

08. LANDSCAPE

LANDSCAPE

Landscaping and Streetscape - Illustrative

While the TFL site is not available to be included within our application, we are looking to enhance the public realm around the Solum and TFL land. Although we do not own or control the TFL land and therefore cannot be obligated to deliver them, we have considered longer term landscape proposals which would benefit the area:

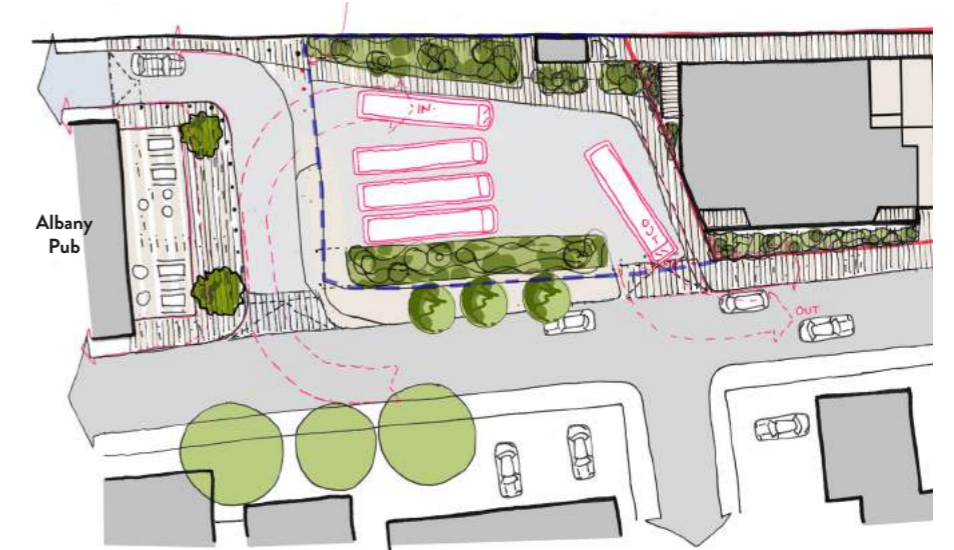
- Re-surfacing and incorporating a landscape design that encourages secondary uses of the site
- Creating connecting pavements between our site and the Albany pub
- Introducing trees, hardier species and planting along Albany side of TFL Land
- Creating planting beds within the Solum site
- Preventing the TFL site being used as cut through and improving pedestrian safety



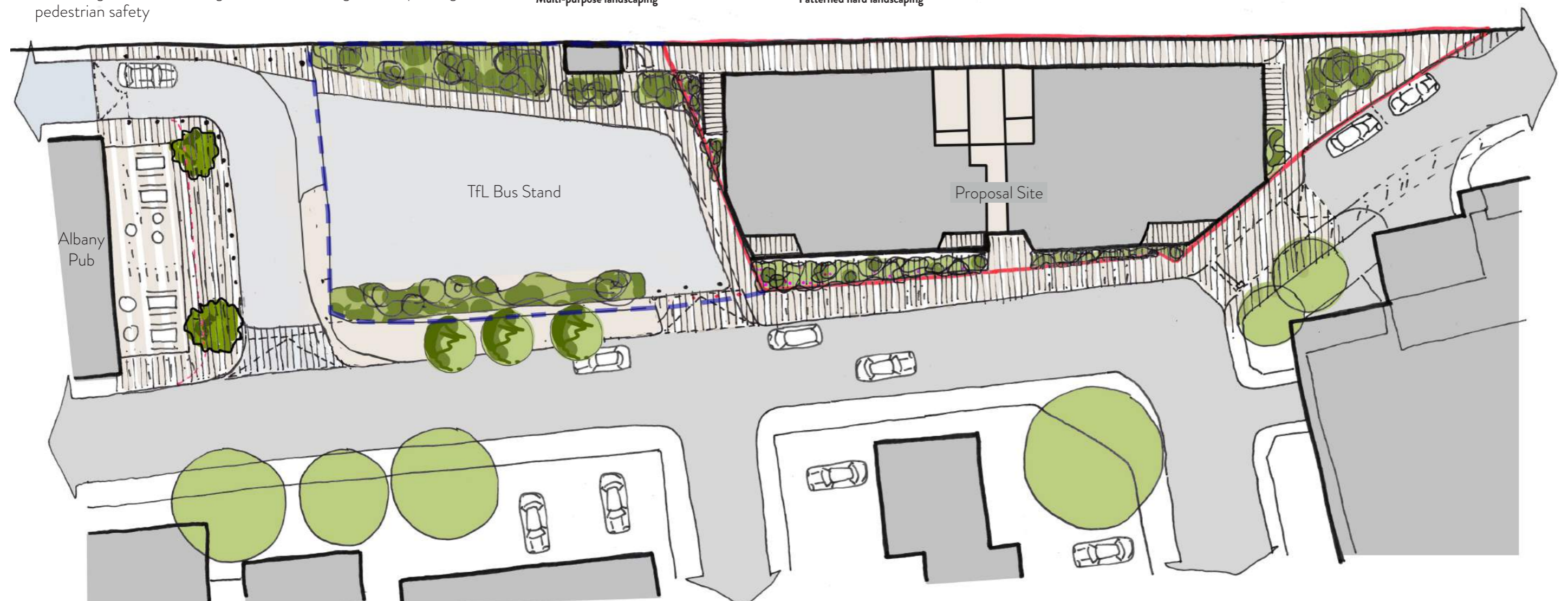
Reference - Chiswick Park
Multi-purpose landscaping

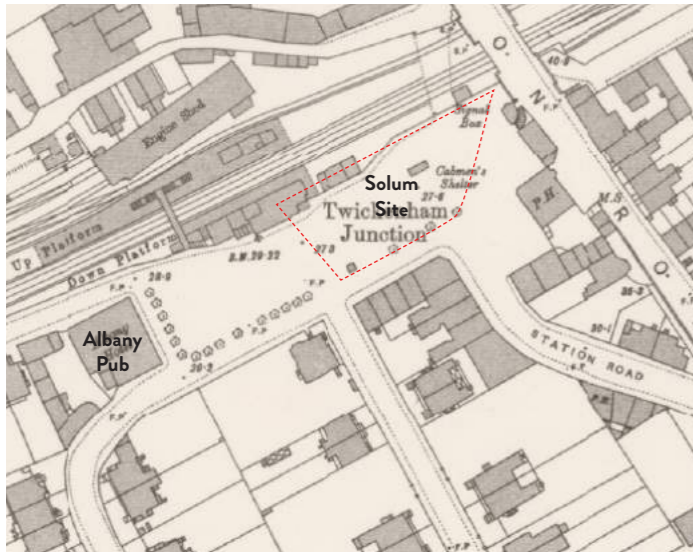


Reference - Exhibition Road
Patterned hard landscaping



TFL operational requirement for 5 buses on TFL Land





Tree Lined Streets around and on 1896 Site

- Existing vegetation contributing to a tree lined street scape
- Proposed vegetation zone, that would provide an reinforce and improve the existing public realm.

Station Yard was originally much more green, with trees all along the boundary of the TFL land. There are still some large trees on the other side of the street from our site.

Additional vegetation at ground level is proposed, addressing the street and primary approach. This will be an improvement upon the existing car park outlook and will complement the existing trees in the streetscape.

The proposal sets back from Station Yard towards the western end of the site, creating a more generous streetscape with the opportunity for more planting, and framing views of The Albany.



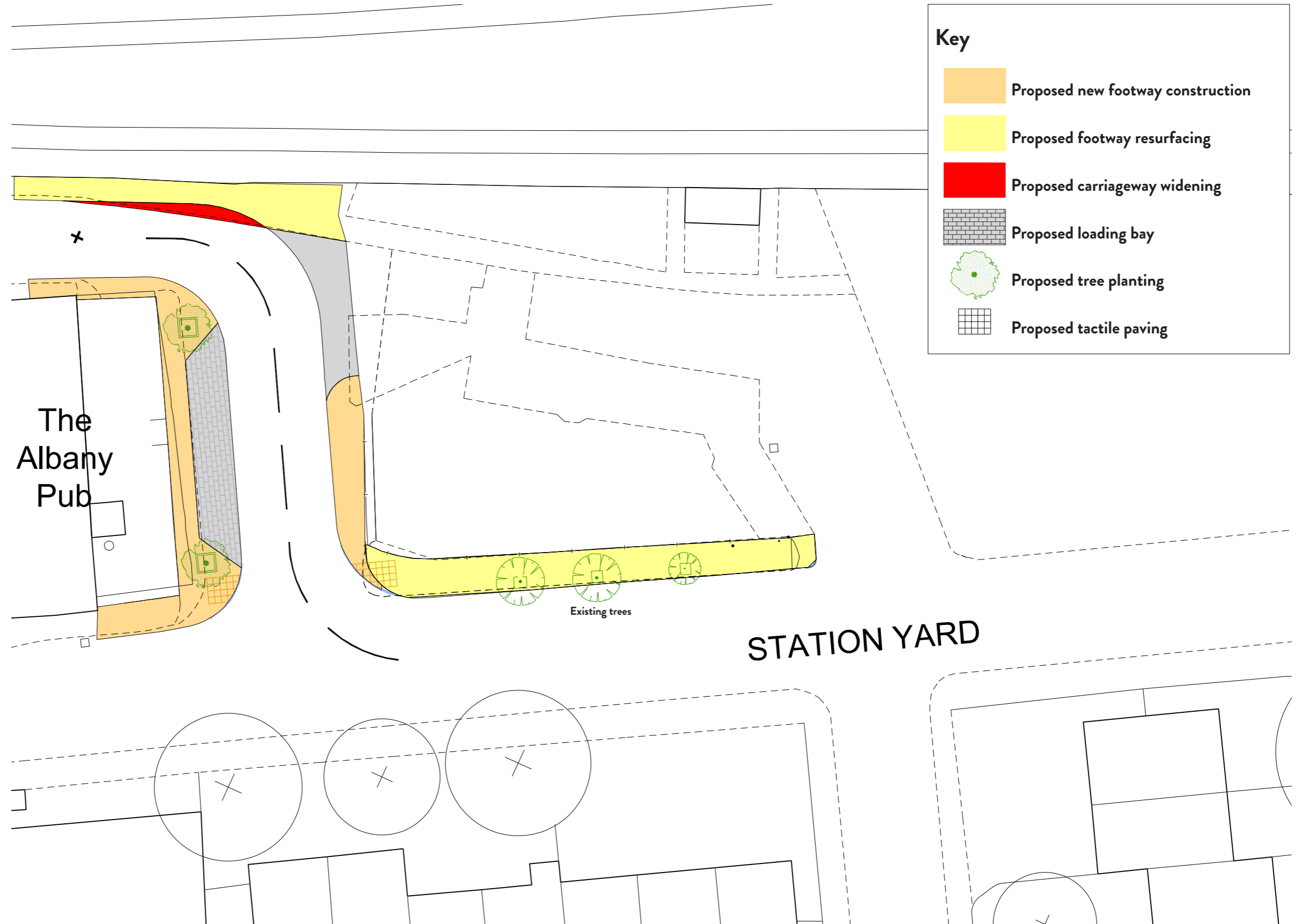
LANDSCAPE

Richmond Council Public Realm Improvements

Richmond Council is undertaking improvements to the public realm around the Albany Pub and Station Yard area.

This will involve improving the road that runs around the Albany, smoothing the carriageway route, adding a loading bay and widening the footway for clients of the pub. The proposal is also to plant 2 new trees outside the pub and add tactile paving.

These improvements will tie in well with our scheme, enabling better access across the whole area.



Landscape Masterplan by BDP

1. Proposed carriageway widening as part of LB Richmond upon Thames proposals
2. Proposed 4m wide footway build out to provide continuous new footway and loading bay as part of LB Richmond upon Thames proposals
3. New trees as part of LB Richmond upon Thames proposals
4. New trees proposed in existing position
5. Improved footway resurfacing as part of LB Richmond upon Thames proposals
6. Secure access to back of house area
7. Perimeter buffer planting
8. Granite 'doormat' to main entrance
9. Shrub planting
10. Disabled car park spaces
11. Raised table threshold and retained vehicle barrier



LANDSCAPE

View from Queen's Road



09. ELEVATIONS

ELEVATIONS

South and North Elevations

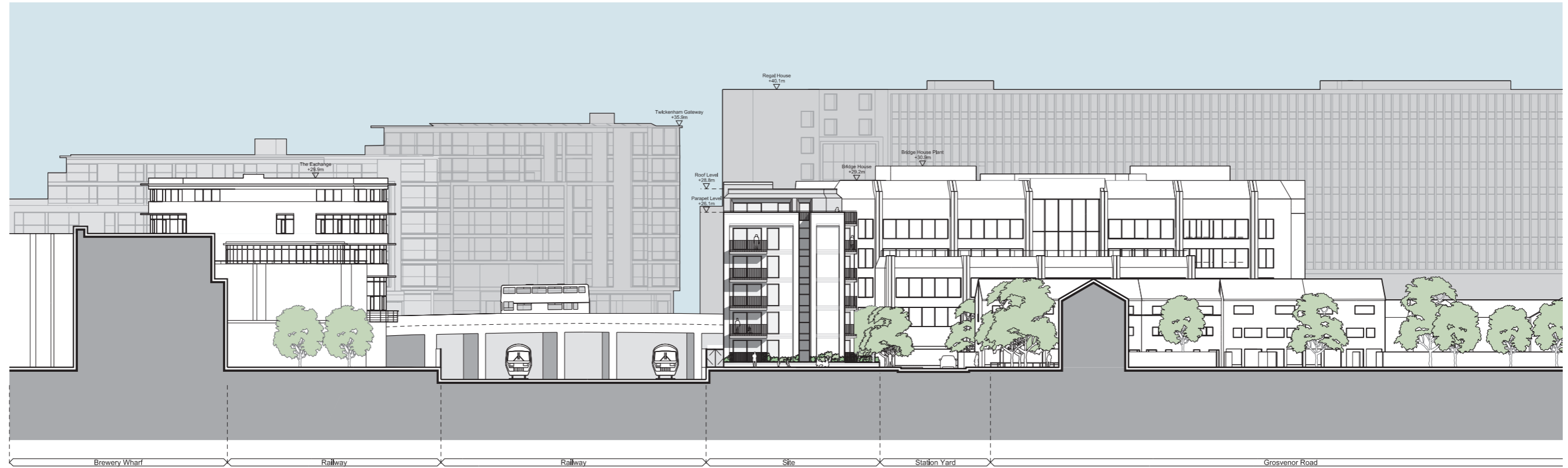


South Elevation *Note that signage, name of development and other branding decisions are to be confirmed later

