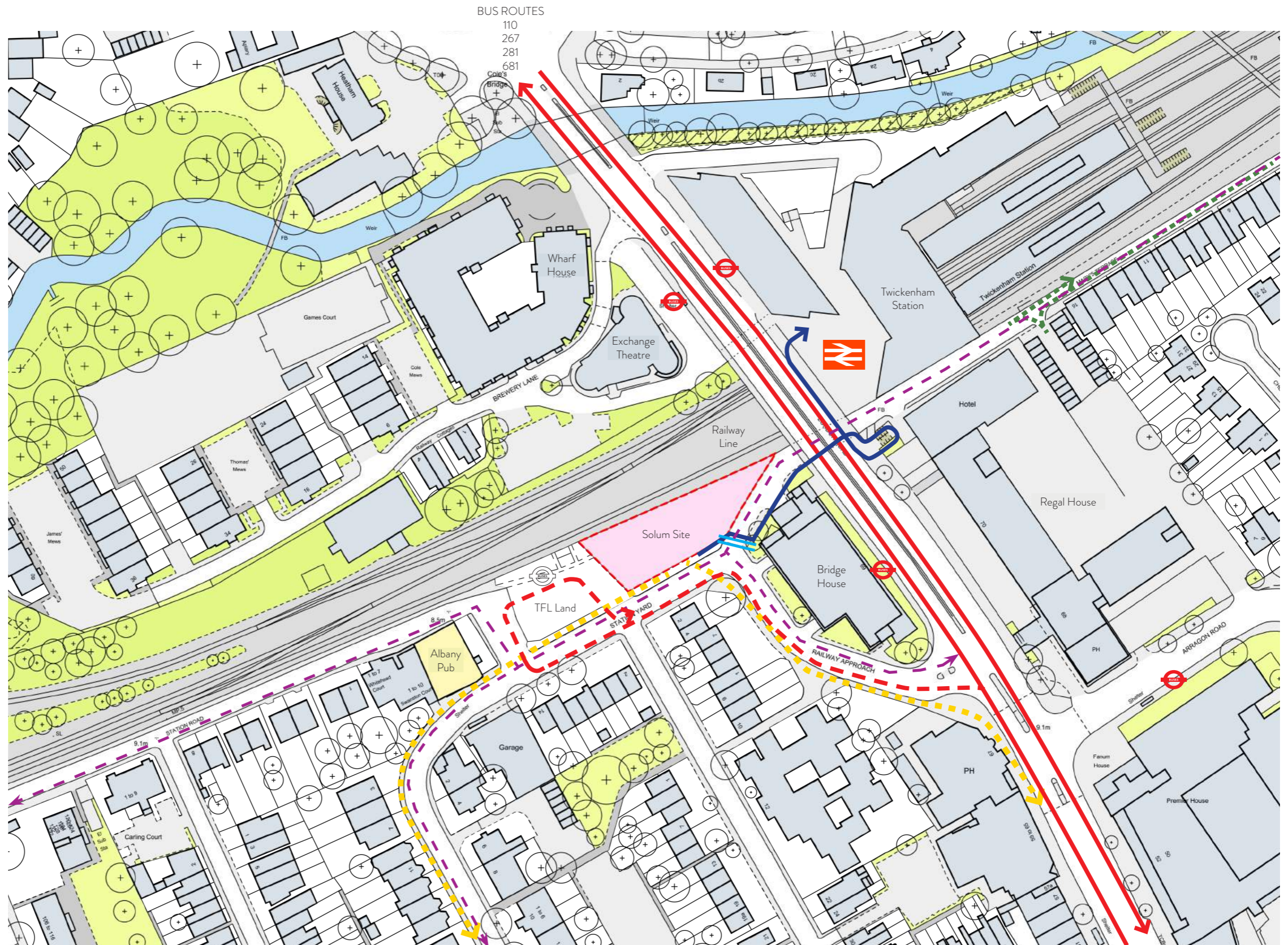
- Opportunity to regenerate the area and celebrate the BTM Albany Pub
- Opportunity to create a new landscaped area in front of the Albany Pub
- Opportunity to build a sustainable residential development opposite to the train station
- Opportunity to infill an empty urban space with a new square and meet the need of the local community.



THE SITE

Existing Circulation Routes

- Quiet cycle routes* 
- Pedestrian route to twickenham shops 
- Pedestrian route to station 
- Vehicle traffic 
- Secure access barrier location 
- Bus matchday emergency overflow 
- Bus routes 
- Bus stop 
- Temporary bus stop 
- Twickenham station 



*Source: <http://www.richmondlcc.co.uk/wordpress/wp-content/uploads/2011/02/Richmond-Cycle-Map.pdf>

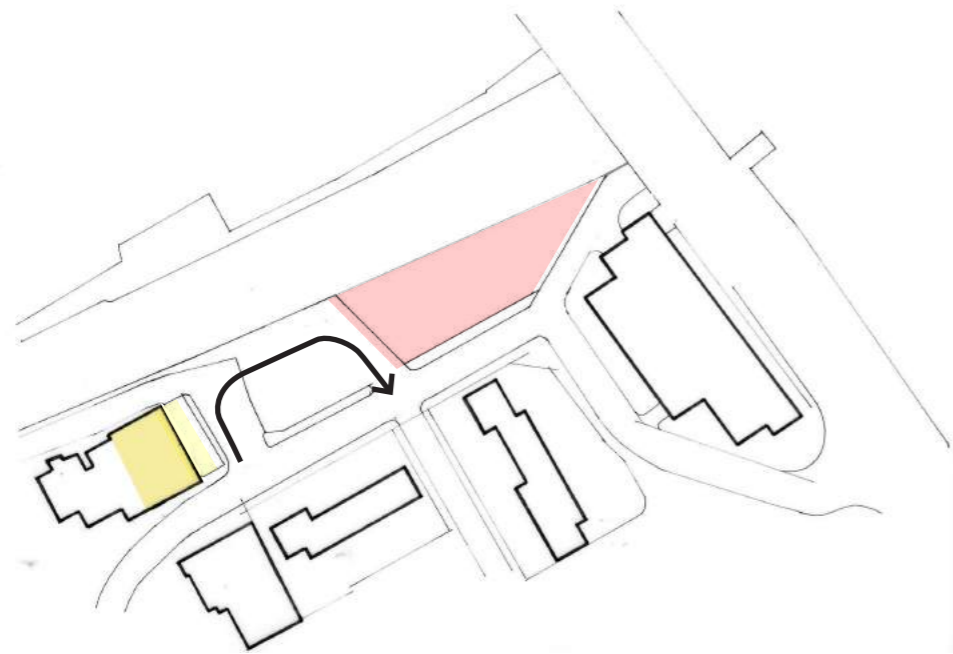
03. SITE RESPONSE

SITE RESPONSE

Previously Considered Masterplan Opportunities

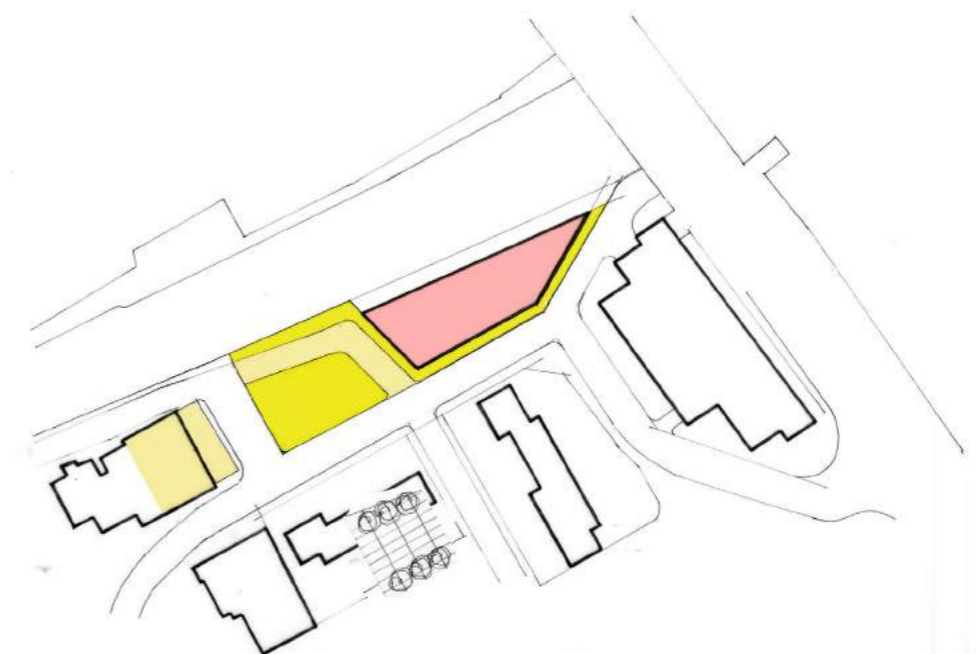
The following studies were undertaken in order to consider the potential for future development of the TFL land and ensure the appropriate form of development was proposed on the application site to ensure the masterplanning of the area and an appropriate redevelopment opportunity remains. However, in light of TFL's ongoing operational requirement for 5 coach/bus stands, no development is proposed at this point in time.

