



UNION 4 PLANNING

SOLUM REGENERATION (TWICKENHAM) LLP

Station Yard, Twickenham, TW1

Redevelopment of existing car park to provide a new building of 5 to 6 storeys, comprising 46 no. residential units (Use Class C3), disabled car parking, cycle parking, landscaping, enhancement to public realm and associated works

TOWNSCAPE AND VISUAL IMPACT ASSESSMENT

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UNION4
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1.0 INTRODUCTION

- 1.1 Union4 Planning are instructed by Solum Regeneration (Twickenham) LLP to provide a Townscape and Visual Impact Assessment (TVIA) in support of the proposed redevelopment of the application site at Station Yard, Twickenham, TW1 ('the Site').
- 1.2 The site is located within the London Borough of Richmond upon Thames and is approximately 0.10 hectares in size. It is bound by Station Yard to the south, the railway line to the north, Mary's Terrace to the east and land forming an area of bus parking belonging to Transport for London (TfL) to the west. The site is currently being used as a car park by residents of Mary's Terrace in connection to development works at Twickenham Railway Station.
- 1.3 The purpose of this TVIA is to assess the likely impacts of the proposed development on the surrounding visual amenity and townscape character. It considers the proposed development in relation to its urban setting, taking into account the relationship between buildings and open spaces, as well as architectural styles and features and the scale of buildings within the area.
- 1.4 The proposed development involves the redevelopment of the site to provide a 5 to 6-storey building comprising 46 no. residential units, disabled parking, cycle parking, landscaping, public realm enhancements and associated works.
- 1.5 The study area for the TVIA generally comprises the visual envelope of the site, including conservation areas, character areas and viewpoints which are described in detail in Section 4.
- 1.6 This report takes on the following structure:
 - Section 2 provides an overview of the methodology adopted for the TVIA;
 - Section 3 summarises the legislative and policy context in respect of this report;
 - Section 4 provides an overview of the baseline conditions of the site, including an analysis of the existing townscape characteristics and visual context;
 - Section 5 describes the proposed development, including existing and proposed views;
 - Section 6 assesses the impact of the proposals on the surrounding townscape; and
 - Section 7 provides a summary and conclusions.

2.0 METHODOLOGY

- 2.1 This section outlines the methodology that has been used to carry out the TVIA.
- 2.2 The TVIA has been undertaken in accordance with the relevant legislation, policy and guidance, and is broadly based on the principles set out in the third edition of 'Guidelines for Landscape and Visual Impact Assessment 2013' (GLVIA3), produced by the Landscape Institute and Institute of Environmental Management & Assessment.
- 2.3 The assessment has taken into account the physical grain of the area including any statutory or locally listed buildings, conservation areas and future development to determine whether the site is appropriate for the proposed development and whether its design is appropriate within the surrounding context.
- 2.4 The GLVIA3 stipulates that the purposes of the baseline studies are to record and analyse the character, quality, enhancement potential and sensitivity of the existing landscape in the vicinity of the proposed development.

This has involved a process of:

- Description (i.e. collecting and presenting the information about the townscape and visual resources in a systematic manner);
- Classification (i.e. sorting the townscape resources into units of distinct and recognisable character);
- Description of the townscape and visual context, classification of areas of distinct and recognisable character and summarising them on a character plan;
- Defining the baseline study area which extends beyond the application site and aims to cover the whole of the area from which the proposed development would be visible. This includes a desk study, field survey and analysis, and should explore patterns and scale of landform, land cover and built development, as well as any special historic and cultural values;
- Identifying specific potential receptors of landscape and visual effects such as important components of the landscape.

Views and Character Areas

- 2.5 Viewpoints have been determined based on their relationship to the proposal site and where it is considered that the development will substantially alter the current view. Character areas have been determined based on their built form including scale, massing, materials, design and

the nature of uses within the area, as well as their relationship and proximity to the application site.

- 2.6 Each view and character area have been considered based on the existing and proposed setting and an assessment is made of the significance of these effects.