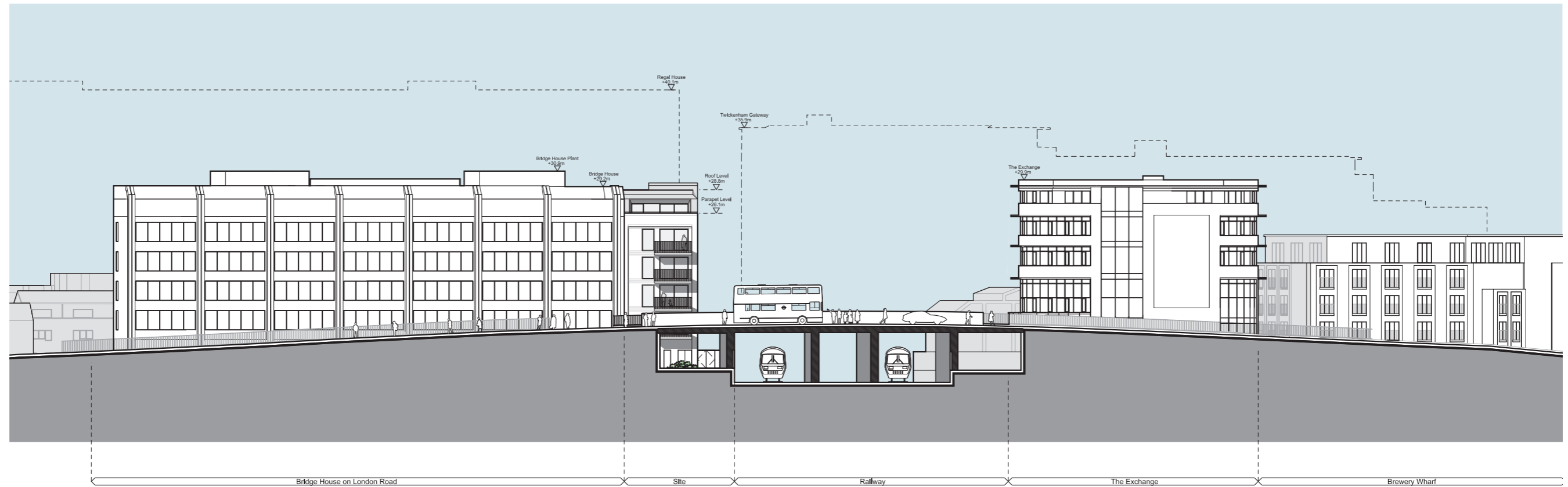


North Elevation

West and East Elevations



West Elevation

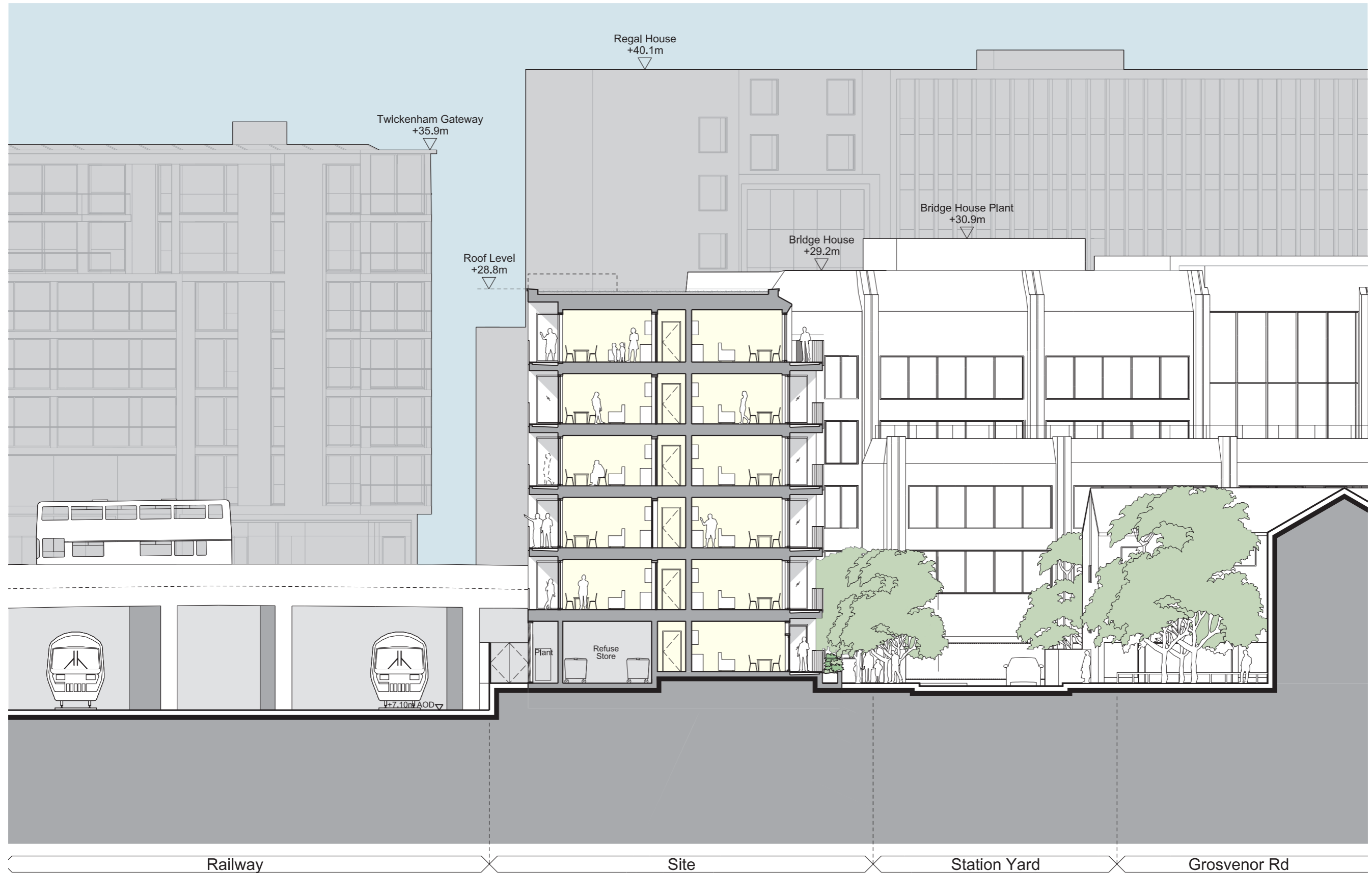


East Elevation

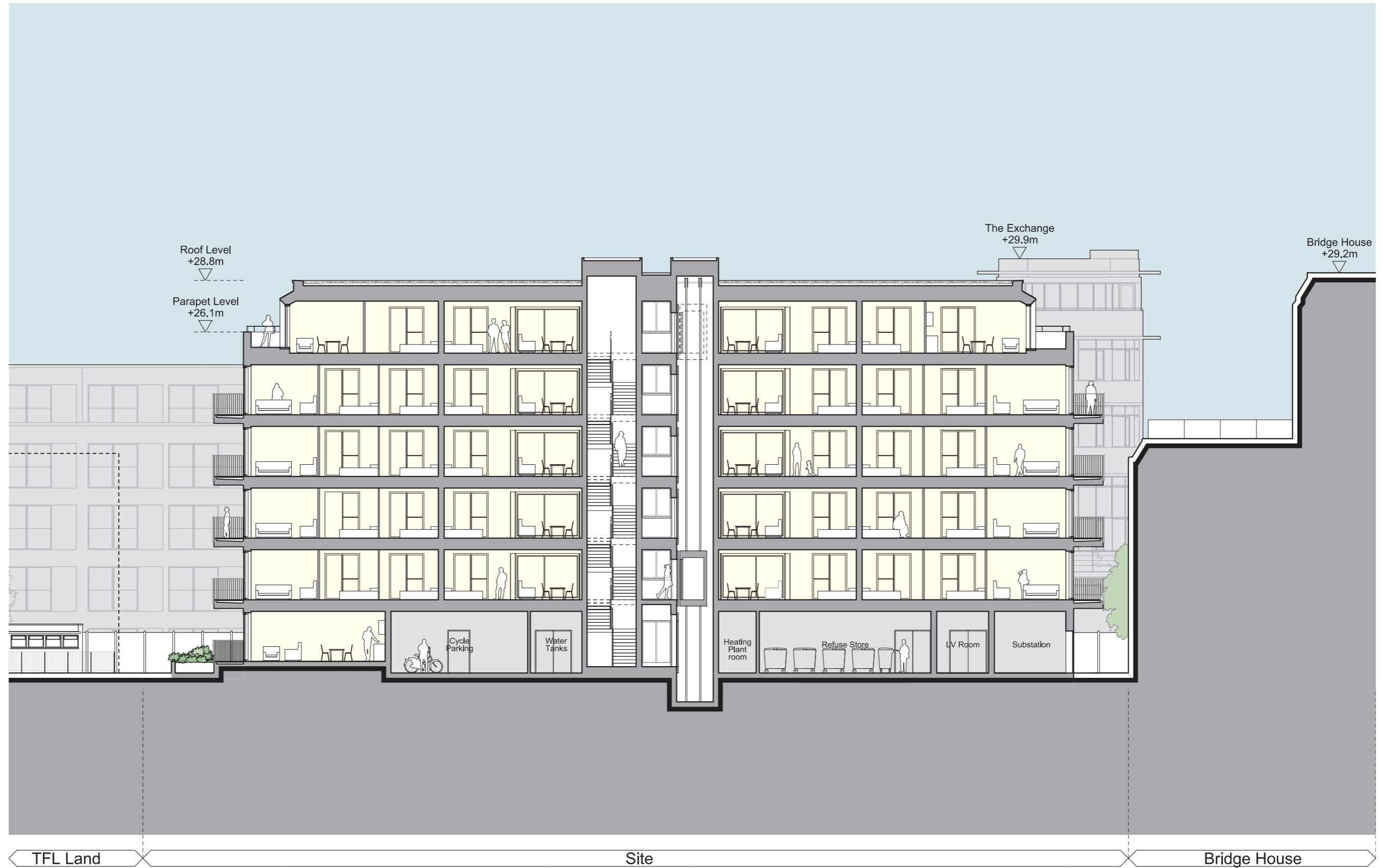
10. SECTIONS

SECTIONS

Short Section



Long Section



11. CONTEXT VIEWS

CONTEXT VIEWS

View 1 from London Road

The following pages show a comparison of photos of the existing site alongside illustrative views of the proposal in its context. These images show that the proposal sits comfortably in the local area and is proportional in scale to the surrounding buildings.