Existing Solum Site



OPTION 1

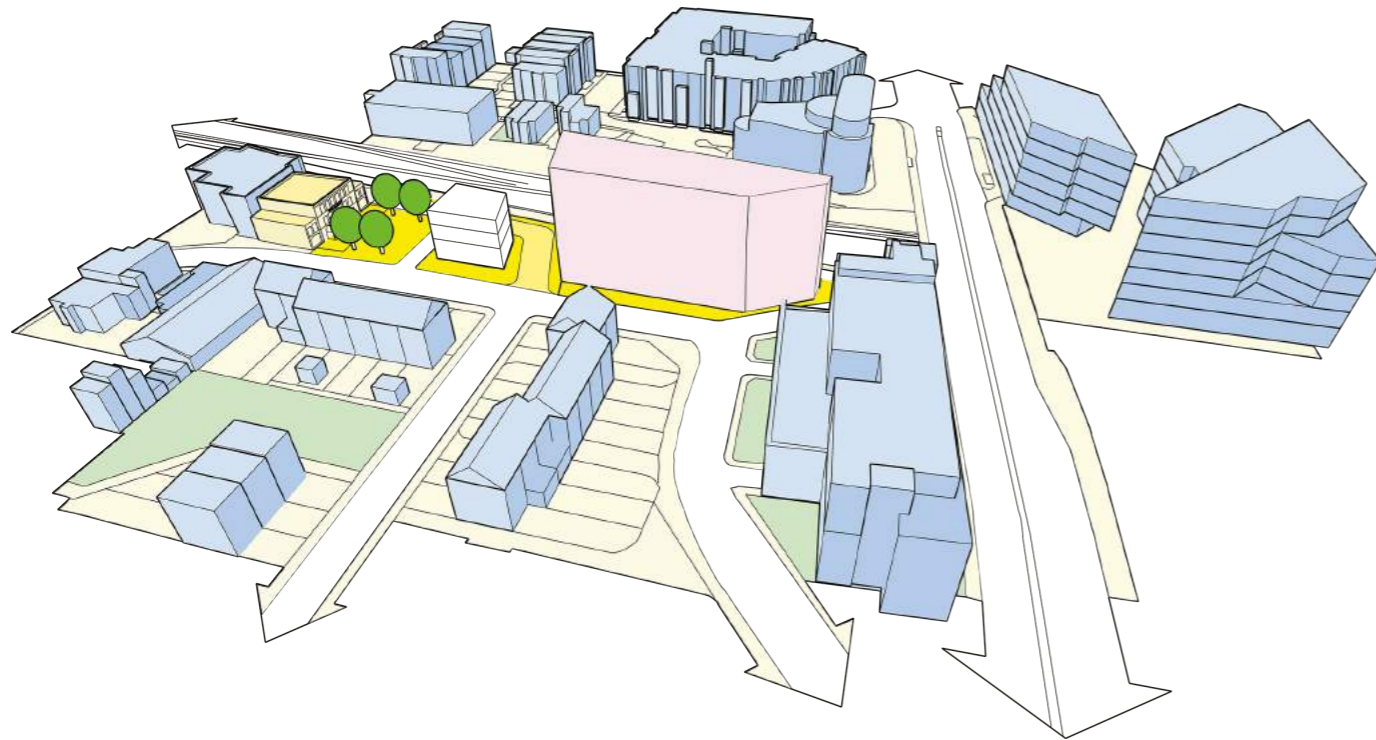
Solum Site developed & TFL Land landscaped to create a new Civic Square opposite The Albany



Previously Considered Masterplan Opportunities

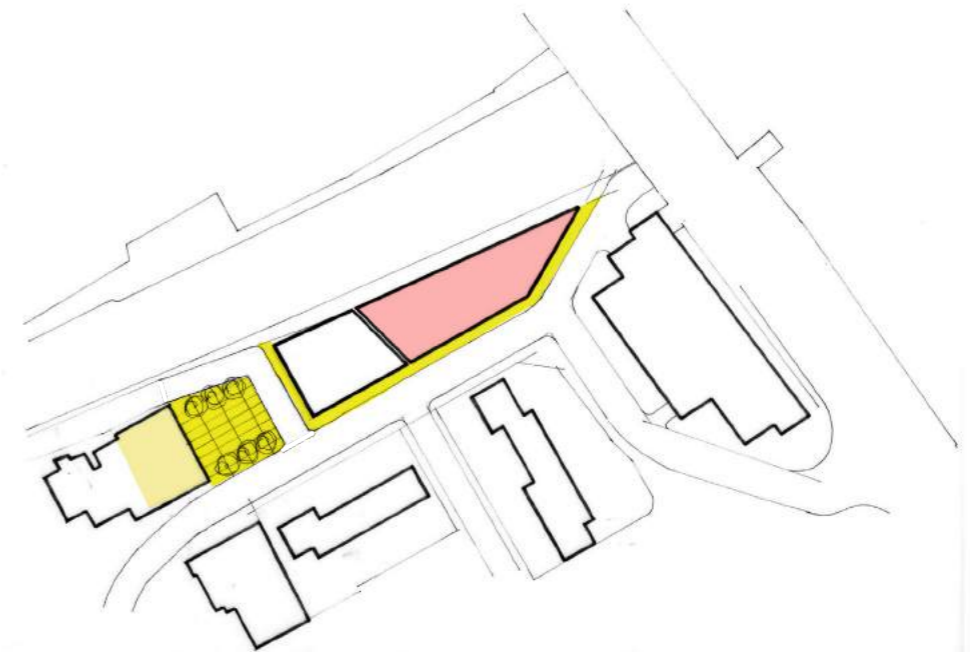
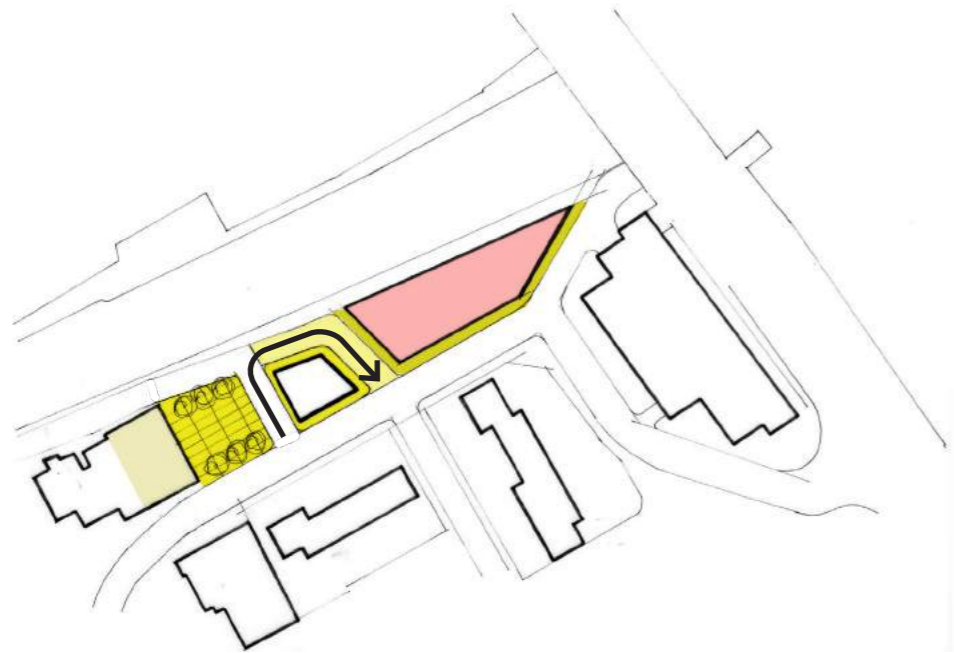
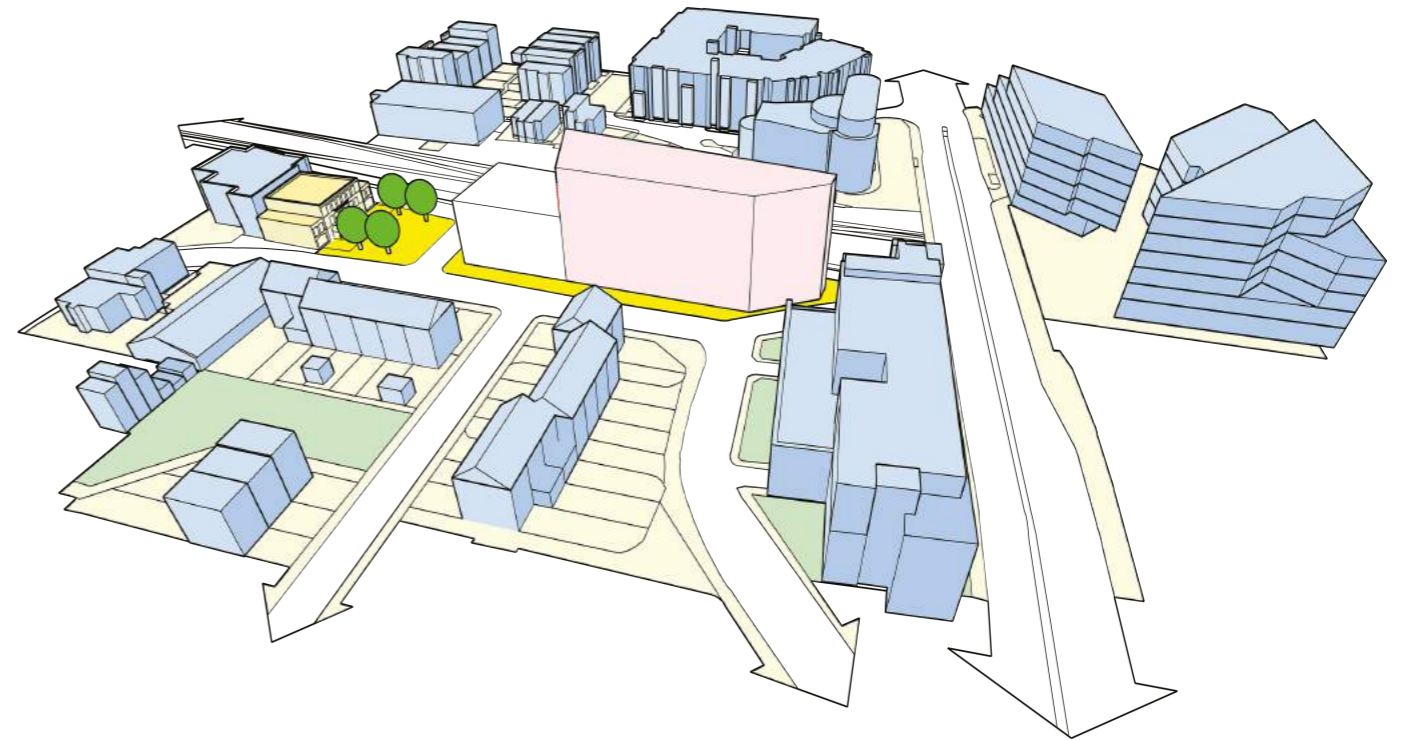
OPTION 2

Solum Site & TFL land developed separately with a new civic square in front of the Albany
*Dependant upon TFL operational requirements being met.



OPTION 3

Maximum development potential for both Solum Site & TFL Land.
*Not possible at this time due to the TFL operational requirements for TFL Land.