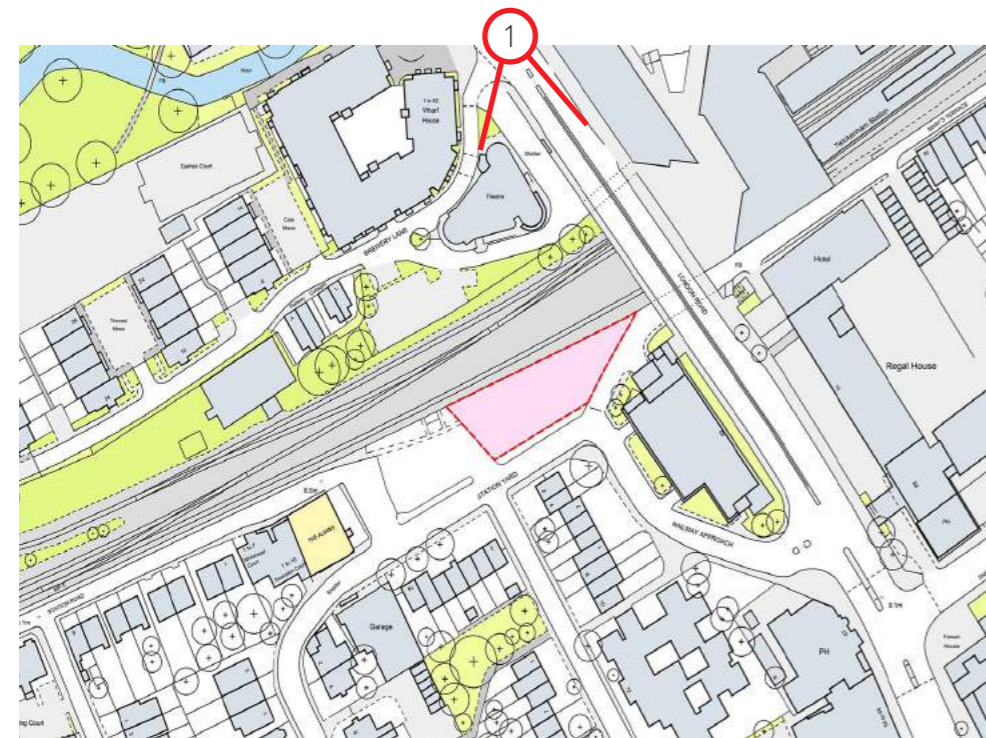


Photo as existing



View with proposal

View 2 from London Road



Photo as existing



View with proposal

CONTEXT VIEWS

View 3 from Railway Approach

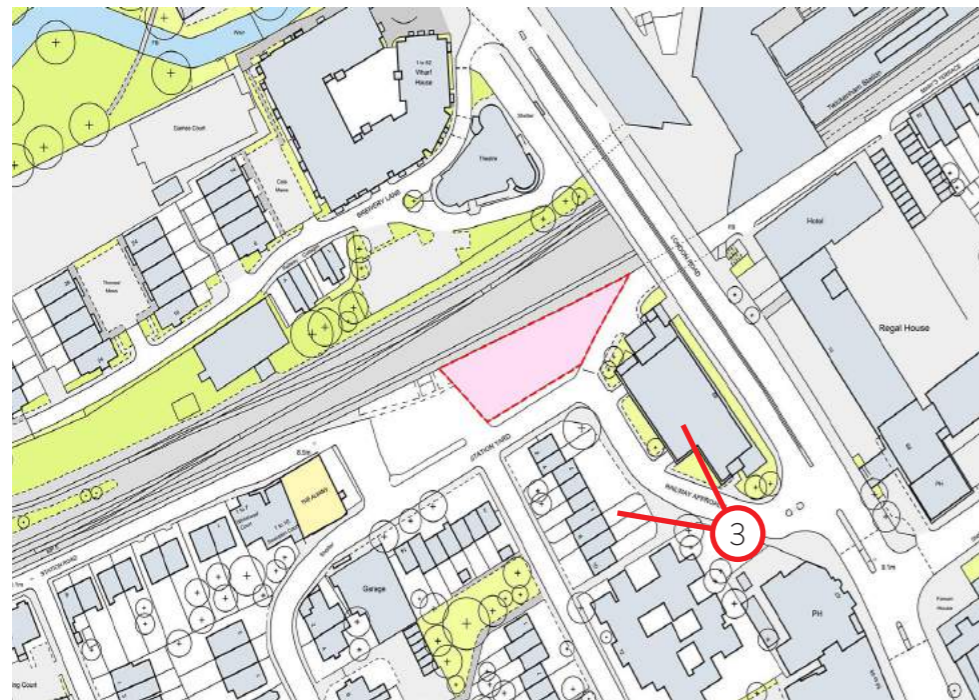


Photo as existing



View with proposal

View 4 from Queen's Road

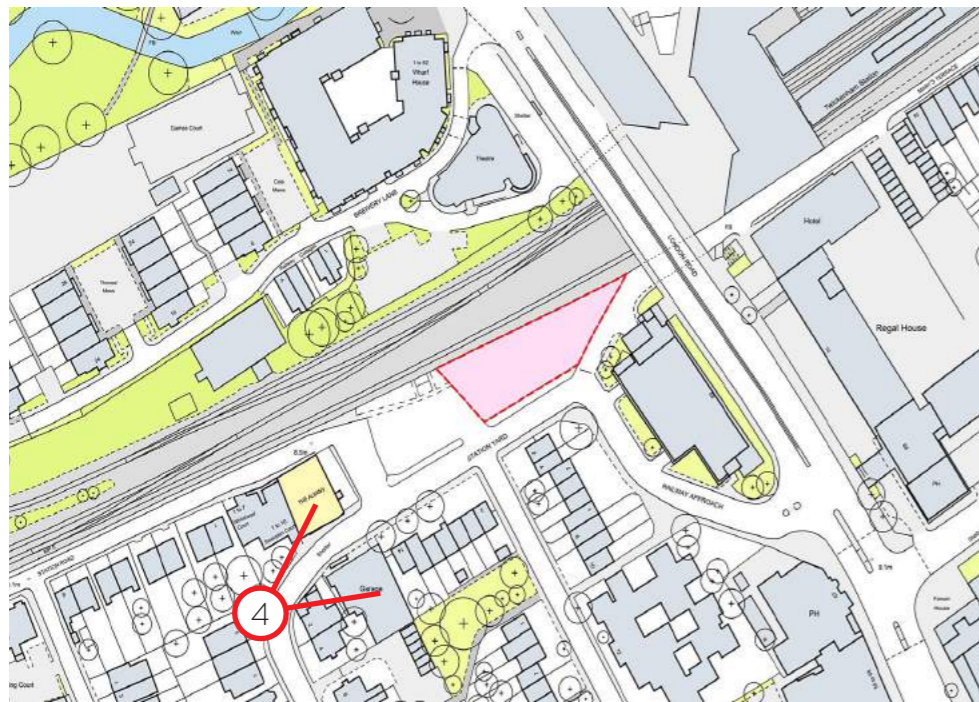


Photo as existing



View with proposal

CONTEXT VIEWS

View 5 from Station Road



Photo as existing

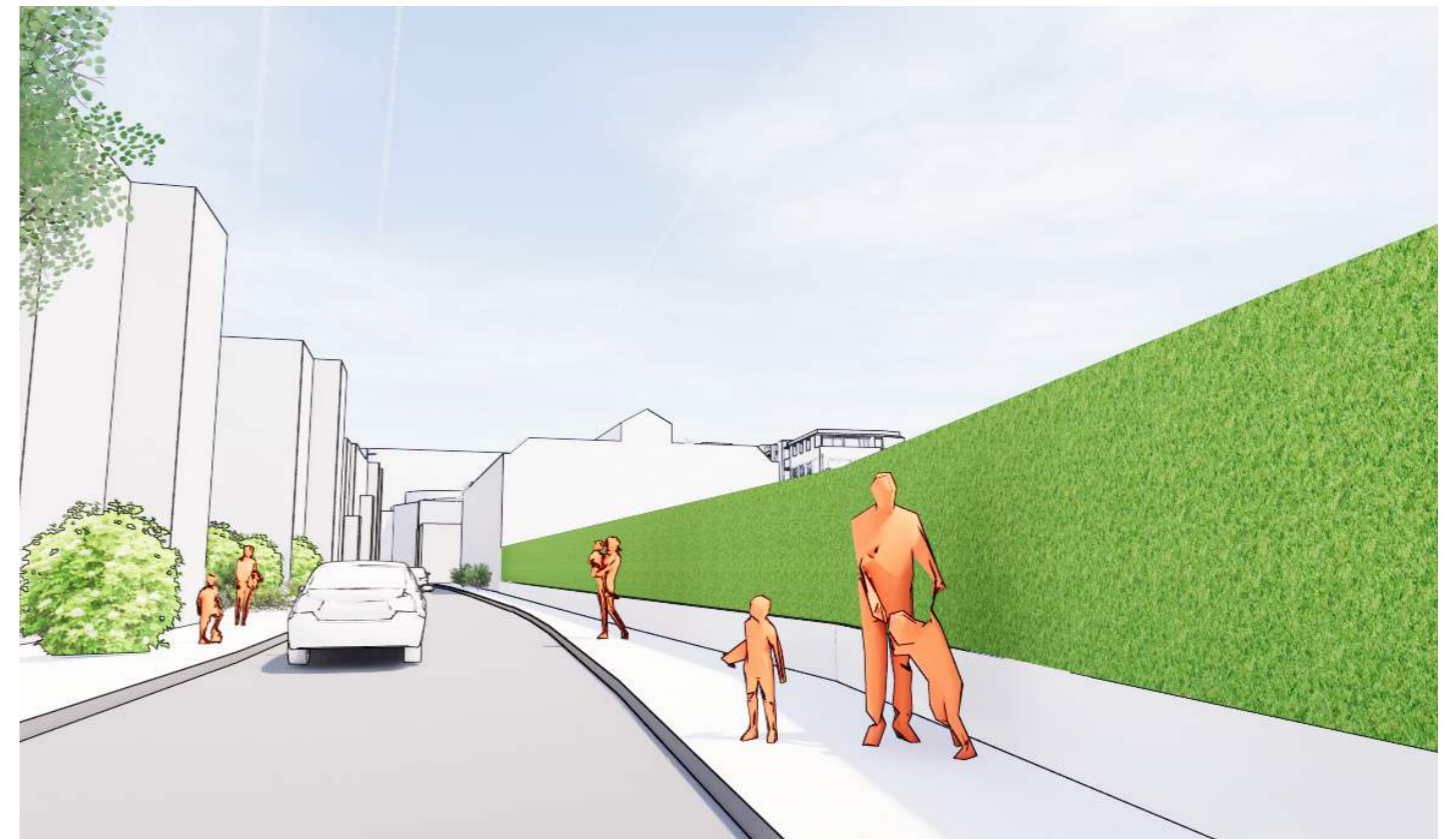


View with proposal

View 6 from Brewery Lane



Photo as existing



View with proposal

CONTEXT VIEWS

View 7 from London Road

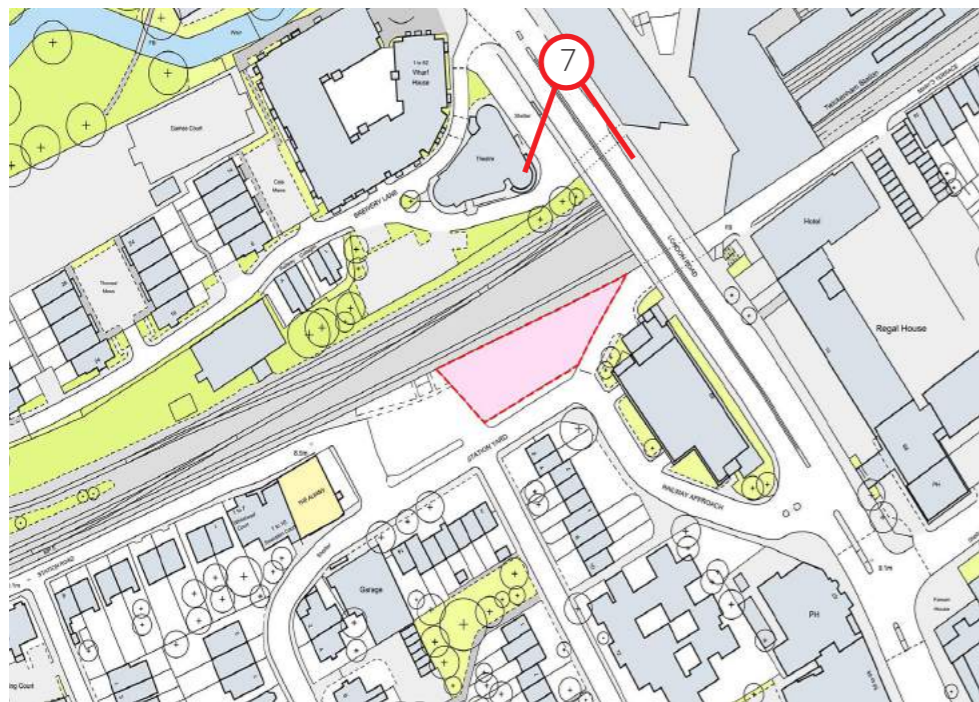


Photo as existing



View with proposal

12. VERIFIED VIEWS

VERIFIED VIEWS

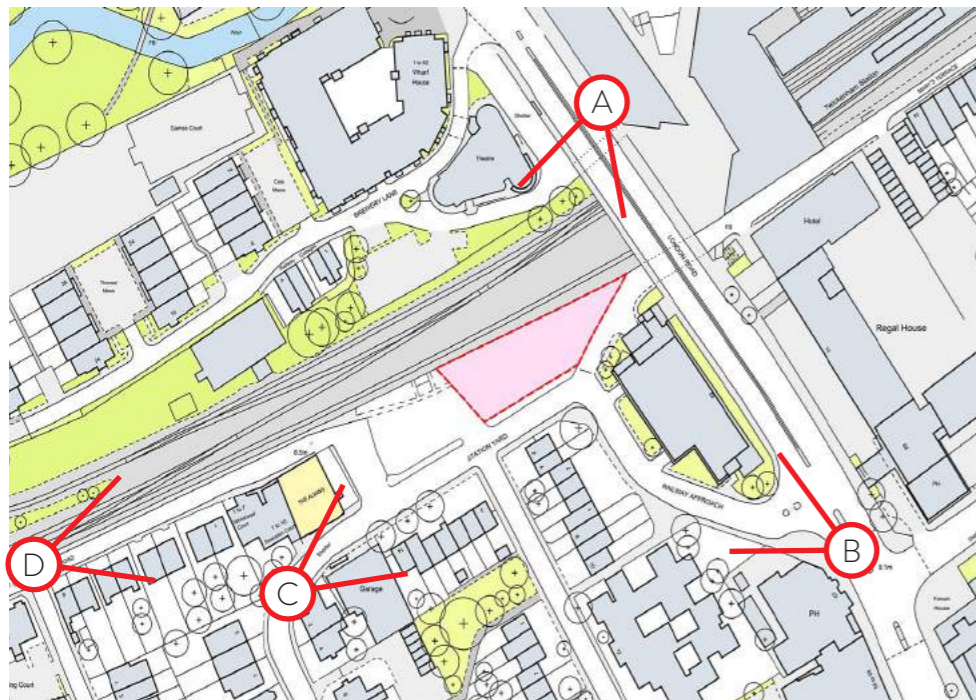
View A & B



View A from London Bridge



View B from London Road



View C & D



View C from Queen's Road



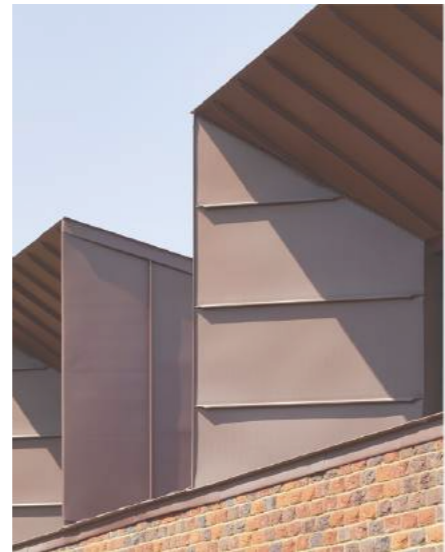
View D from Station Road

13. MATERIALITY

MATERIALITY

Facade Materials

We want to ensure the character and design of our proposal complements and enhances the surrounding area and adjacent buildings. Therefore the proposed building will relate well to its surroundings, including Bridge House to the east, the Albany pub to the west and the existing housing on Station Yard and Railway Approach to the south. The Albany pub is a building of local significance and we are keen that our proposals complement it and deliver enhancements to its immediate environment and public realm. We are considering a palette of materials that references local buildings, including other residential developments and railway architecture.



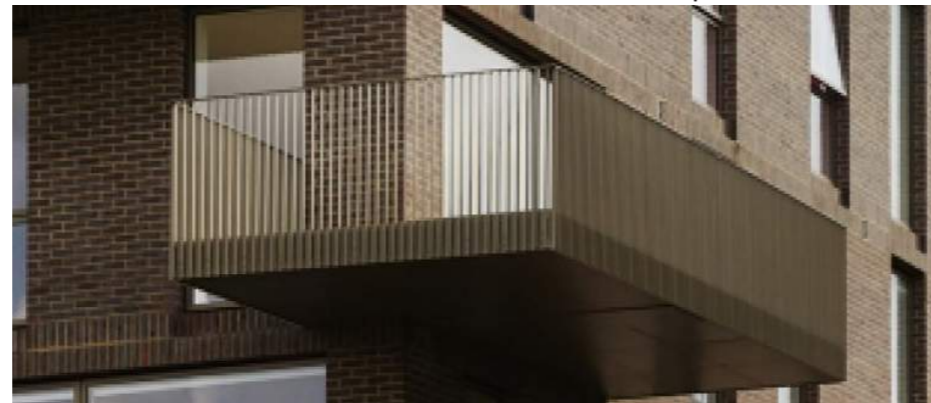
Metal cladding



Metal windows



Brick detail & material precedent



Flat bar balustrades



South Elevation Detail Bay - *Note that signage, name of development and other branding decisions are to be confirmed later

Detail Bays - North and West



North Elevation Detail Bay

West Elevation Detail Bay

MATERIALITY

Elevational Materials



Proposed North Elevation

Proposed East Elevation



Proposed South Elevation

*Note that signage, name of development and other branding decisions are to be confirmed later

Proposed West Elevation

View from Railway Approach looking towards the Albany



*Note that signage, name of development and other branding decisions are to be confirmed later

14. DESIGN REVIEW PANEL

DESIGN REVIEW PANEL

Summary of DRP Suggested Enhancements

The council ran a Design Review Panel for the scheme on the 12 July 2019. The following pages present a summary of the feedback received and the responses which followed.

Overall, the panel was positive about the prospect of the development of an under-used, centrally located brownfield site, and applauded the proposals as high-quality and a significant improvement to the current situation. In terms of massing, the panel agreed that the proposed height is a bridge between Bridge House, Regal House and Twickenham Station and the lower scale and density residential buildings to the south and west. They were, therefore, comfortable with the height and the way the building creates a transition. The panel encouraged the design team to draw on the railway heritage more.

Key comments:

1. All Flats To Have Level Access From Street Level

It was recommended that the design team explore ways to make all ground floor flats accessible/Part M compliant, and preclude the need for future occupants to retrofit individual stair lifts by introducing ramps that provide level access to ground floor accommodation.

2. Consider Utilising The Roof As A Residents Garden

Given the restricted amount of green space around the building, the Team was encouraged to explore the introduction of green roofs. The rooftop offers opportunity to provide green amenity/garden space for the residents as well as forming a pleasant outlook from surrounding buildings.

3. Provide A Quality Public Realm Proposal

The Panel wanted the new square to be a quality piece of public realm and supported the intention to involve an experienced Landscape Architect in its design. They also encouraged the Team to explore the opportunity to incorporate an art strategy into the proposals; this could also reference the transport heritage of the site.