SITE RESPONSE

Technical Studies - TFL Land development Opportunities

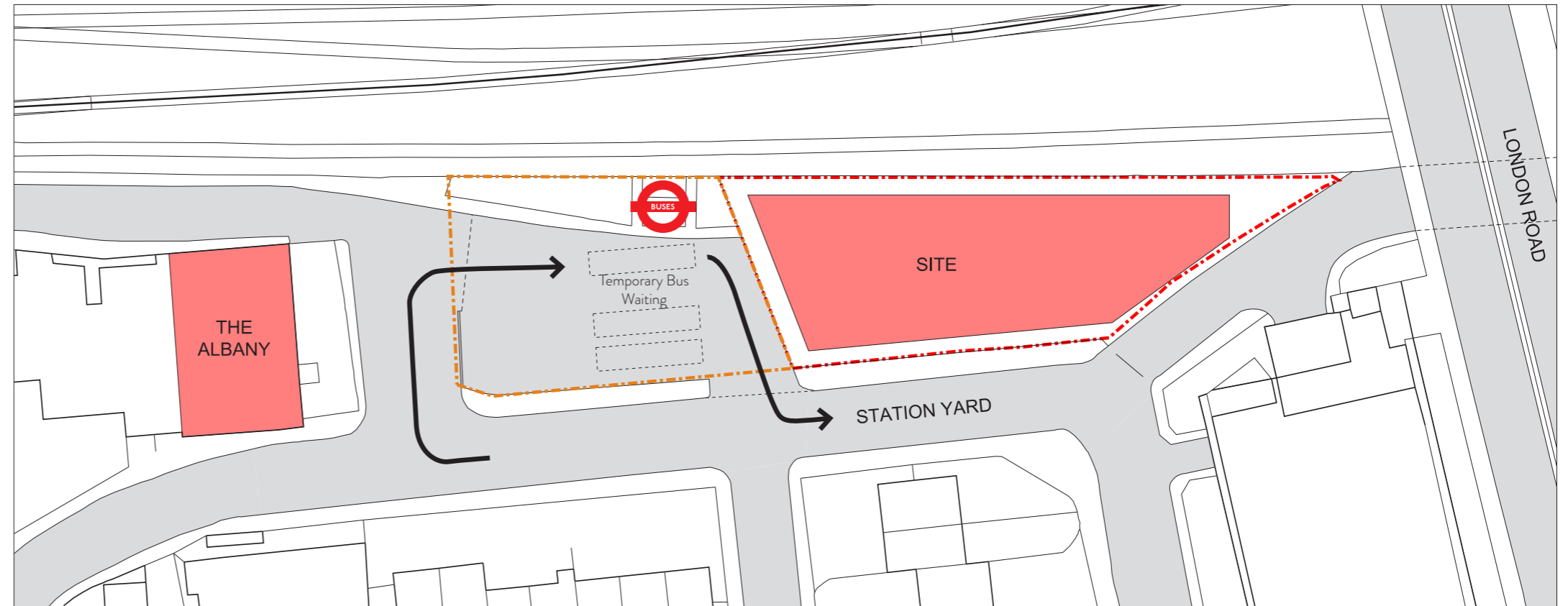


The adjacent land to the Solum site is owned by TFL. Solum have been in discussions with TFL and these pre-application proposals acknowledge that the TFL land cannot be delivered at this stage given the current operational requirement for a bus stand in this location.

The following studies look at the potential for the sites to be developed separately, whilst maintaining the current TFL operational requirements as turning circle.

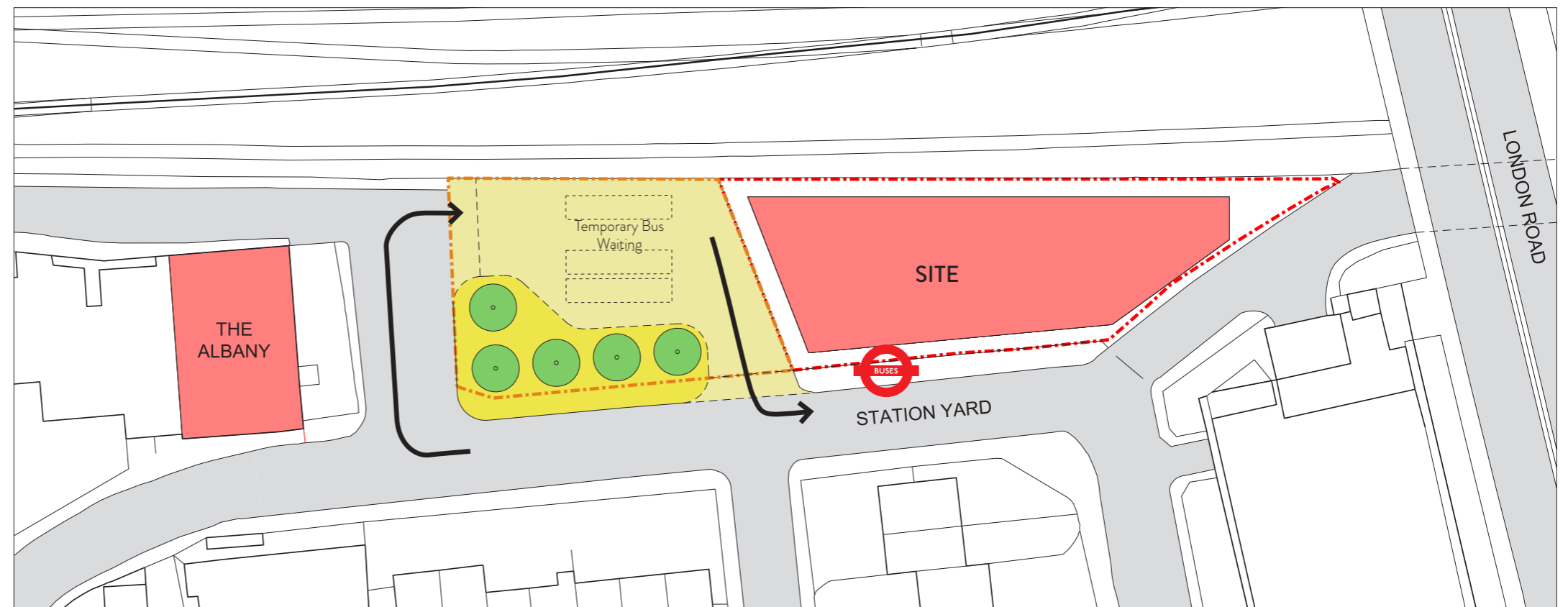
TFL Land Existing Operation

The TFL land adjacent to the Solum site is used to park buses on event days at Twickenham Stadium.



Option 1 - Landscape TFL Land

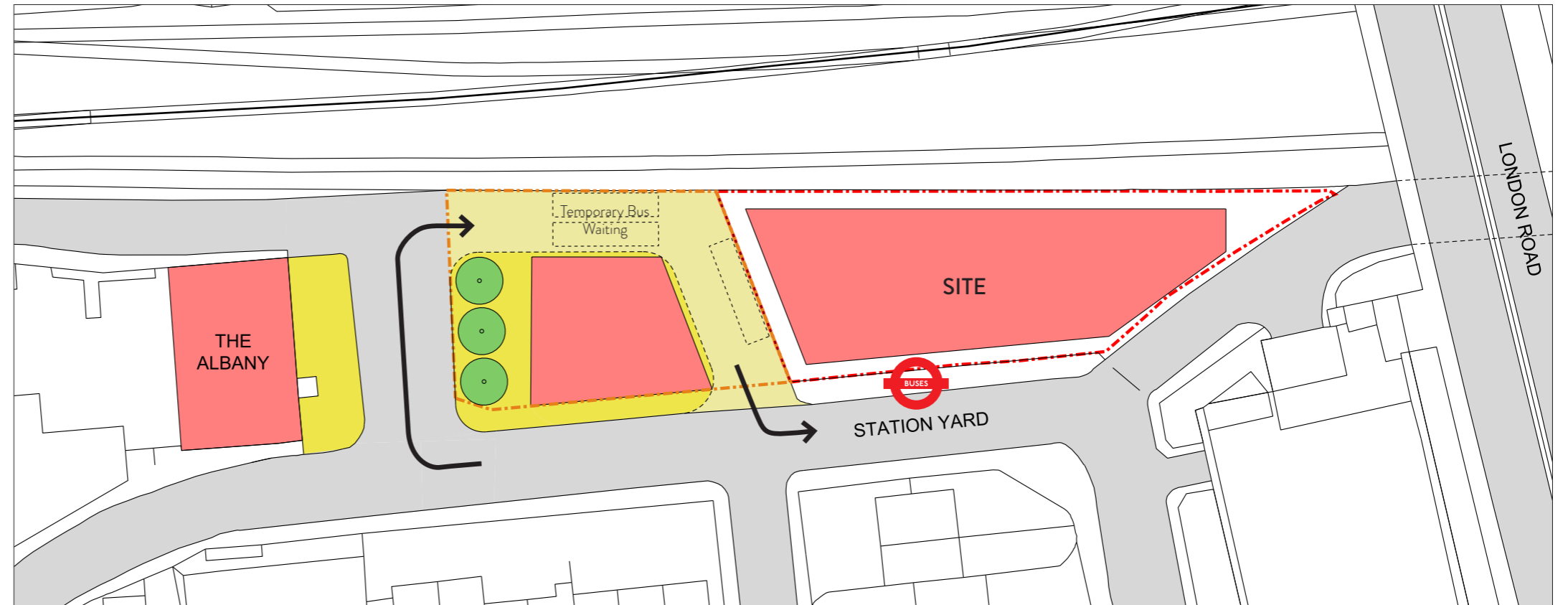
The TFL Land would be hard landscaped and used as a plaza with the ability to maintain its occasional function as an overflow parking area.





Option 2 - Develop South West Corner of TFL Land & Pedestrianise the Remainder

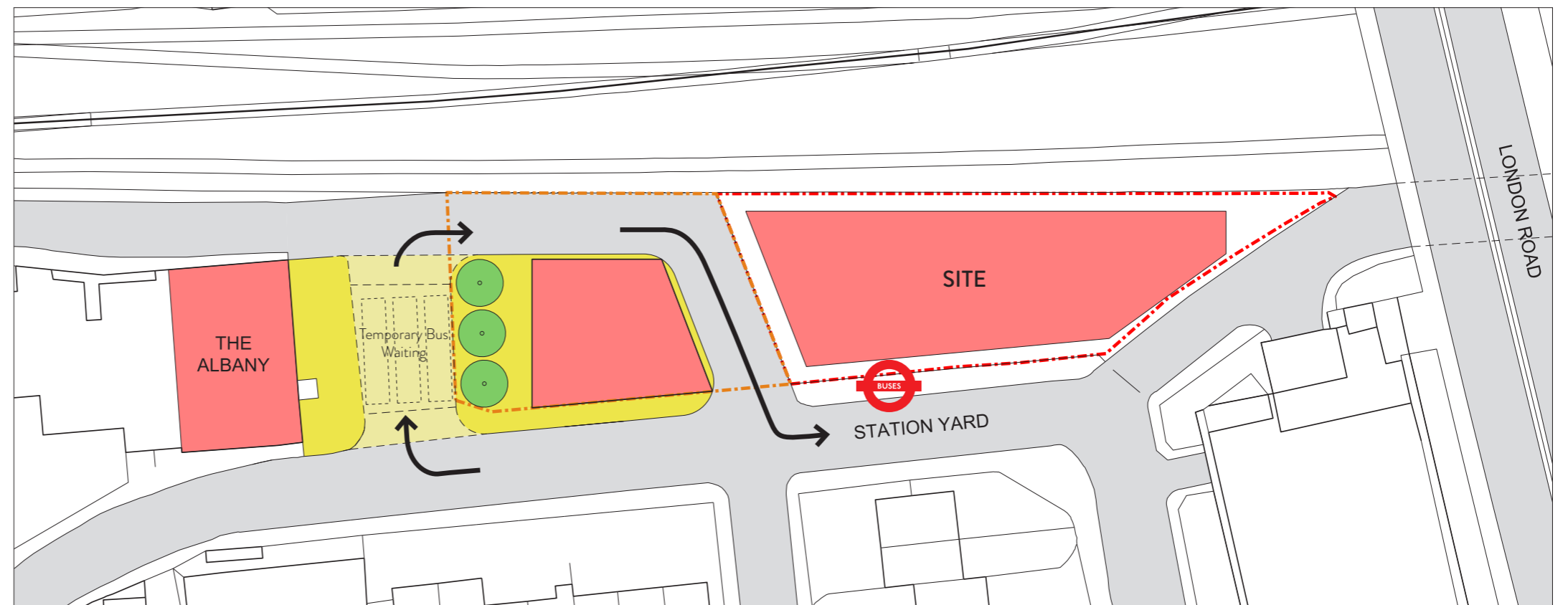
The south west corner of TFL land is developed and temporary bus overflow parking is accommodated to the periphery of the site. These are pedestrianised zones when bus parking is not required.



Option 3 - Develop TFL Land & Pedestrianise The Road In Front Of The Albany

Reroute road to provide an improved plaza connected to The Albany.

Overflow bus parking is relocated to the hard landscaped road in front of the Albany.



04. MASSING

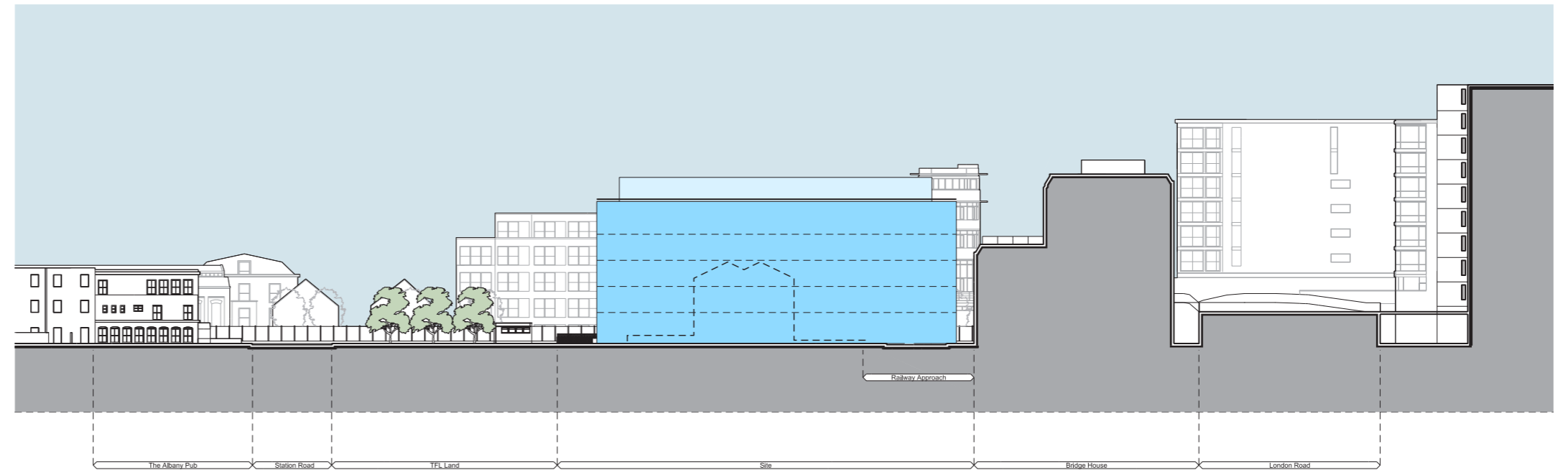
MASSING

Proposal

South Elevation

At Pre-App 1, it was indicated that the proposals for the Station Yard site should be limited to a maximum height of the neighbouring building, Bridge House, the highest point of which is +30.9m (AOD).

Therefore, we propose an overall height of ground + 5 storeys, filling in the step in the massing between Bridge House and the Albany Pub.



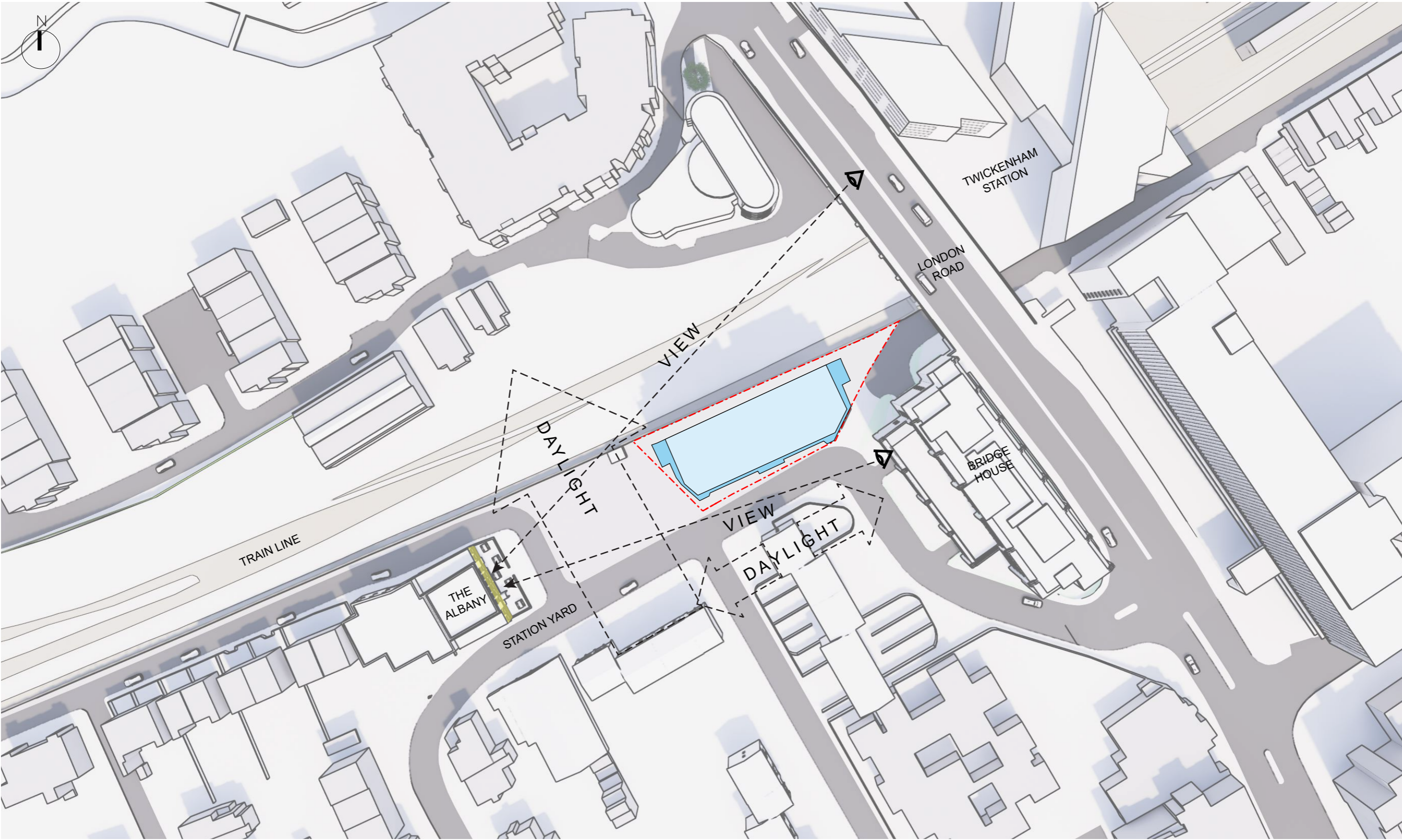
South Elevation

West Elevation

To further reduce the perceived size of the building, we are proposing that the top floor will be stepped back. This will reduce the visual impact from street level and ensure the building appears deferential to Bridge House.