4. Environmental Strategy

The panel encouraged the design team to consider the environmental strategy as part of the planning application. They were supportive of the consideration of air source heat pumps and roof solar panels as a way to make the building more sustainable.



The boards presented at the DRP

1. Level Access Provision

Previously, the ground floor flats were accessed via steps. In response to the DRP comments, a ramp system has been incorporated within the building circulation that is Part M compliant, rises up 600mm to the FFL of the ground level flats and removes any restrictions on access.

Entrances

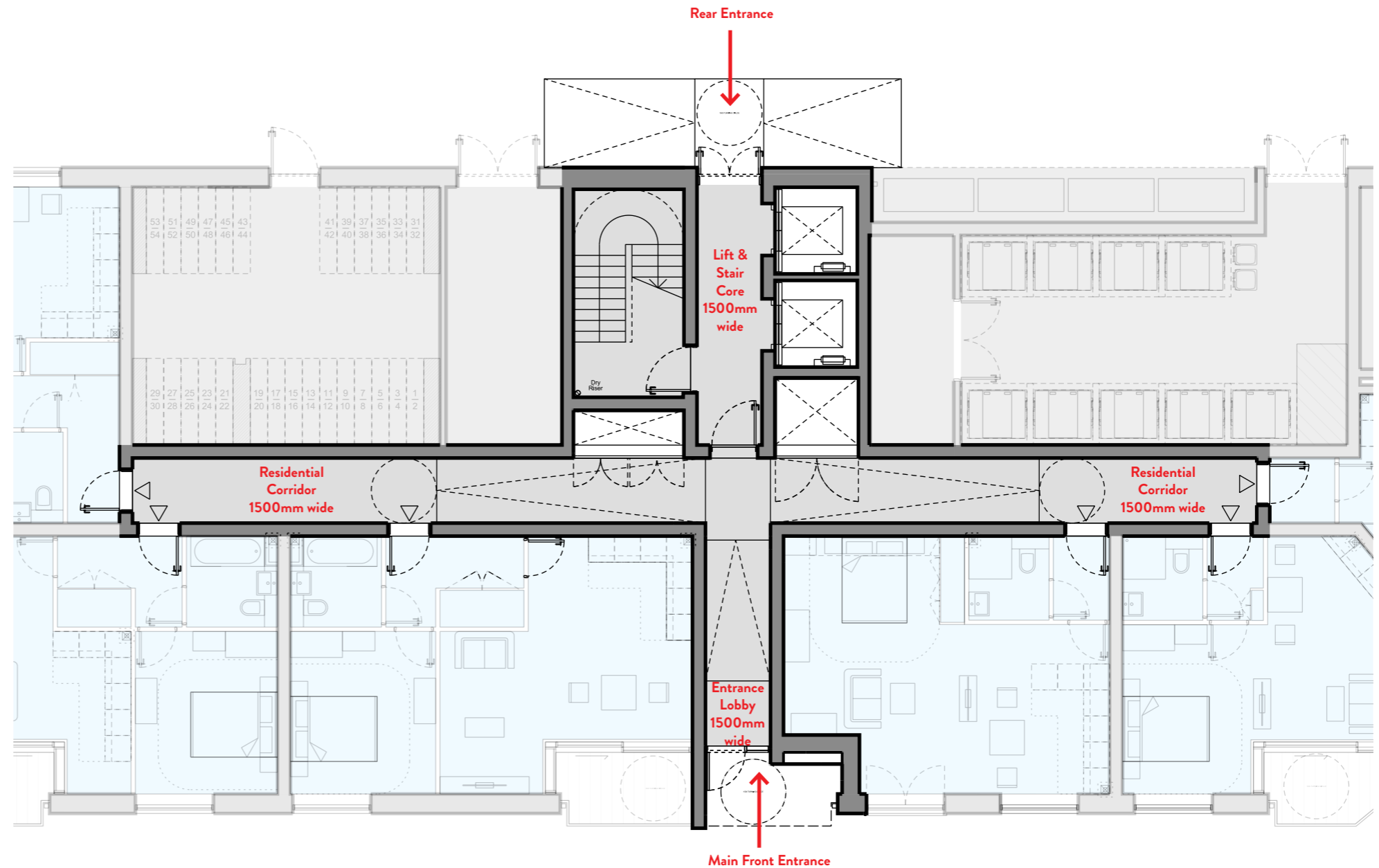
There is one main entrance for the residential units at the front (south) of the building, which opens into the lobby and corridor. There is also a rear entrance which opens straight into the stair and lift core and which is accessed from a ramp along the rear (north) of the site. Both entrances are level from the street, with ramped access rather than steps.

Internal Circulation

The main entrance lobby, the stair core and all corridors are 1.5m wide. This allows a wheelchair to turn.

Vertical Circulation

The stair and lift core houses a fire escape stair and two lifts for normal use. The lifts are for 8 people, with car dimensions of approximately 1400 x 1100mm.



Ground Floor Plan Showing Entrances, Internal Circulation and Vertical Circulation

DESIGN REVIEW PANEL

2. Roof Level Plan

Roof Garden Study

Following on from the DRP suggestion that the roof could be used as accessible garden space, we explored the potential for a roof garden.

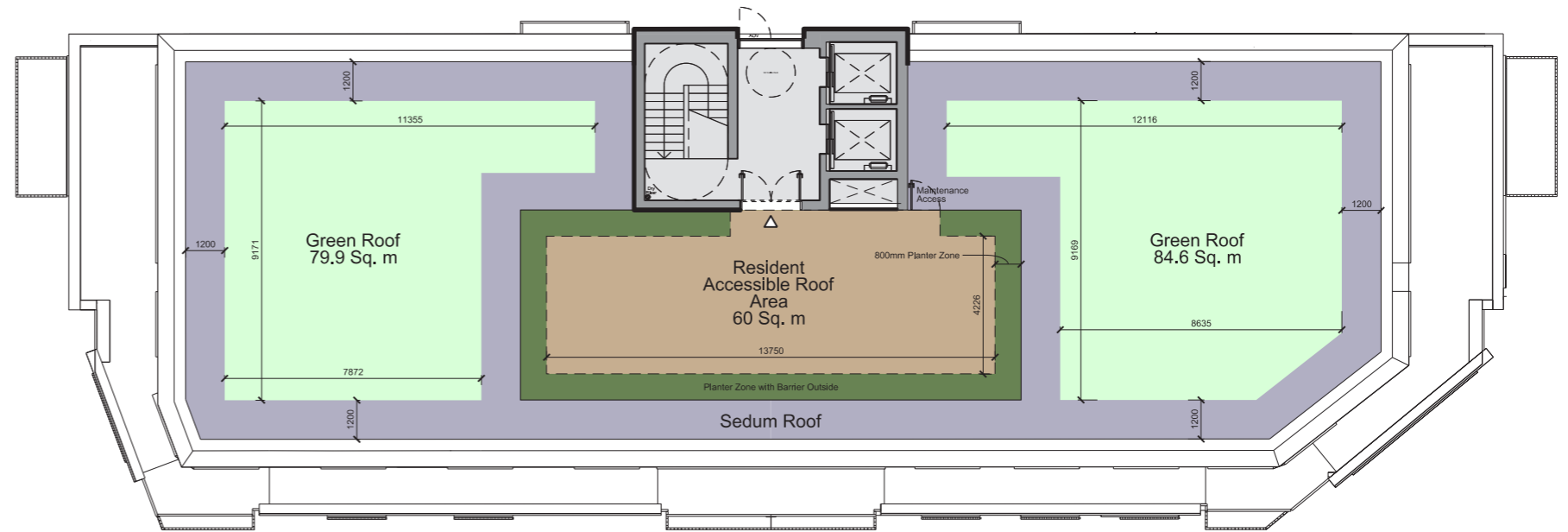
The roof garden is limited to 60 Sq. m or 1 Sq. m per person for a maximum 60 people, to comply with fire regulations.

The adjacent roof plan sketch indicates a decked area enclosed within planters with handrails or glass barriers to provide the 1100mm required. Access to the roof for maintenance is via a gate in the roof garden barrier.

Roof Garden Conclusion

A massing model showing the lifts and stairs that would be required to access the roof level garden is indicated below.

Our opinion is that the proposal contradicts a Pre-app requirement that the proposals should be lower than Bridge House. The access requirements result in a lift and stair core that is increased in height and makes the building visibly taller than the adjacent Bridge House.



3. Public Realm

Public Realm Improvements

Our scheme includes planting to much of the site, to soften the building and improve the local environment. It also increases privacy for the residents on ground floor level, creating a buffer from the street.

Permeable paving is proposed for the rest of the site as part of the SUDS strategy. Part of Mary's Terrace is outside the site but within the ownership of the client, therefore improvements to this area are proposed, including the 2 new accessible parking spaces.

TFL Land

Subject to agreement with TFL who own the adjacent site, we would like to make improvements to their site. This could include planting of shrubs and trees and improvements to the hard surfaces.

All of these proposals fit well with Richmond's scheme to make public realm improvements outside the Albany Pub.

4. Environmental Strategy

Integration UK have been consulted to provide an environmental strategy for the scheme. Refer to the section on Services.

15. PUBLIC CONSULTATION

PUBLIC CONSULTATION

Summary of Public Consultation Feedback

A Public Consultation was held on the 16 July 2019 and 96 people attended.

Summary from Portland Communications

- As of Monday 22 July, we have received **49** responses. Not all respondents provided answers to each of the questions asked. 1 was submitted via email, 2 were submitted by post and 46 were submitted at the consultation event.
- Based on a review of all the questions – including the free form – the main concerns raised so far have centred around the potential for **increased traffic** and difficulty **parking**.
- Top priorities are: ensuring the design is **sustainable** and **in-keeping** with the surrounding area and ensuring **affordable** housing is made available as part of the scheme.
- There has been positive support for **public realm improvements**, the most popular of which are commitments to plant more trees and improve pavements around The Albany.
- Support for this scheme is far from negative, with the vast **majority** of respondents either **supportive of the plans or neutral** to them. Out of 48 people who responded to the question on how supportive they were of the scheme, 19 were either strongly supportive or supportive and 15 were neutral.

The main **concerns** highlighted were:

- The potential for a reduction in **parking** options which are already limited
- An increase in road **congestion**
- The **height** of the development

Residents were **positive** about:

- Improving** the area, which is in need of **development**
- The potential for **affordable** housing
- Car-free** development plans
- Public realm improvements**, particularly tree planting



Photograph of the Public Consultation on 16 July 2019 in Twickenham

WELCOME TO OUR CONSULTATION

STATION YARD DEVELOPMENT, TWICKENHAM

About the Proposals
This consultation is designed to provide you with information about Solum's proposals to deliver a residential development at Twickenham Station Yard.

Before leaving today, please fill out one of our feedback forms. These give us the opportunity to consider your thoughts prior to the submission of a planning application.

About the Developer
Solum is a partnership formed between Network Rail and Kier to attract private investment into the rail network and build much needed new homes.

About the Architect
Wimshurst Pelleriti is a dynamic, design focused architecture studio based in London, striving to deliver outstanding designs that exceed clients' expectations.

About the Planning Consultant
Union4 Planning is an independent planning consultancy that advises on development, regeneration and place making.

Additional Team Members

- Quantity Surveyor - Gardner & Theobald
- Civil & Structural - Pell Frischmann
- Services - Integration UK
- Transport - Mayer Brown
- Rights of Light - Paragon Building Consultancy
- Landscape - BDP

Concept view from London Road looking north towards the proposed Central with The Exchange on the right and Bridge House on the left.

STATION YARD DEVELOPMENT - OVERVIEW OF PROPOSALS

The current proposals are likely to deliver:

- Around 44 new homes - including new affordable homes.
- Financial contributions through CIL towards local education provision, sports and recreation, town centre and local infrastructure.
- Pedestrian, cycle and public realm improvements leading to Mary's Terrace and Station Road, working alongside the potential improvements to the land in front of the Albany.

Concept view from Station Yard looking east towards the proposed Central with the proposed public realm (left).

STATION YARD DEVELOPMENT - CHARACTER & DESIGN

We want to ensure the character and design of our proposals complements and enhances the surrounding area and adjacent buildings. Therefore:

- The proposed building will relate well to its surroundings, including Bridge House to the east, the Albany pub to the west and the existing housing on Station Yard and Railway Approach to the south.
- The proposals adopt a shoulder height of 5 storeys, with a recessed sixth floor set back from the main facade.
- We have considered the siting and design of the development, including windows and balconies, to maintain privacy and minimise overlooking.
- The Albany pub is a building of local significance and we are keen that our proposals complement it and deliver enhancements to its immediate environment and public realm.
- We are considering a palette of materials that references local buildings, including other residential developments and railway architecture.
- The ground floor is stepped to avoid overlooking.

Wood cladding

Brick detail & material palette

Flat top balconies

Concept detail keys and materials

STATION YARD DEVELOPMENT - SITE & CONTEXT

Twickenham Station Yard sits to the south of the railway line, with Bridge House adjacent to the east and Twickenham Station further away to the north east. The site is currently used as a car park.

Immediately to the west of the site is land owned by Transport for London (TfL) as a bus waiting station. The Albany pub is beyond this to the west.

While the TfL site is not available to be included within our application, we are looking to enhance the public realm in and around this land as part of our proposals.

Transport route in the vicinity of the site.

STATION YARD DEVELOPMENT - PUBLIC REALM, PARKING & ACCESS

We are conscious that our proposals sit close to a residential area and busy town centre. As such, we have taken a number of steps to minimise the impact of our scheme:

- The site is physically separate from the station so the construction would have no impact on train operation.
- No roads are proposed to be closed during the construction period to avoid disruption to local drivers.
- We are currently carrying out assessments on whether our scheme would impact nearby residents and what steps are needed to minimise or avoid such impacts.

We also want to make sure that we deliver the best environment for the area, which is why:

- The development would be car free. The site is ideal for this due to its proximity to the town centre and public transport.
- Our proposals are low carbon, incorporating air source heat pumps to provide efficient, low energy homes.
- Enhanced planting and nature conservation features would be included in the design, so that there is a net gain in biodiversity and improvement of the streetscape.

Proposed public realm strategy with TfL operational requirements for 5 bases on TfL land.

STATION YARD DEVELOPMENT - NEXT STEPS

YOUR VIEWS ARE IMPORTANT TO US

Before leaving today, please fill out one of our feedback forms. We want to hear your thoughts on these proposals to feed into the design process and inform any subsequent planning application.

We aim to submit our application towards the end of September 2019. If a decision is made by the end of the year, we would expect work to start on site in 2021 and to be completed in 2022.

Please do speak to a member of the team if you have any questions about the scheme.

ALTERNATIVELY, SEND FEEDBACK TO

stationyard@portland-communications.com

Concept view from Railway Approach looking north west towards proposed Central with Bridge House on the right and proposed housing on the left.

Concept view from London Road bridge looking south towards proposed Central with The Exchange on the right and Bridge House on the left.