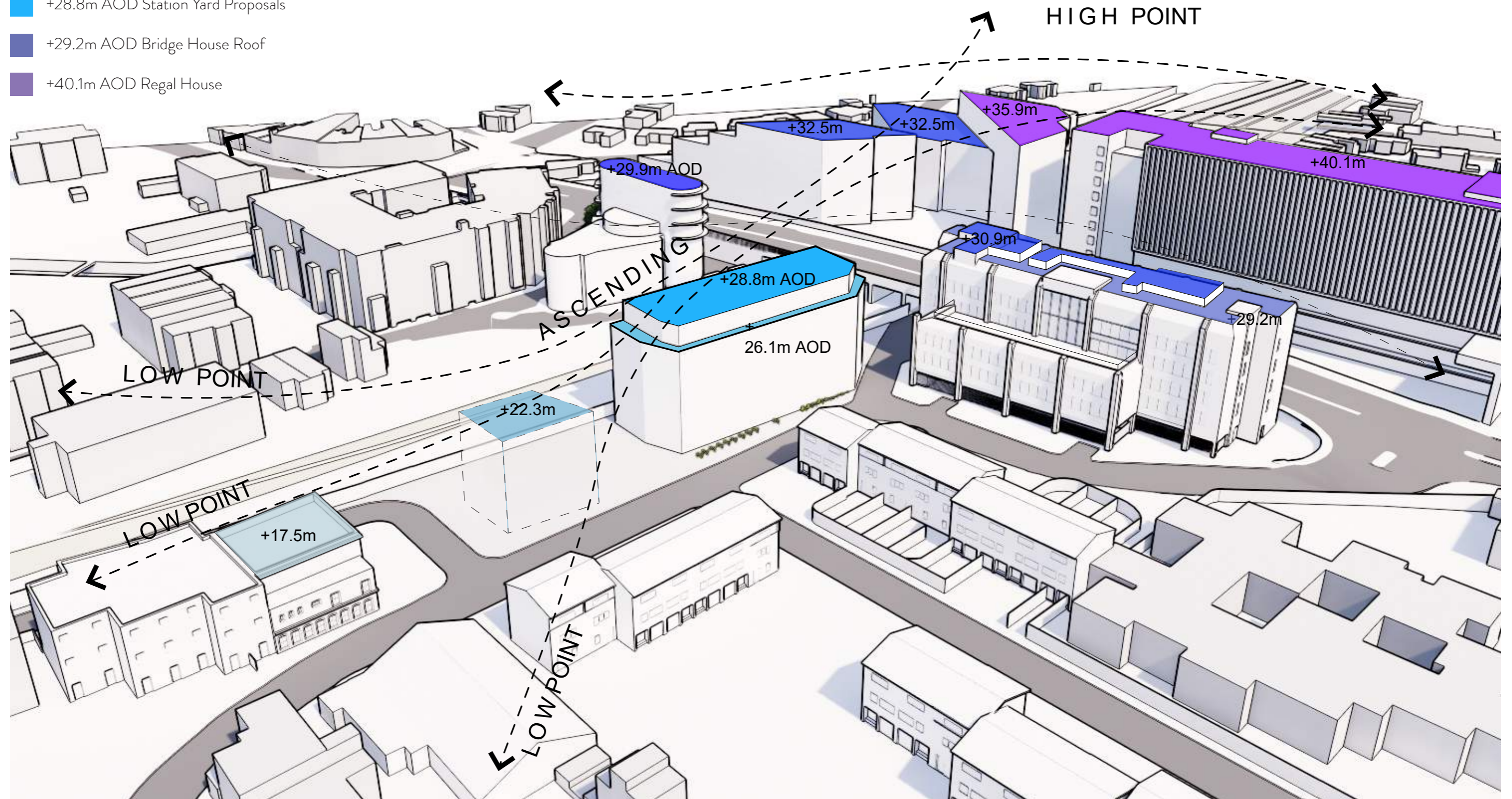
West Elevation



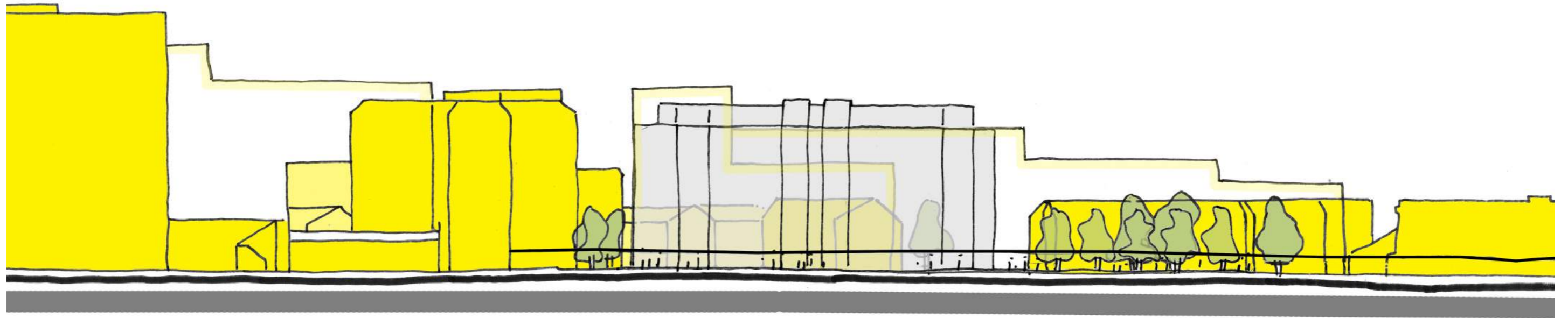
MASSING

Scale Within Wider Context

- +17.5m AOD The Albany
- +22.3m AOD Potential Future Development
- +28.8m AOD Station Yard Proposals
- +29.2m AOD Bridge House Roof
- +40.1m AOD Regal House



Scale Within Wider Context



Regal House

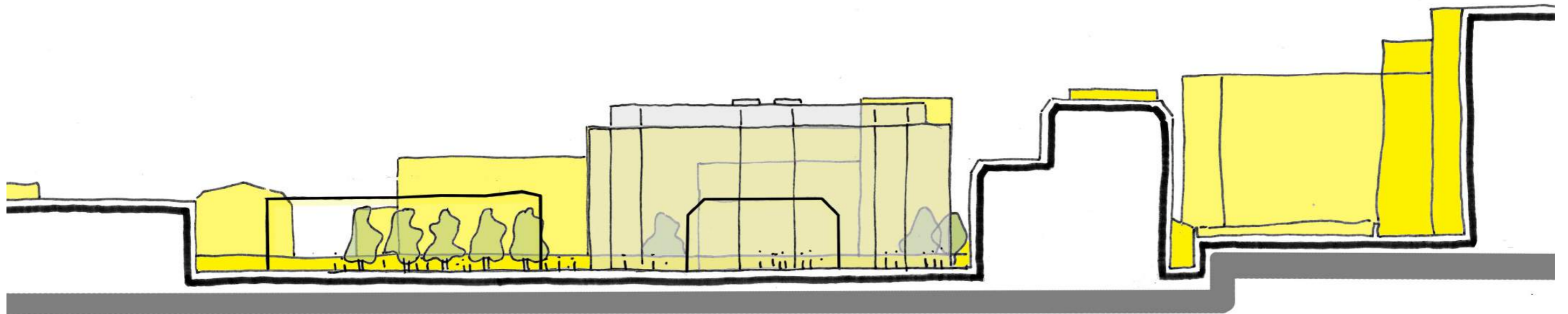
Bridge House

Station Yard

TfL Land

The Albany

North Elevation Massing Analysis



The Albany

TfL Land

Station Yard

Bridge House

Regal House

South Elevation Massing Analysis

MASSING

Scale Within Wider Context

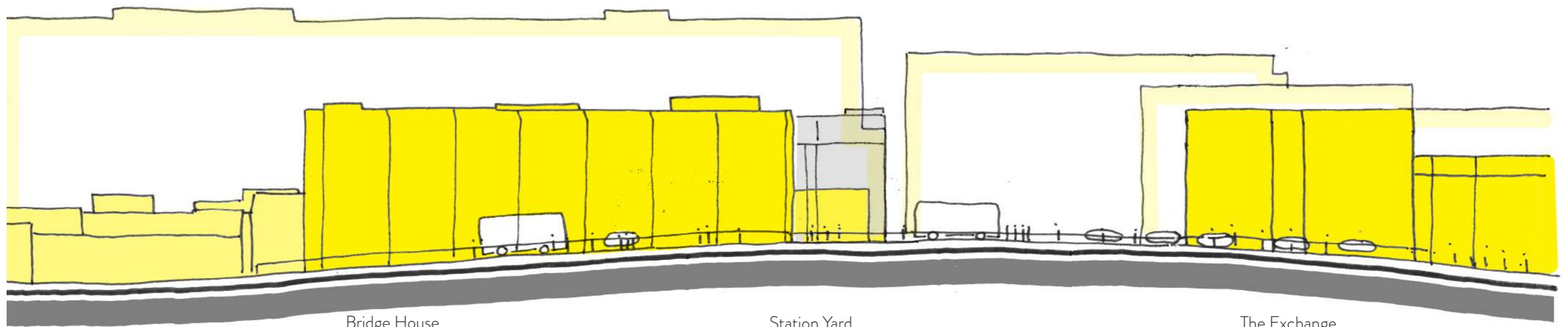


Brewery Wharf Development

Railway

Station Yard

West Elevation Massing Analysis



Bridge House

Station Yard

The Exchange

East Elevation Massing Analysis

05. TECHNICAL CONSTRAINTS

TECHNICAL CONSTRAINTS

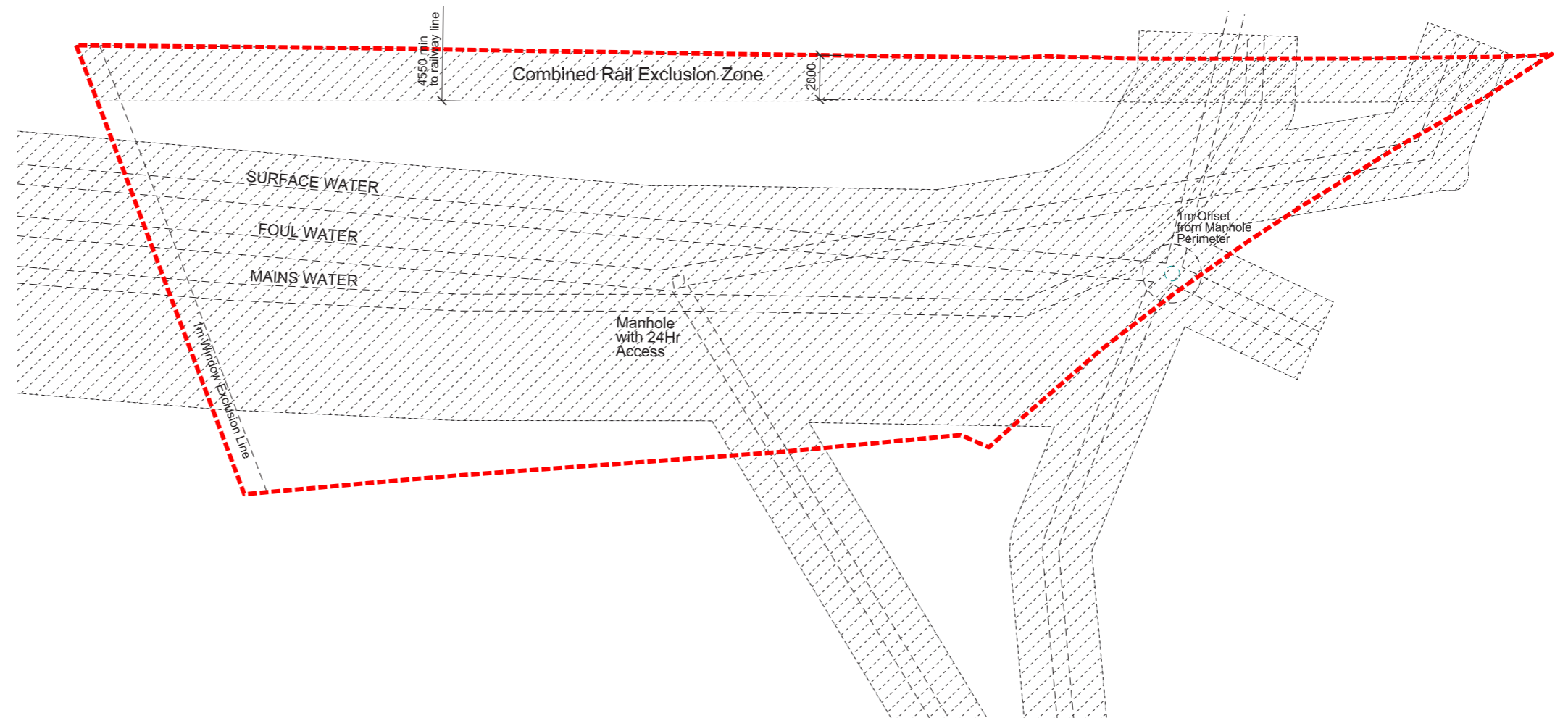
Existing Underground Services & Offsets

Existing Underground Utilities Plan

A variety of underground services lie beneath the surface, including surface and foul water and manholes. In close proximity to the site there are buried electricity, telecoms, gas and street light lines.

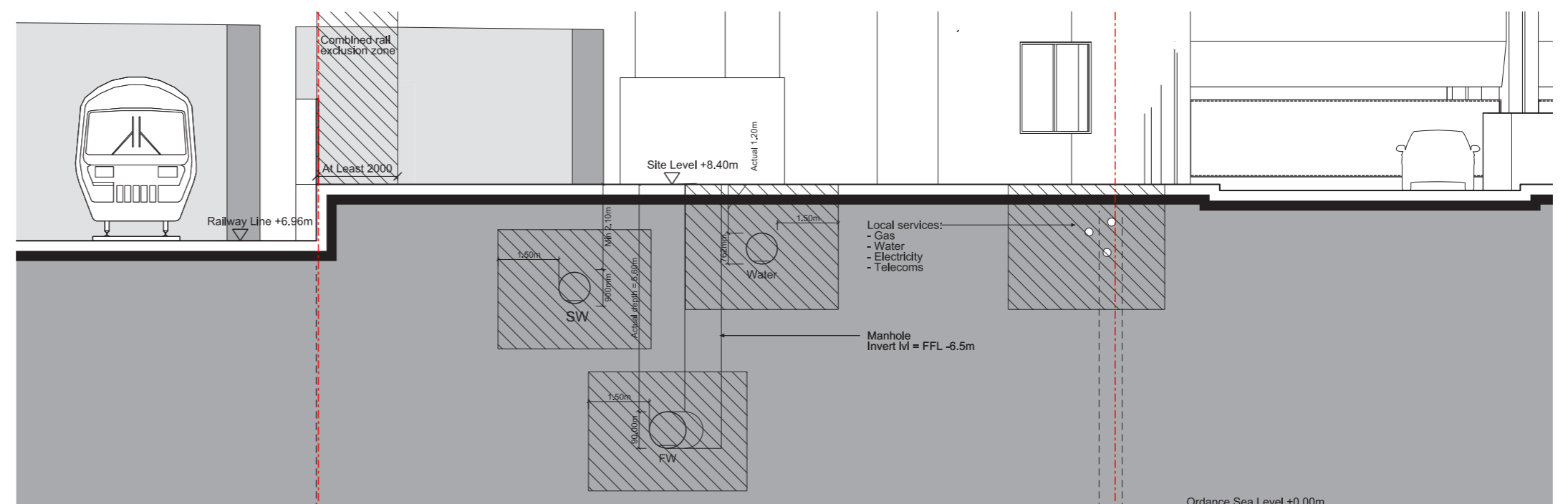
These foul water and surface water drains impose exclusion zones of 1.5m to either side, whilst the mains water has a 5m offset. This restricts where foundations and lift pits can be placed, therefore impacting on the planning and layout of any proposed scheme.

Network Rail requirements dictate that there is a 2m offset from the railway to the edge of the building, whilst there is also a minimum offset of 4.55m from the centre line of the nearest rail to avoid the structure having to be designed for derailment impact.



Existing Underground Utilities Section

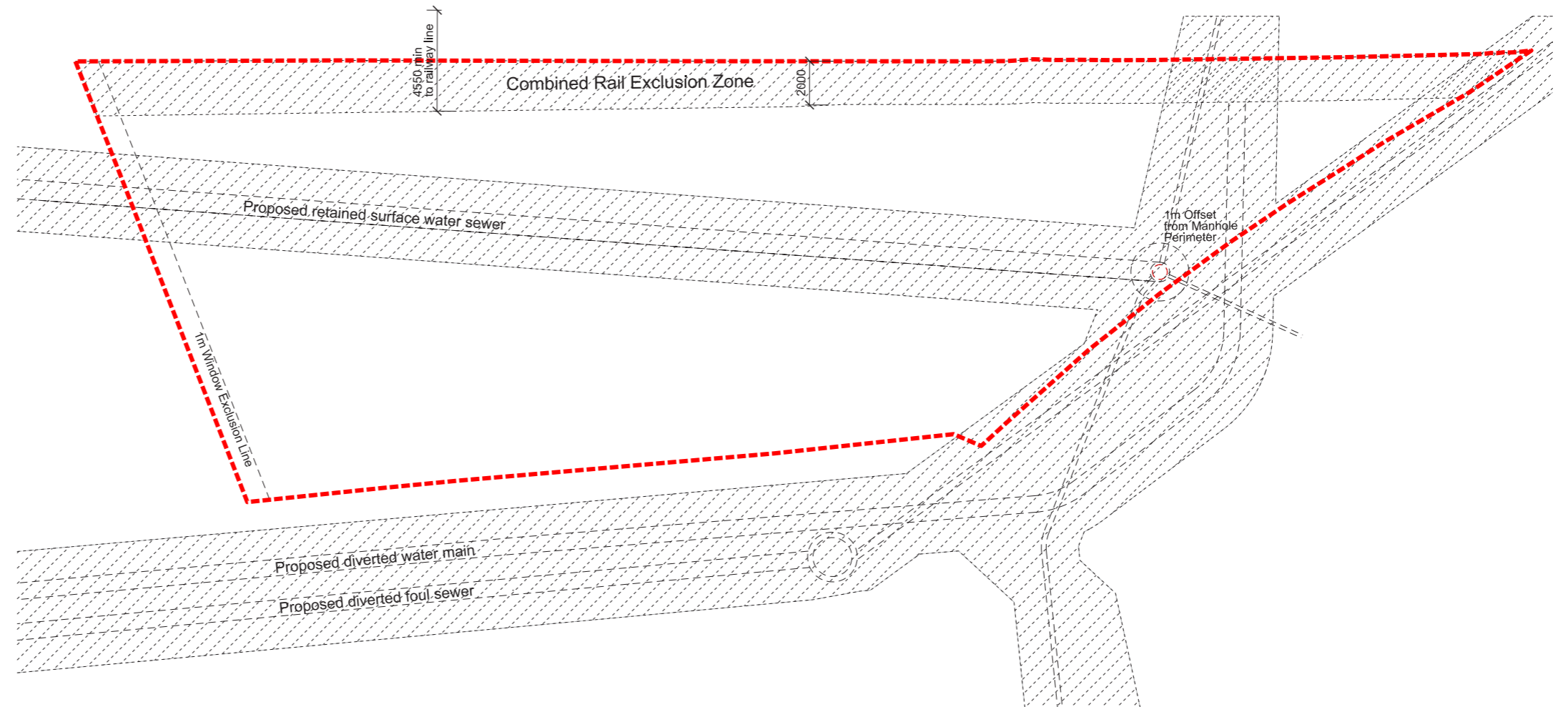
The underground utilities lie at various depths underneath the site. Some of these depths are known, however others are estimated, and require further survey work to ascertain their precise depth.



Proposed Constraints

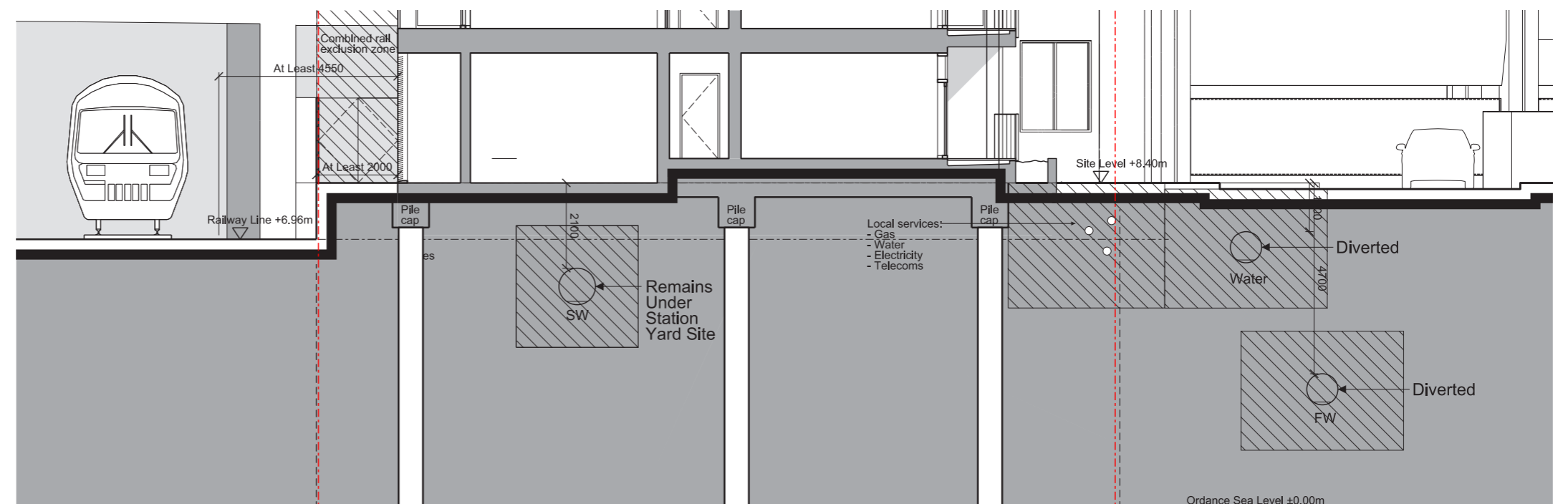
Proposed Underground Utilities Plan

Solum are in negotiation with Thames Water, but it is likely that the surface, foul and mains water sewers will be re-routed. This is better for the location of foundations and removes the need for an internal manhole.



Proposed Underground Utilities Section

Only the surface water sewer is proposed to remain under the site. The mains water and foul water are proposed to be moved outside into the road.