The boards presented at the Public Consultation

Response to Public Consultation Feedback

We have responded to the key issues raised in the Public Consultation as follows:

Increased traffic and parking problems

The proposal will not aggravate the existing problems in the area with parking, nor will it greatly increase traffic to the local streets. This is because the scheme is car-free, with the exception of two accessible parking spaces for the wheelchair adaptable units. The developer will work alongside Richmond Council to restrict parking permits for residents. It would be written into the lease that a car cannot be kept in the locality; this is something that the developer has done before. This is a feasible solution due to the proximity to the station and other public transport routes, with a local PTAL rating of 4 and 5.

Sustainability

The scheme incorporates a number of sustainable features, including a sedum roof, SUDs-compliant permeable paving, planting areas and air source heat pumps. These elements, along with a well-designed fabric to be detailed later, enable the scheme to reduce its carbon footprint, increase biodiversity and improve the local environment. This will reduce its impact on the local area and make it attractive to new residents.

In keeping with the surrounding area

The proposal has gone through multiple stages of design to optimise the massing, proportions, materials and detailing in a way that situates the building in its local context. The massing and proportions are consistent with existing buildings in the area, and mediate between taller buildings to the east and smaller residential houses to the south and west. The materials suggested have been inspired by those found in the local area, such as red brick and metal roofing. Elements of brick detailing have been taken from the stepped corbelling effect seen on the local railway buildings. These elements work together to create a building that is contemporary yet sensitive to its varied local context.

Public realm improvements

As explained in the DRP section and the Landscape section, the proposal involves a landscape design on the proposal site, as well as the potential to improve the landscaping on the adjacent land subject to TFL's permission. These improvements would work with Richmond Council's proposals to improve the land outside the Albany pub, with the aim to enhance the public realm in and around the site.

Height of development

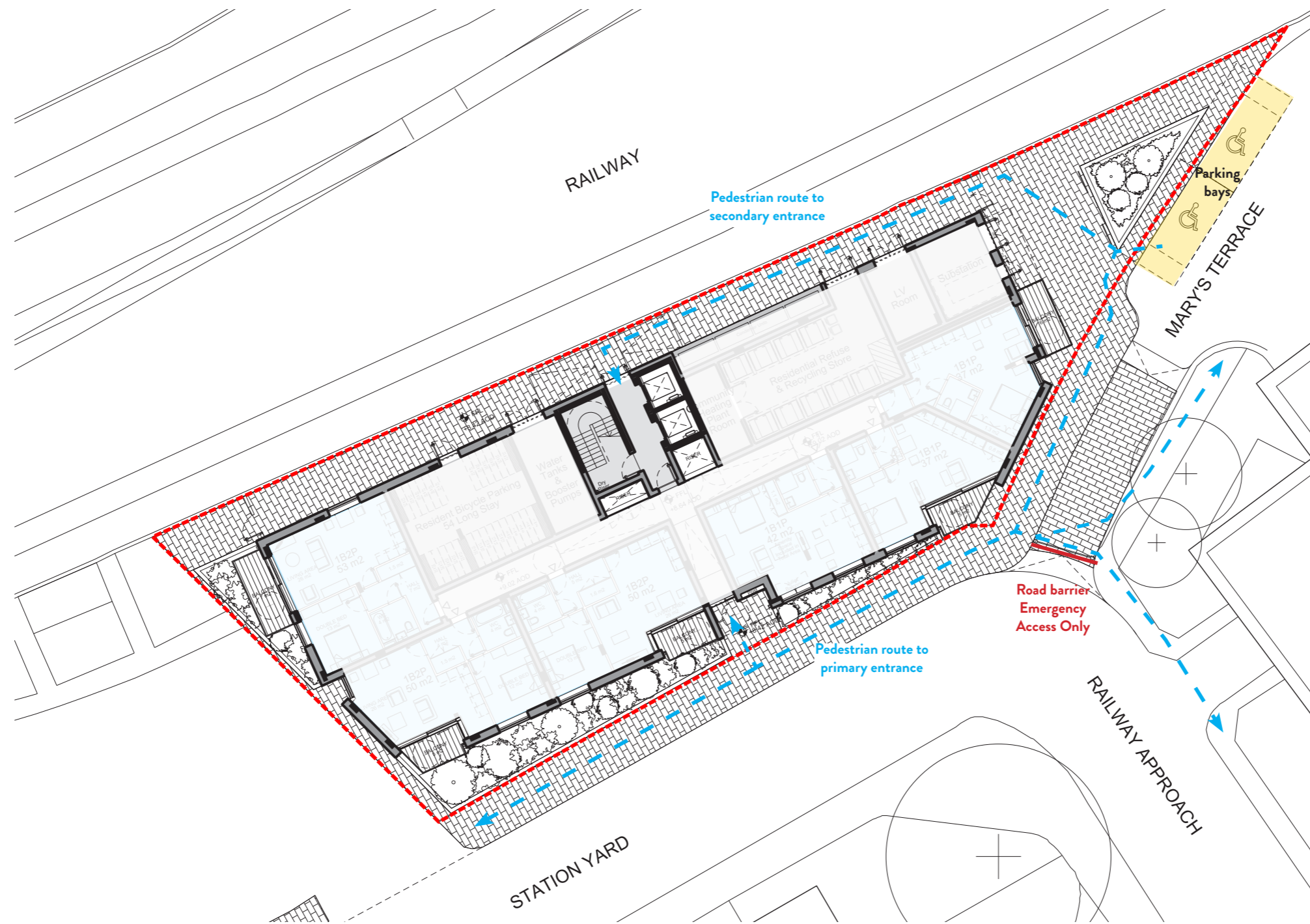
The proposal has been limited to a maximum height of Bridge House at 29.17m AOD, as discussed at Pre-App 1. The DRP indicated that they were very comfortable with the proposed height.

16. PARKING AND REFUSE

PARKING AND REFUSE

Car Parking

The development would be car free, with the exception of two disabled parking bays. The site is ideal for this due to its proximity to the town centre, good access to public transport and its mix of flat types which is more suitable for individuals and couples rather than large families.



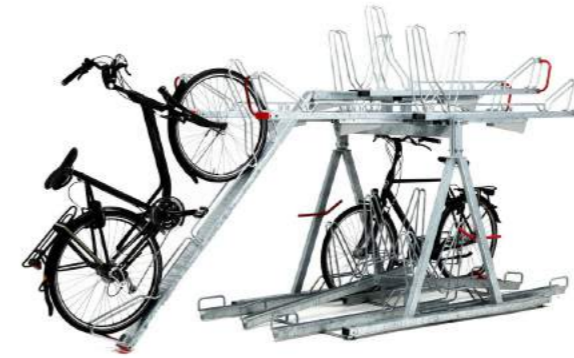
Site plan showing accessible parking spaces and associated routes

Cycle Storage

The proposal meets Richmond Council's minimum requirements for residents' cycle parking and an appropriate storage area has been integrated into the scheme. Richmond Cycle Parking Standards from the London Plan state that for dwellings, 1 long stay space is required per 1 bedroom units, 2 long stay spaces for all other dwellings, and 1 short stay space per 40 units. For our scheme, this totals 54 two-tier long stay racks and 1 short stay space, which will be situated externally.

Cycle Parking

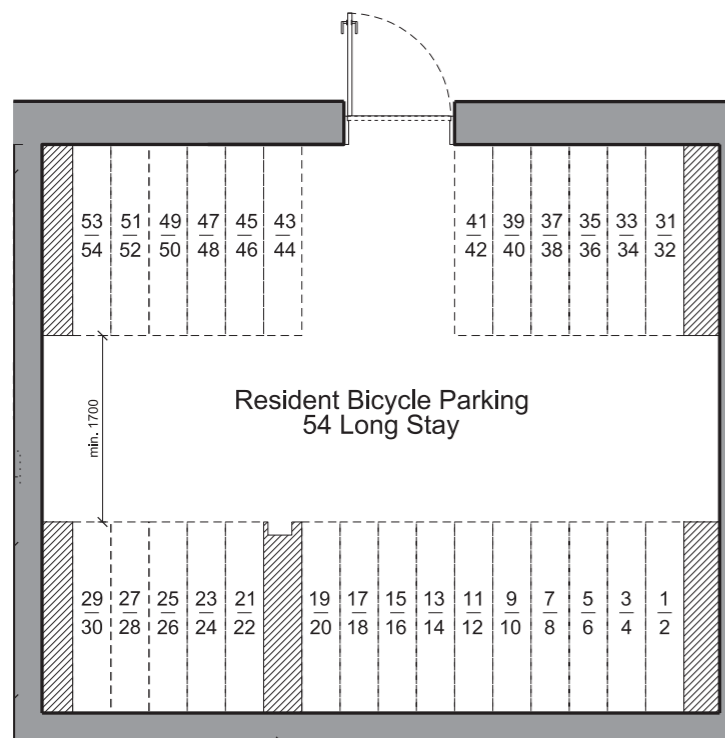
	Long Stay Space per 1 Bed Unit	Long Stay Space per 2+ Bed Unit	Short Stay Space per Unit
Number of Units	38	8	46
Richmond Cycle Space Requirement	1	2	1 per 40
Number of Spaces	38	16	1
Total		54	55



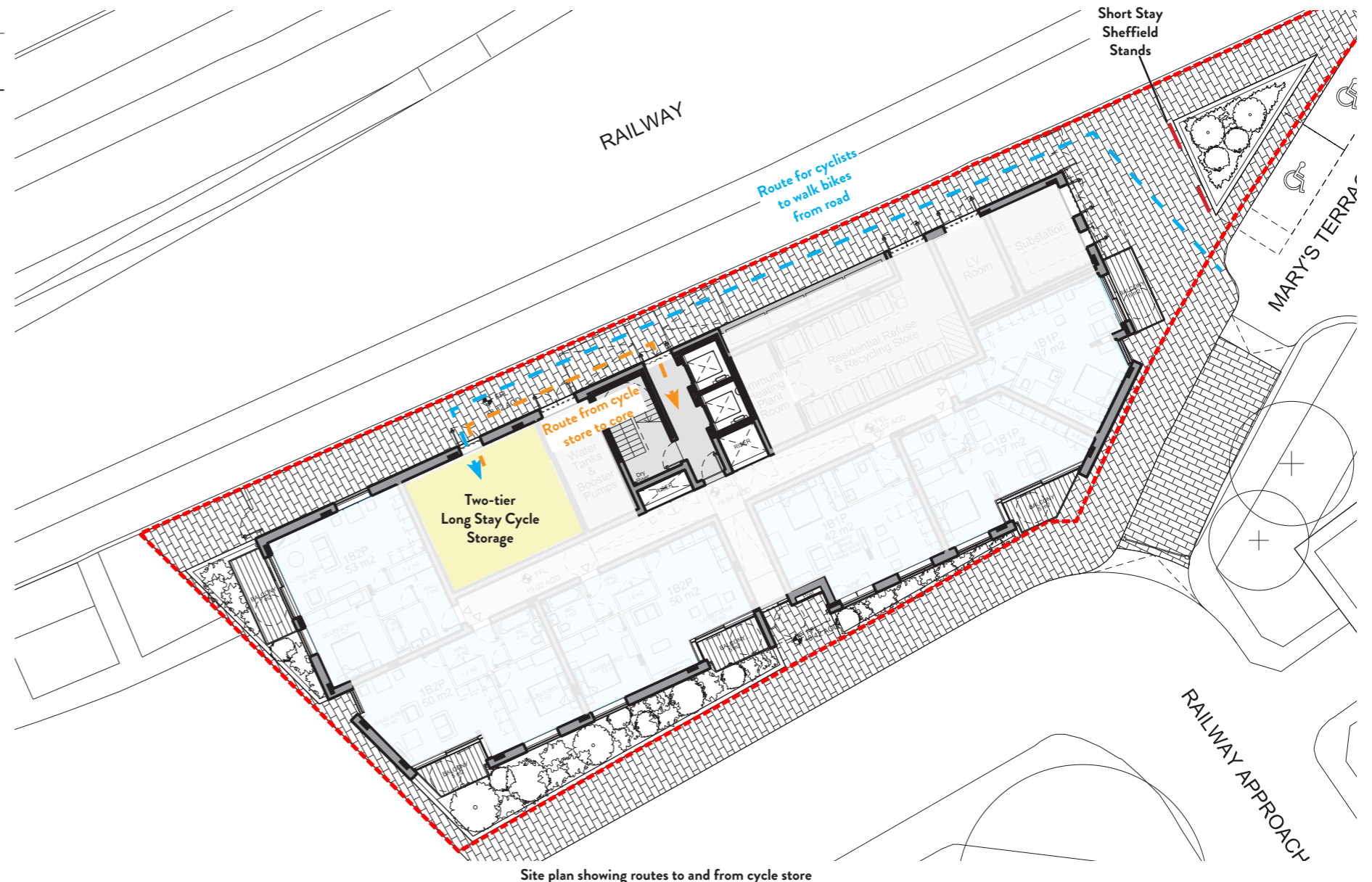
Two Tier Cycle Rack



Sheffield Cycle Stand



Resident Bicycle Parking Area Plan



Site plan showing routes to and from cycle store

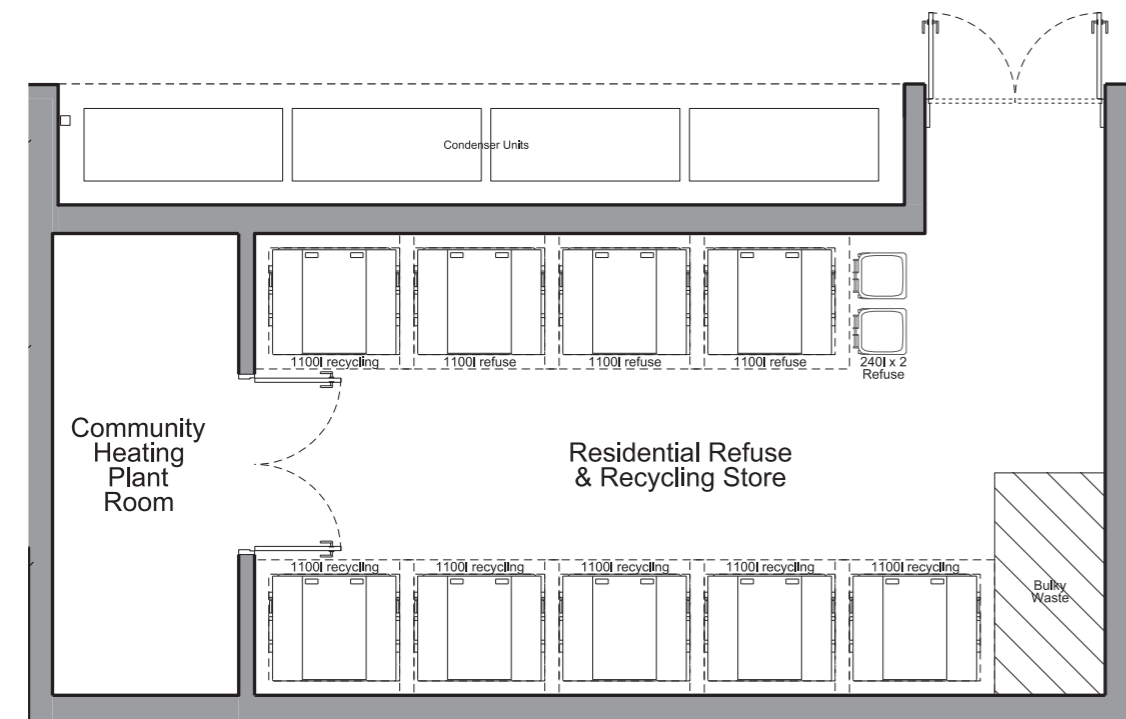
*Source: <https://www.london.gov.uk/what-we-do/planning/london-plan/current-london-plan/london-plan-chapter-six-londons-transport-2>