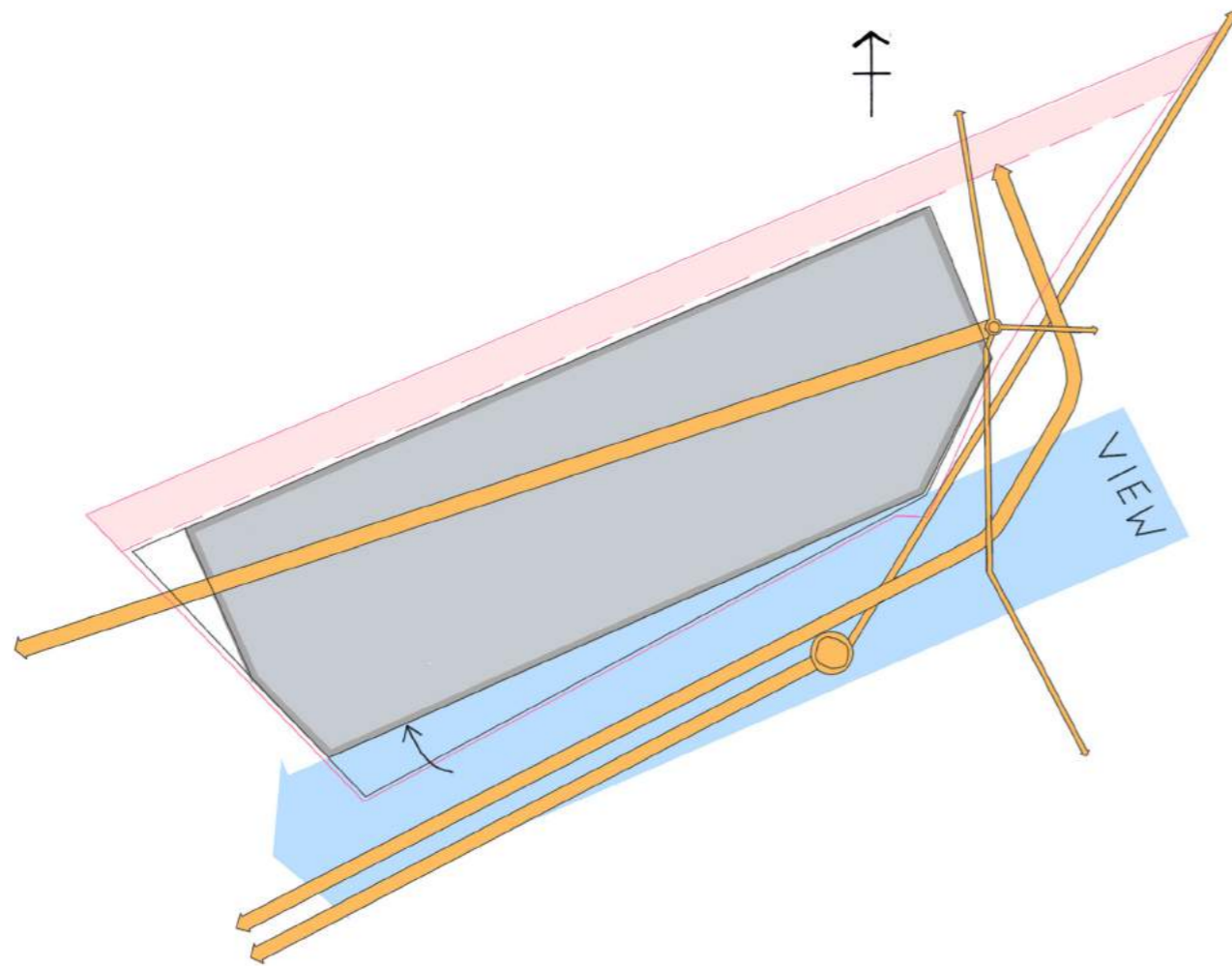


TECHNICAL CONSTRAINTS

Constraints Diagrams

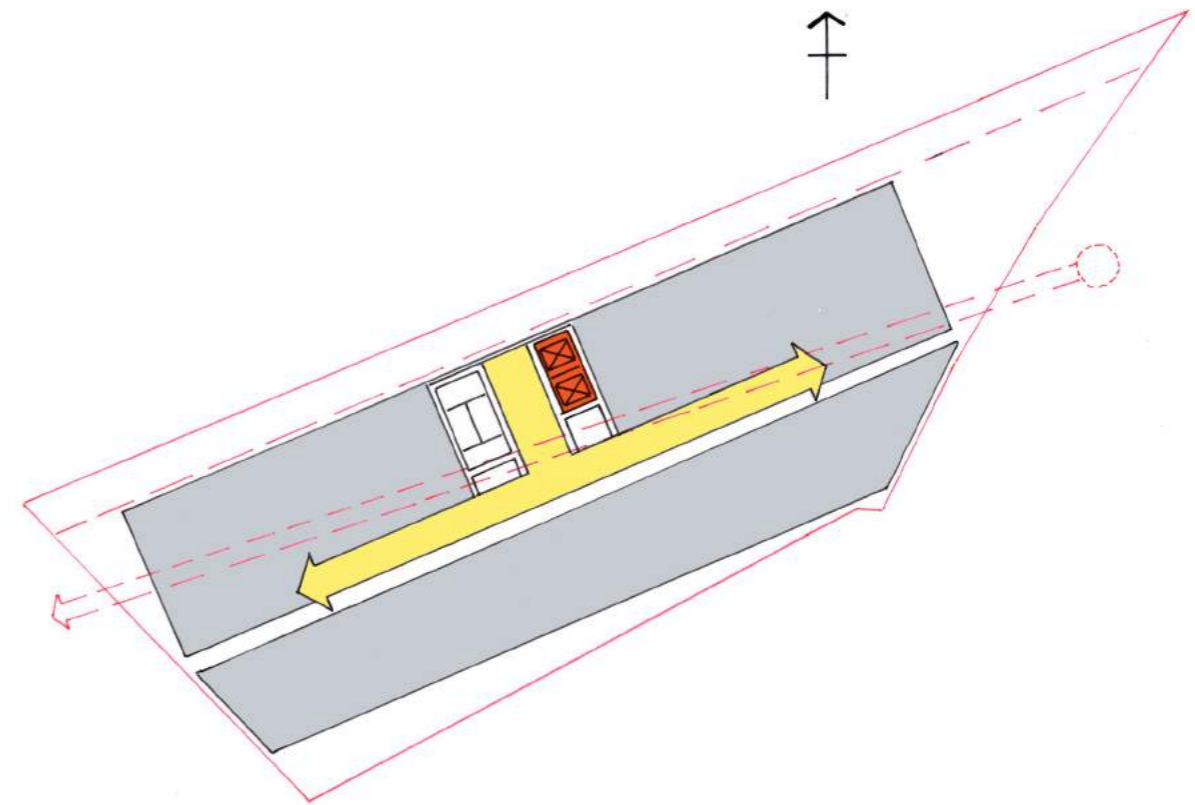
Combined Constraints Diagram

In the below diagram, the blue arrow represents the proposal to set the building back from the street in order to maintain views of The Albany. The pink band at the top shows the 2m offset from the boundary adjacent to the railway, and the orange lines represent underground services. This leaves the grey area as the possible footprint for the proposed building.



Constraints on Core Positioning

This diagram illustrates how the building is restricted to a single central core location due to the underground services limiting the potential for two lift pit locations.



06. PRECEDENTS

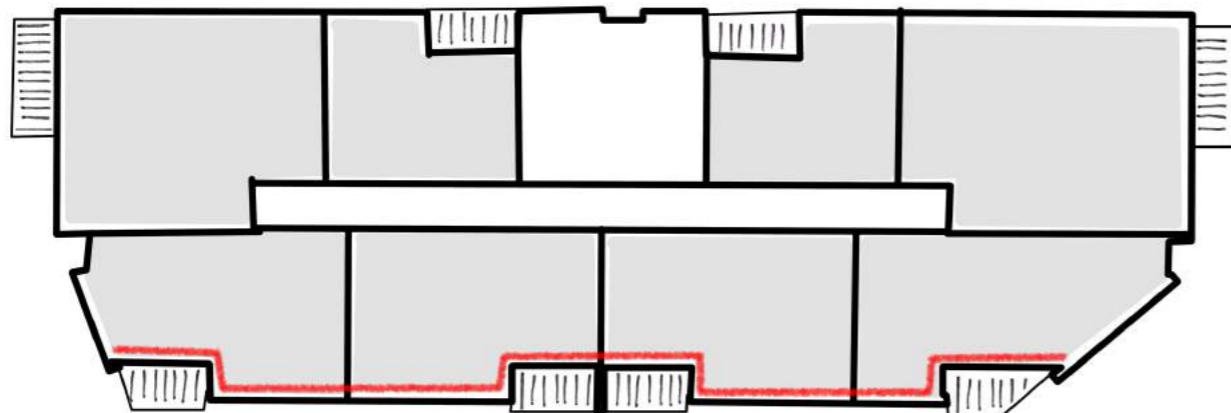
PRECEDENTS

Mansion Blocks

The mansion block, a large and elegant apartment typology typical in London, is a useful precedent for our scheme. There are similarities in scale, rhythm and proportion. Mansion blocks are also typically constructed from red brick, with undulating recessed balconies. They often have mansard roofs or recessed upper floors.

The mansion block is a good example of how a large mass can be broken down by the use of horizontal and vertical elements, different materials, articulated recesses and regular openings. These elements are key to making our scheme visually elegant and modulating the changing scale of buildings on either side of the site.

This diagram shows how the facade steps in and out in a similar manner to a typical mansion block.



Facade Diagram



Albany Mansions, Battersea



Mansion Block in Willoughby Road, Twickenham

Raised Ground Floor

There is precedent in the surrounding area for houses with raised ground floors, often behind a low front wall. This creates privacy from the street, which is vitally important for our scheme due to the proximity of the building to the pavement. This is especially critical on rugby match days, when many people go to the Albany pub along Station Yard.

We are therefore providing a ground floor that is approximately 600mm above street level, behind a low wall with planting at the site boundary.



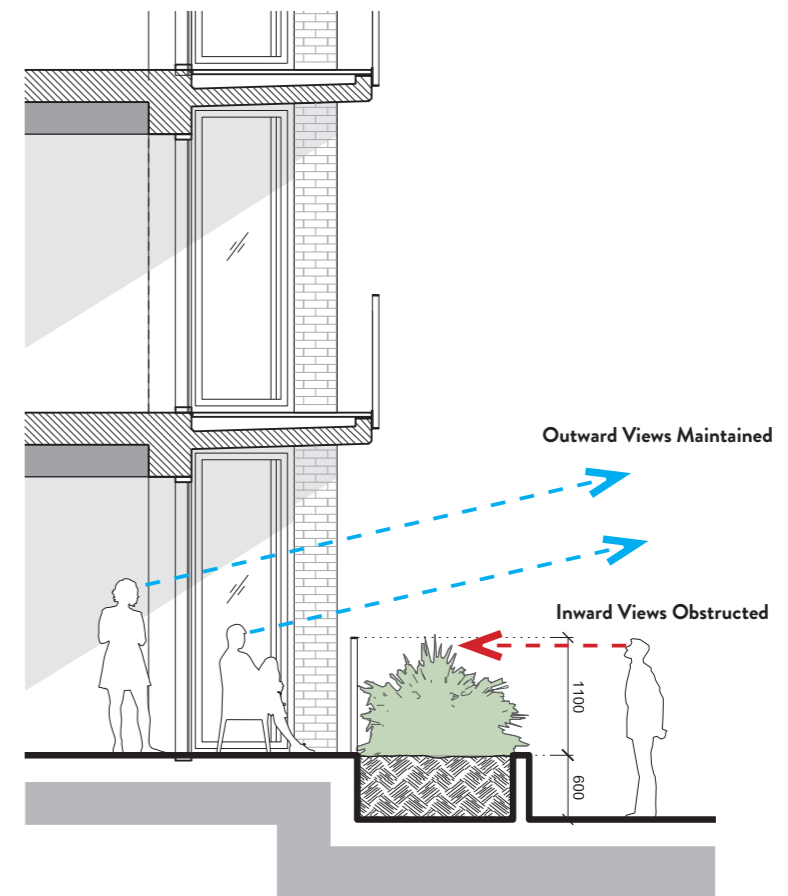
Raised Ground Floor at Queens' Road, Twickenham



Front Boundary Wall at Lion Road, Twickenham



Proposed front elevation - raised ground floor flats, vegetation and low wall



Proposed section - vegetation, low wall and balustrades to maintain privacy

PRECEDENTS

Brickwork and Detailing

Railway architecture is a key feature of the local area. On Brewery Lane there is a 19th Century brick engine shed which is a useful precedent for us. Its use of two tones of brick, regular openings and simple corbelling details adds elegance and articulation to an otherwise simple form.

As recommended by the DRP, the railway heritage has been referenced in our design to tie it in with the local context and create a modern interpretation of an appealing local typology.



Brick Detailing on Brewery Lane



Brick Detailing in the Proposal



Railway Engine Shed, Brewery Lane, Twickenham

07. PLANS

PLANS

Ground Floor Plan

The general plan is arranged with residential units fronting onto the street, whilst services and storage areas are concealed to the rear of the building, creating active frontage to Station Yard & Mary's Terrace.

The main entrance is accessed from Station Yard. The secondary entrance is accessed securely via gates at either end of the 2m wide alley at the rear of the building, and provides easy access to lifts or a secondary means of entry.

The ground floor residential units are in close proximity to the street and the internal finish floor level of the units is proposed to be elevated 600mm above the external street level. In conjunction with a 1100mm high fence on the balconies, designed to prevent inward views, the units are afforded privacy but also unrestricted views out, as per the diagram in the "Raised Ground Floor" Section above.

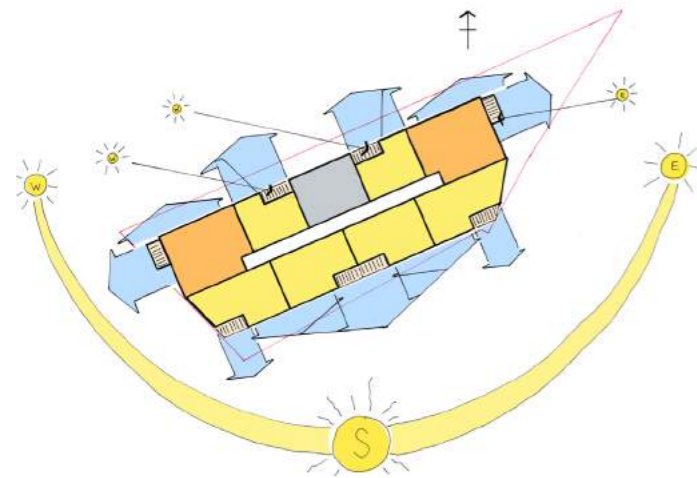


Typical Floor Plan

The typical floor consists of eight flats per floor, accessed via a corridor off the central core. The units have been arranged to maximise natural light with balconies and living rooms enjoying largely southern aspect.

The core has been positioned on the north side of the building to minimise the northern aspect and the units either side have been designed to maximise direct light.

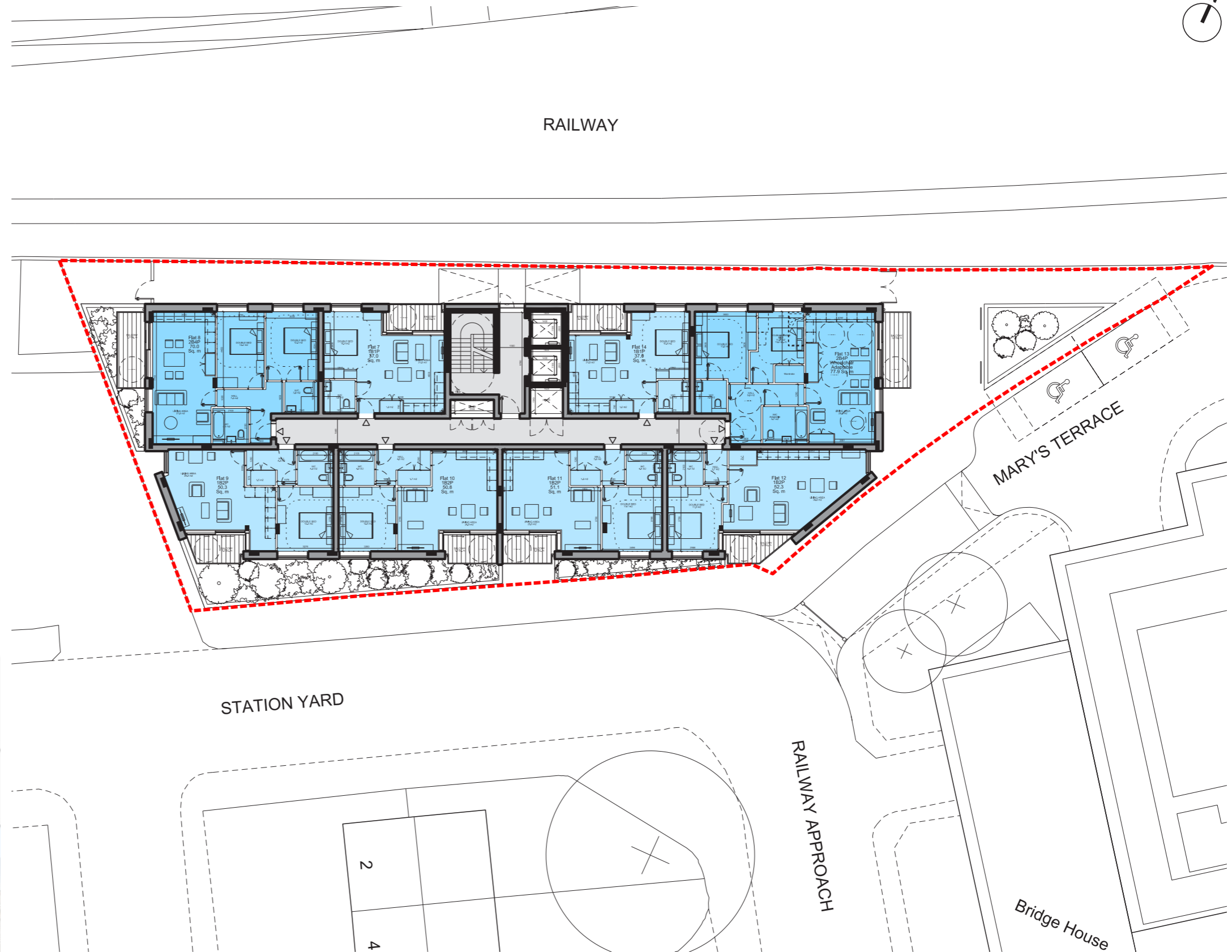
All flats meet Richmond's area requirements for external amenity space. Balconies on the southern facade have been positioned to minimise overlooking, with the central balconies facing the flank wall of the adjacent house.



Units are organised to maximise southern aspect & direct sunlight



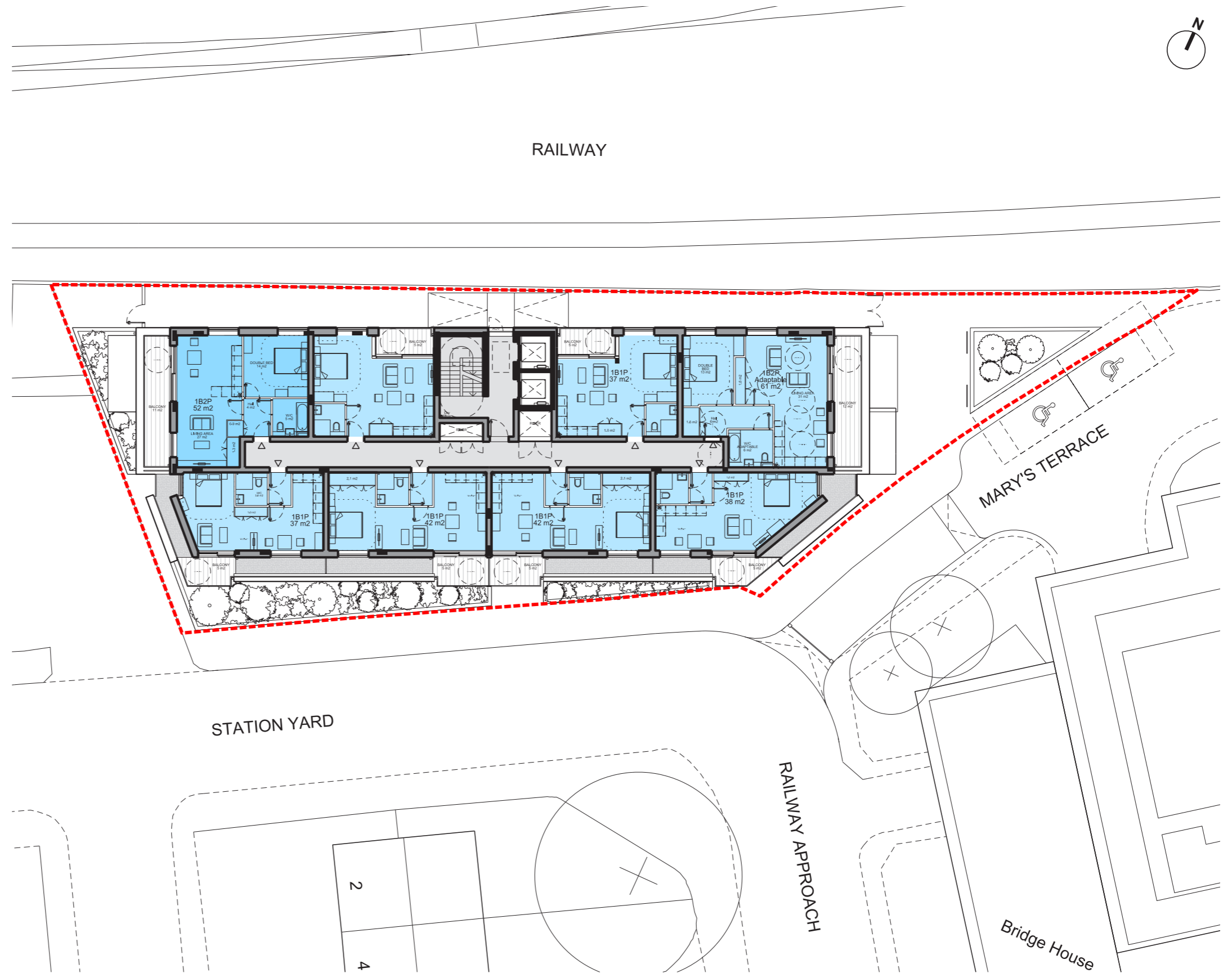
Views from balconies do not overlook properties. Trees visible from Railway Approach



PLANS

Fifth Floor Plan

The fifth floor is formed of eight residential units, with the facade along the east, south and west set back to ensure the upper floor has a reduced visibility from street level and to ensure the mass appears smaller overall. The corner flats have generous balconies.



Roof Plan

The the flat roof is covered with a sedum roof and is accessed via an access hatch. There is an AOV above the stair.