PARKING AND REFUSE

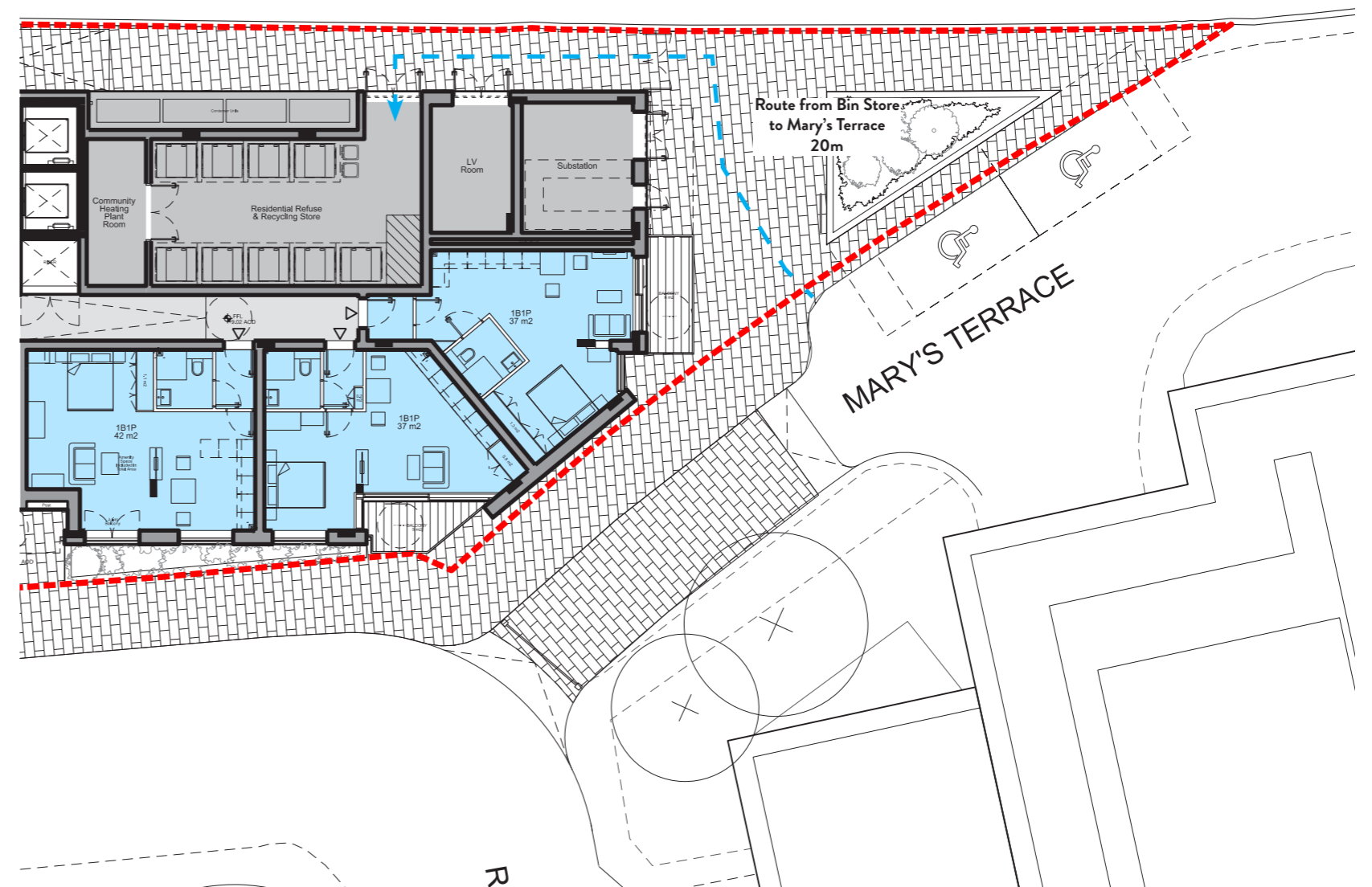
Refuse Storage

Richmond Council's refuse requirements* state that 70l of communal refuse storage containers are required per bedroom, and 6 1100l recycling bins are needed for 46 to 70 flats. This equates to 9 x 1100l bins and 2 x 240l bins for our scheme.

Waste and Recycling				
	Requirement	Number of beds or units	Total Litres	No. of 1100l Bins
Refuse	70l per bed	54	3780	3.4
Dry Recycling	6 x 1100l for 46-70	46	6600	6
		Total	10,380	9.4 bins



Refuse Store Plan



Ground Floor Plan showing route from Bin Store to Mary's Terrace

*Source: https://www.richmond.gov.uk/media/7627/refuse_and_recycling_storage_requirements_spd.pdf

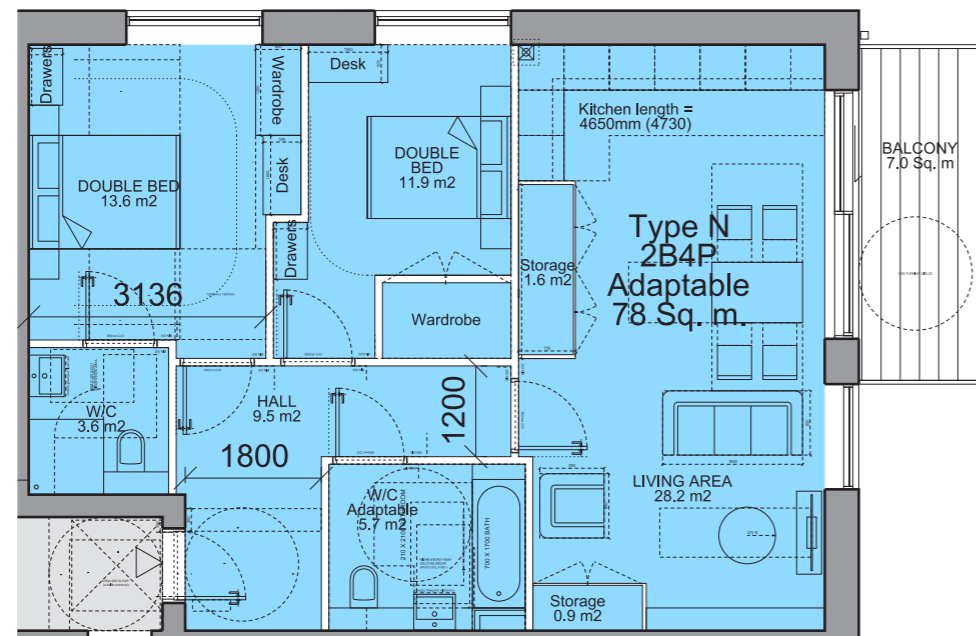
17. ACCESSIBLE APARTMENTS

ACCESSIBLE APARTMENTS

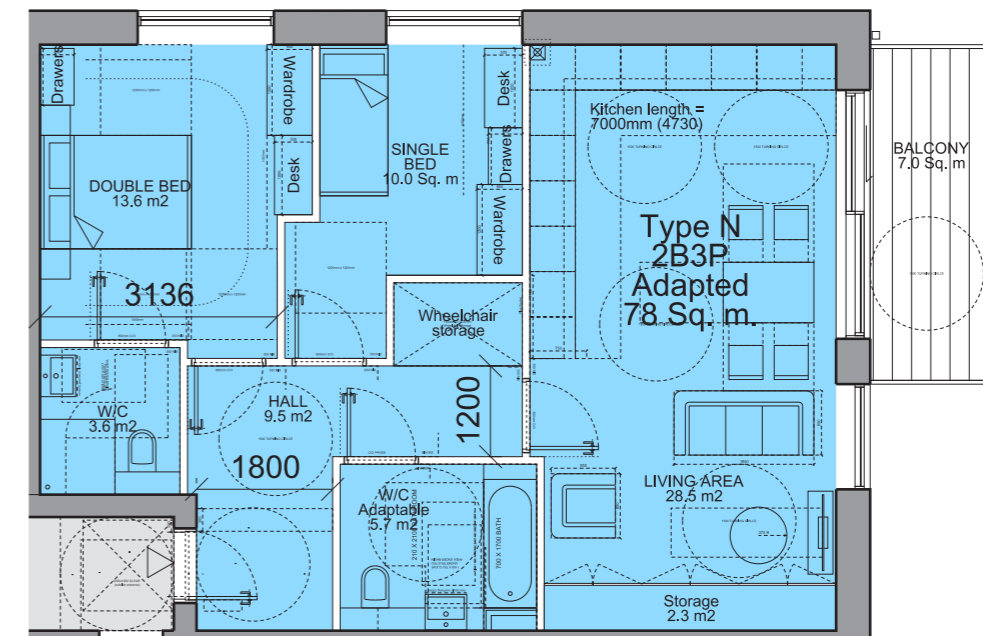
Wheelchair Adaptable Dwellings - M4(3)(2)(a)

There is one 2B3P wheelchair adaptable dwelling per typical floor and a 1B2P wheelchair adaptable dwelling on the fifth floor, totalling 5no. M4(3) units across the whole development (10% of the total number of flats).

Before the 2B3P unit is adapted it is a 2B4P unit. These units are flat type N, flat numbers 13, 21, 29, and 37.

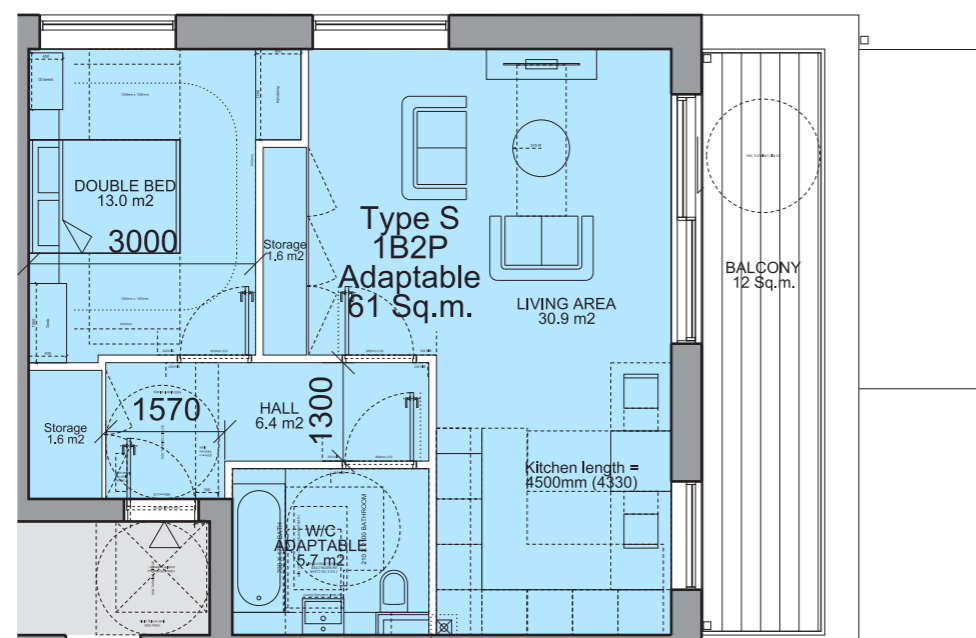


M4(3)(2)(a) Adaptable 2B4P Apartment

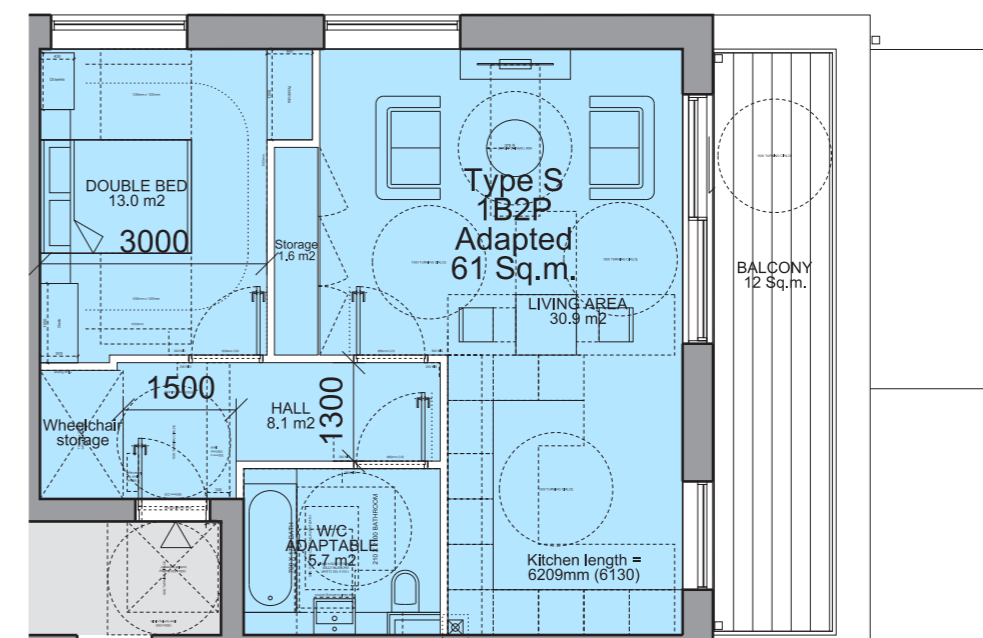


M4(3)(2)(a) Adapted 2B3P Apartment

The 1B2P unit is flat type S, flat number 45.



M4(3)(2)(a) Adaptable 1B2P Apartment



M4(3)(2)(a) Adapted 2B4P Apartment

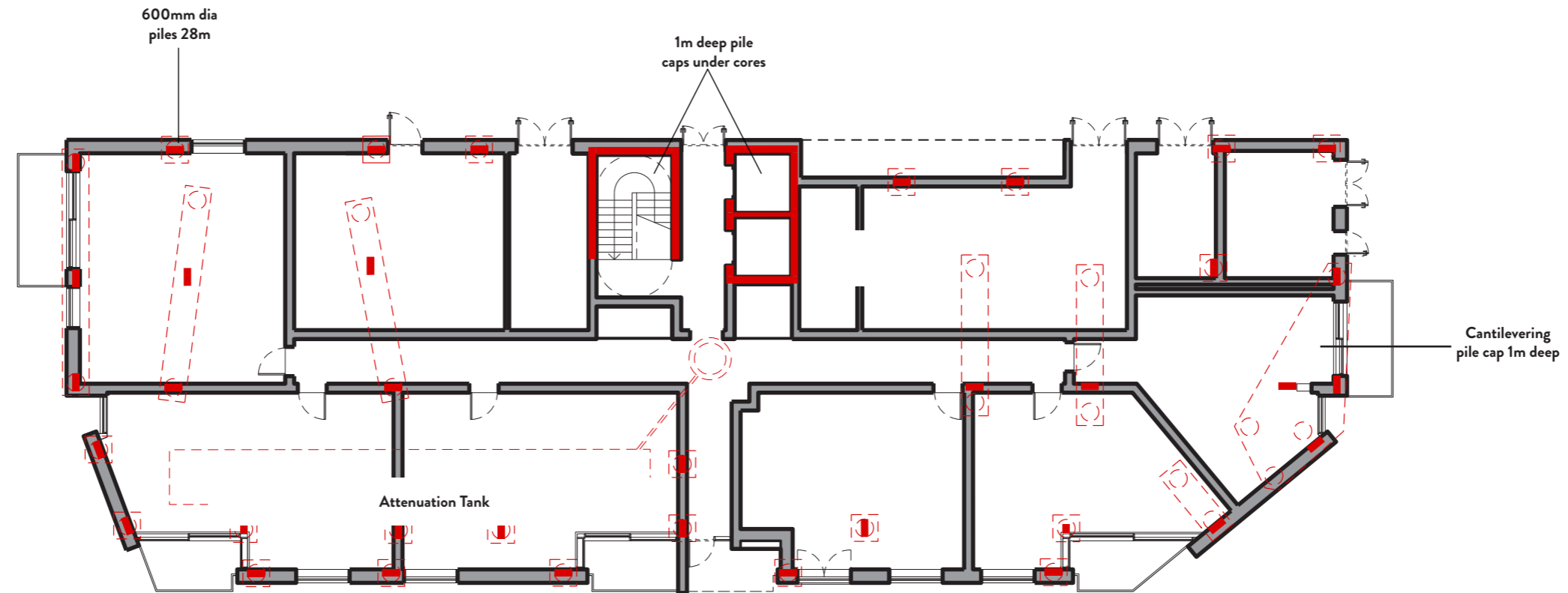
18. STRUCTURE

STRUCTURE

Foundations and Ground Floor Structure

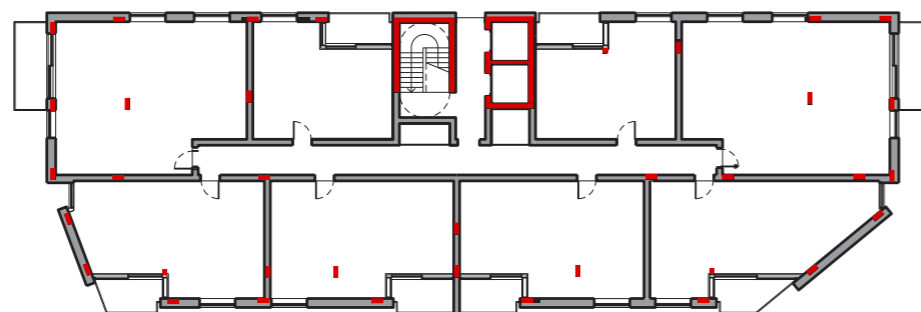
Ground Floor showing Foundations and Structure

Taking into account the site constraints, columns have been carefully positioned outside of the underground service offset zones where possible. Where not possible, localised cantilevered/bridging foundations will transfer the loads to pile foundations outside of the service exclusion zones.



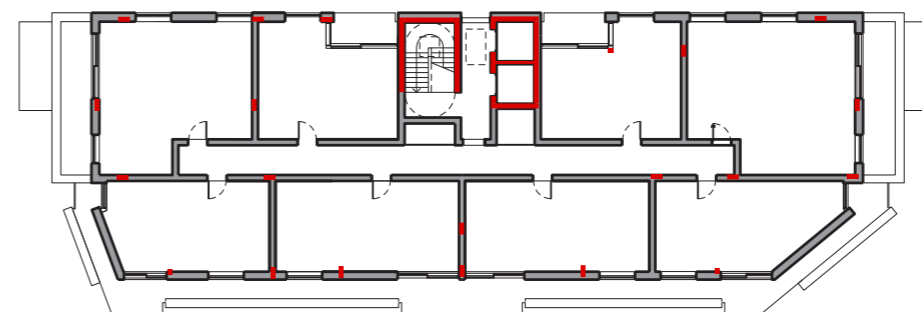
Typical Level

Columns are typically placed within partition locations that run vertically through the building.



Fifth Level

Fifth floor should ideally be a more light weight structure / smaller column sizes.



19. SERVICES

SERVICES

Ground Level Services

All services are located at ground level at the rear of the building, accessed via a 2m pathway.

Heating will be provided by air source heat pumps, four communal condenser units and a community heating plant room.

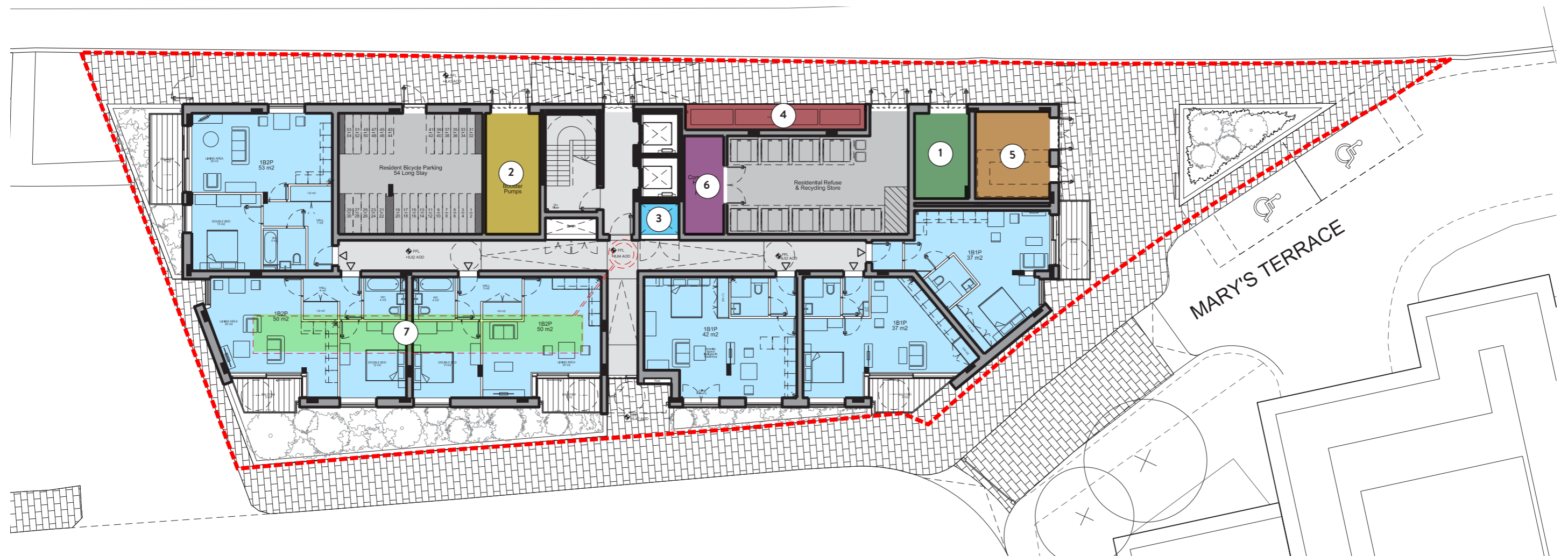
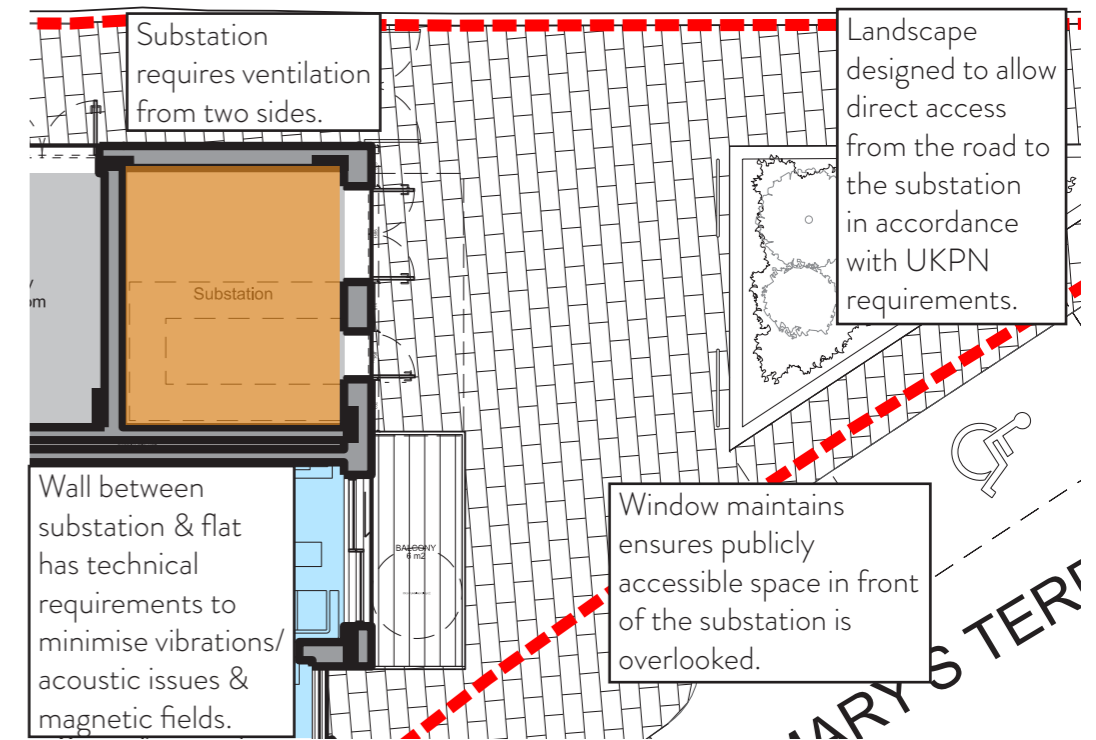
A substation has been incorporated into the ground floor plan as per UKPNs requirements for the energy strategy. Having the substation separate from the building is not possible on the site due to a 10m offset distance required and the limitations of the site.

The substation is a defined size, needs cross ventilation hence its positioning on the corner and has technical impacts on the spaces around it. For instance, the wall between the substation and the adjacent dwelling will need to mitigate vibration, noise and magnetic fields.

Access to the substation is to be 24 hour and cannot be from the rear walkway, due to the size restriction and logistics.

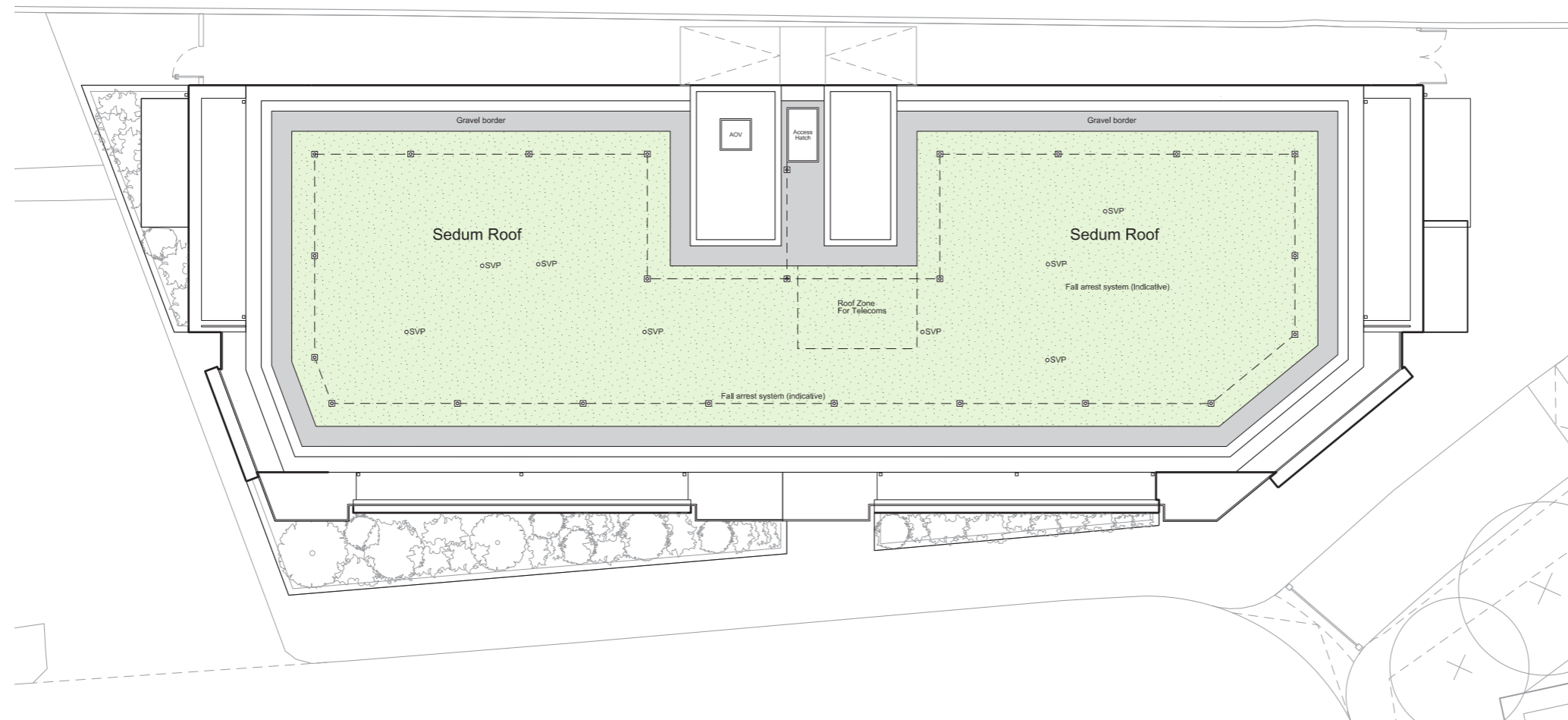
Plant Areas

1. LV Switchroom 4.2x3.1m (12.8m²) 3m high
2. Water Tanks and Booster Pumps 5.9x2.2m (13.1m²) 3.5m high
3. Telecoms Room 2.1x1.8m (3.6m²) 2.5m high
4. Double Stacked Air Source Heat Pumps (32 no.)
5. Electrical Substation 3.6x4.3m (15.5m²)
6. Community Heating Plantroom 1.6x4.8m (7.7m²)
7. Attenuation Tank



Roof Level Services

- For the SUDs strategy, the proposed development seeks to incorporate sedum roofs where possible, and a 35m³ buried geo-cellular attenuation tank below the building slab.
- Permeable paving is being considered for the car parking bays. The strategy will aid in reducing runoff across the site, improving attenuation, improving building performance and adding ecological value.

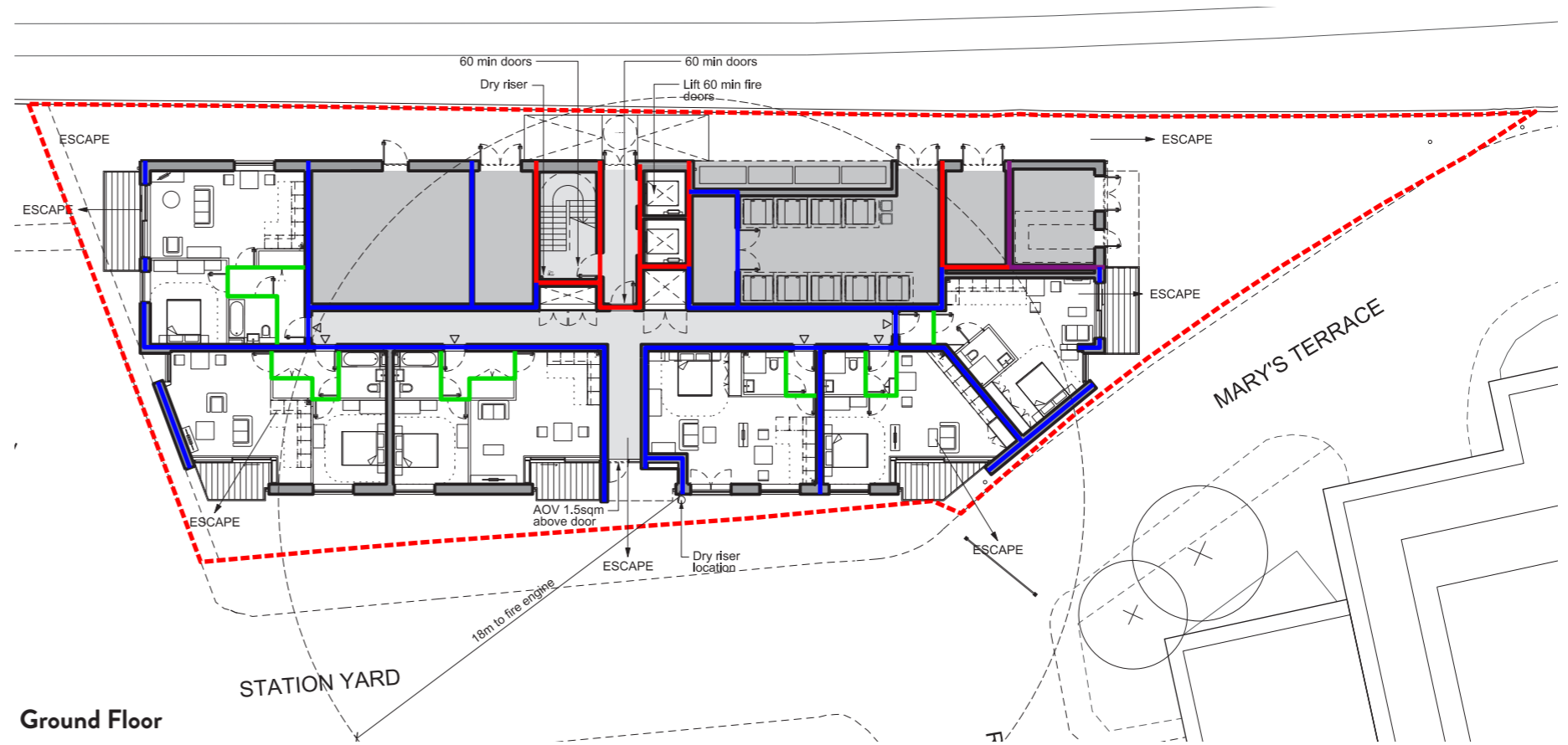


20. FIRE STRATEGY

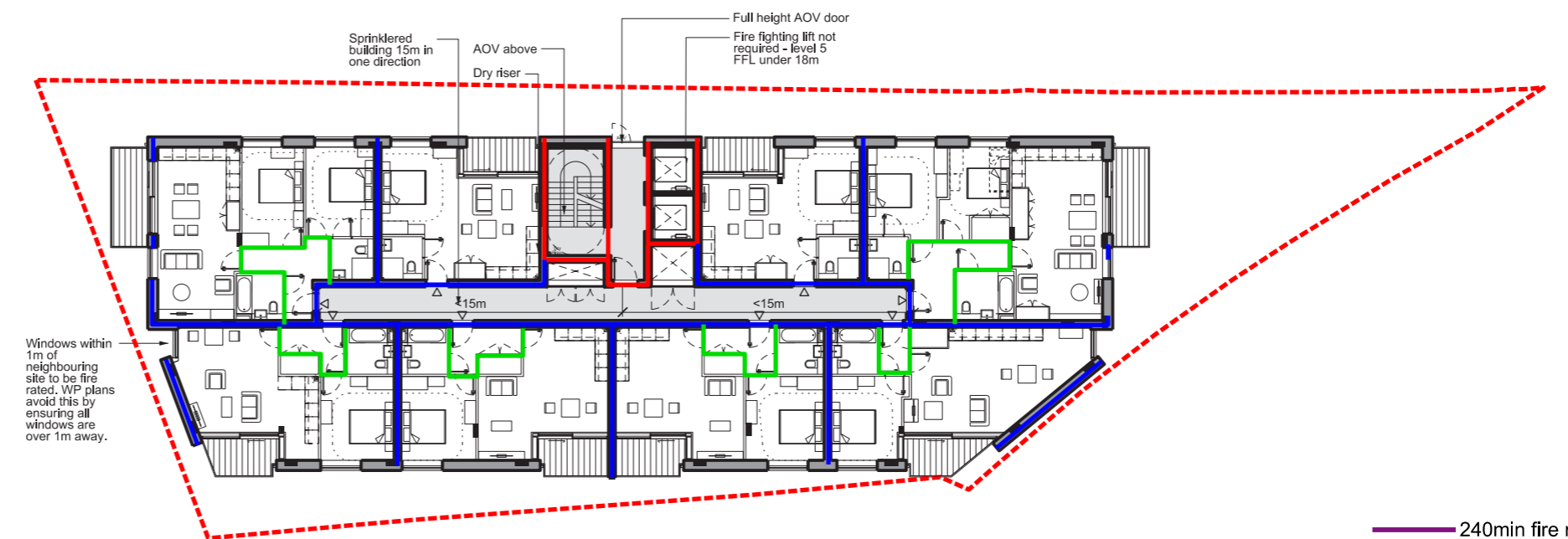
FIRE STRATEGY

Emergency Escape and Fire

- Main escape doors are the main front and rear entrances
- Fully sprinklered building
- Rail exclusion zone forms an escape route
- Dry riser at front of building, can be accessed by fire engine within 18m
- Dry riser in the stair core
- 1.5m² AOV windows in core
- FD60 doors to lifts, stairs and stair lobby
- FD30 doors to risers and apartment
- FD20 doors to internal apartment doors
- Most party walls 60min fire rated
- Core walls 120min fire rated
- Internal lobbies to flats 30min fire rated
- Substation walls 240min fire rated



Ground Floor



Typical Floor

- 240min fire rated
- 120min fire rated
- 90min fire rated
- 60min fire rated
- 30min fire rated

21. AREAS & MIX

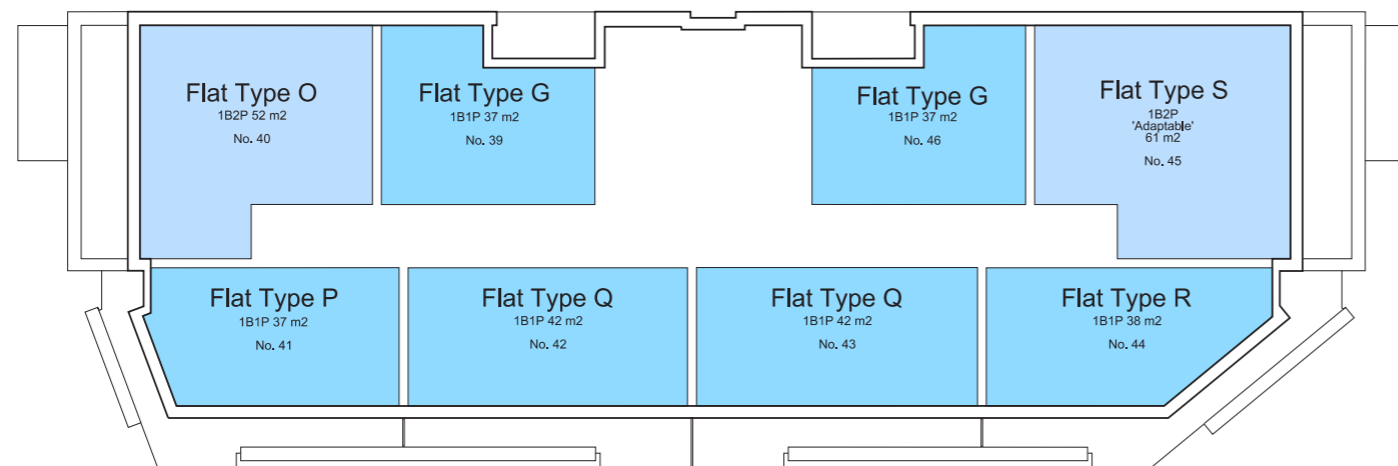
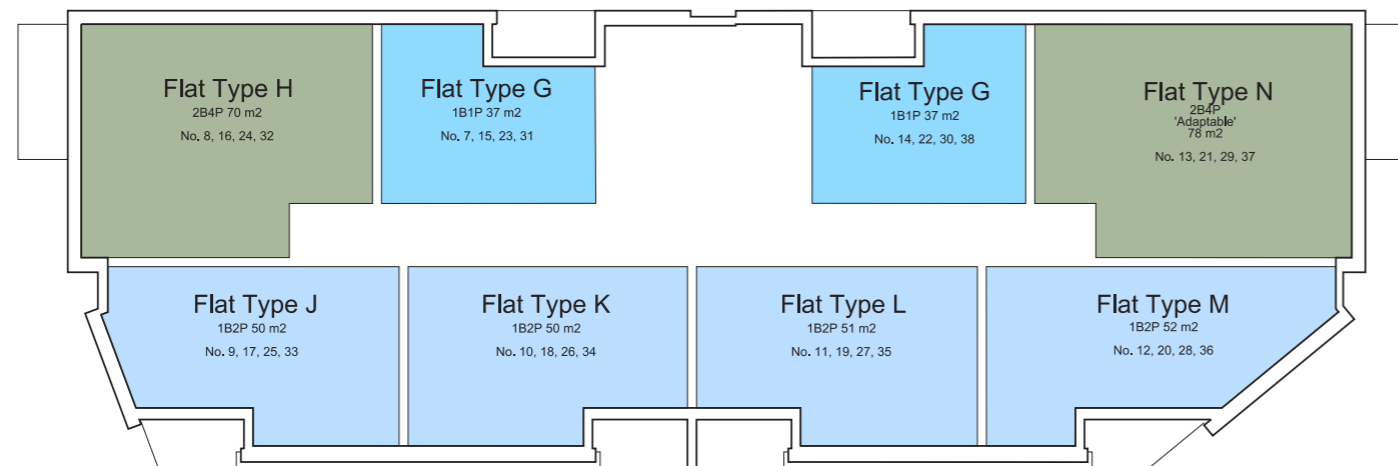
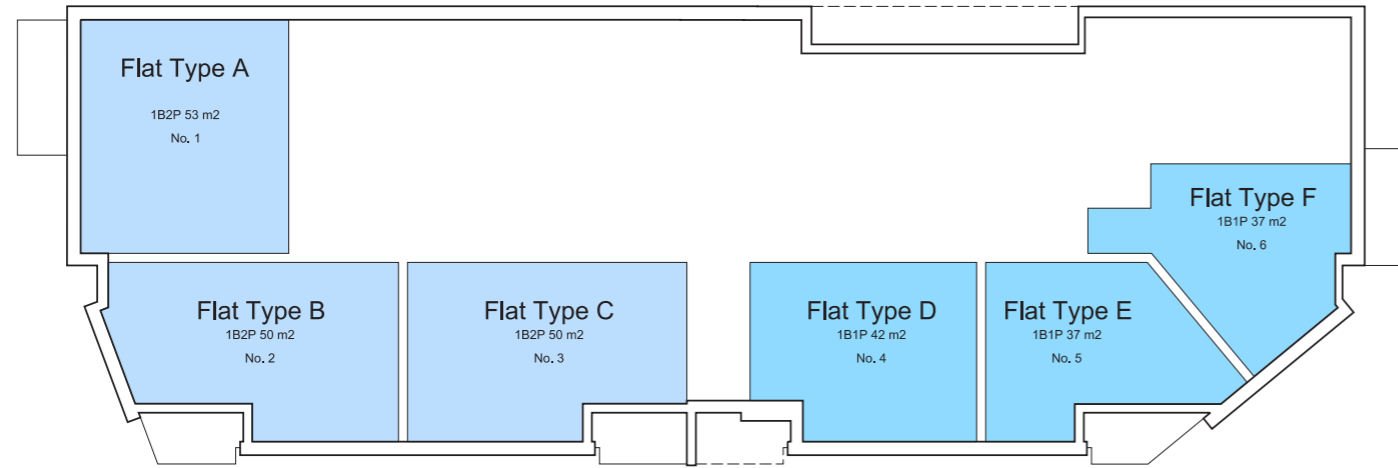
AREAS & MIX

Flat Mix & Areas

- 46 flats in total
- 18 flat types
- 83% 1 bed flats, 17% 2 bed flats, justified by the lack of parking and proximity to the station which makes the proposal ideal for individuals or couples rather than large families
- 5 wheelchair adaptable units (10% total)
- Floors 1, 2, 3 and 4 are typical - they have identical layouts

Flat Mix				
Level	1B1P (37sqm)	1B2P (50sqm)	2B4P (70sqm)	Total
Ground	3	3	0	6
First	2	4	2 (1 no. WAU)	8
Second	2	4	2 (1 no. WAU)	8
Third	2	4	2 (1 no. WAU)	8
Fourth	2	4	2 (1 no. WAU)	8
Fifth	6	2 (1 no. WAU)	0	8
Total	17	21	8	46
%	37%	46%	17%	100%
	38		8	
	83%		17%	
	Total 1 Beds		Total 2 Beds	

Flat Types



Flat Types			
Level	Flat Type	Total Number	Area m ²
Level 0	A	1	54
	B	1	50
	C	1	51
	D	1	42
	E	1	38
	F	1	38
Levels 1, 2, 3, 4, and 5	G	10	37
	H	4	70
	J	4	50
	K	4	51
	L	4	51
	M	4	52
	N	4	78
Level 5 only	O	1	53
	P	1	37
	Q	2	43
	R	1	38
	S	1	61
Total	18 Types	46 Flats	

The copyright in this report belongs to Wimshurst Pelleriti. The client has a non-exclusive licence to copy this material for the purpose only of publicising, financing or marketing the scheme but not for executing the scheme. Copying or use for any other purpose requires Wimshurst Pelleriti's written consent.

WIMSHURST
PELLERITI

The Mews,
6 Putney Common, SW15 1HL

0208 780 2206
info@wp.uk.com
wimshurst-pelleriti.com