Impact Assessment

- 2.7 The impacts in this TVIA have been assessed in terms of magnitude and significance, as well as indicating the nature of the impact as follows:

Magnitude of Impact	
Major	Change resulting in a high degree of deterioration or improvement.
Moderate	Change resulting in a material deterioration or improvement.
Low	Change resulting in a low degree of deterioration or improvement.
Negligible	Change resulting in a negligible degree of deterioration or improvement.
Neutral	No change.
Significance of Impact	
Major	Change that materially affects the receiving environment where there is little or no scope to accommodate change.
Moderate	Change that materially affects the receiving environment that may have the ability to accommodate change.
Low	Change that has a limited effect on the receiving environment that has the ability to accommodate change.
Negligible	Change that has little effect on the receiving environment that has the ability to accommodate change.
Neutral	Change that is entirely compatible with the receiving environment and is of neutral significance.

Nature of Impact
Positive
Adverse
None

3.0 LEGISLATION AND POLICY CONTEXT

3.1 This section sets out the relevant legislation as well as national, regional and local planning policy. The legislative framework for the site comprises the following documents:

- Town and Country Planning Act 1990;
- Planning and Compulsory Purchase Act 2004;
- Planning (Listed Buildings and Conservation Areas) Act 1990;
- Planning Act 2008; and
- Localism Act 2011.

3.2 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning applications are determined in accordance with the development plan for the site unless material considerations indicate otherwise. The development plan comprises the following documents:

- London Plan (2016);
- Richmond Local Plan (2018);
- Twickenham Area Action Plan (2013); and
- Twickenham Station and Surroundings Design Standards Supplementary Planning Document (2010).

3.3 This section summarises the national policies and development plan policies, which are considered relevant to the TVIA.

National Planning Policy Framework

3.4 The new National Planning Policy Framework (NPPF) was published in February 2019 and sets out the Government's overarching planning policies with a presumption in favour of sustainable development.

3.5 Section 12 of the NPPF 'Achieving well-designed places' reinforces the importance of good design as a key aspect of sustainable development, creating better places in which to live and work and helping to make development acceptable to communities.

3.6 Paragraph 127 states that planning policies and decisions should ensure that developments;

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;

- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

London Plan (2016)

- 3.7 The London Plan is the overall strategic plan for London, setting out an integrated economic, environmental, transport and social framework for the development of London over the next 20–25 years. The key policies relating to the assessment of heritage, townscape and visual impacts are contained within Chapter 7 'London's Living Spaces and Places.'
- 3.8 Policy 7.4 'Local Character' states that development should have regard to the form, function, and structure of an area, place or street and the scale, mass and orientation of surrounding buildings.
- 3.9 Buildings, streets and open spaces should provide a high quality design response that:
 - a) has regard to the pattern and grain of the existing spaces and streets in orientation, scale, proportion and mass
 - b) contributes to a positive relationship between the urban structure and natural landscape features, including the underlying landform and topography of an area
 - c) is human in scale, ensuring buildings create a positive relationship with street level activity and people feel comfortable with their surroundings
 - d) allows existing buildings and structures that make a positive contribution to the character of a place to influence the future character of the area

e) is informed by the surrounding historic environment.

- 3.10 Policy 7.5 'Public Realm' states that development should make the public realm comprehensible at a human scale, using gateways, focal points and landmarks as appropriate to help people find their way.
- 3.11 Policy 7.6 'Architecture' requires that buildings and structures are of the highest architectural quality and are of a proportion, composition, scale and orientation that enhances, activates and appropriately defines the public realm. Details and materials should complement but not necessarily replicate the local architecture, and development should not cause unacceptable harm to the amenity of surrounding land and buildings, particularly residential buildings.
- 3.12 Policy 7.9 'Heritage-led Regeneration' states that regeneration schemes should identify and make use of heritage assets and reinforce the qualities that make them significant so they can help stimulate environmental, economic and community regeneration. This includes buildings, landscape features, views, Blue Ribbon Network and public realm.

Richmond Local Plan (2018)

- 3.13 Policy LP 1 'Local Character and Design Quality' requires all development to be of a high architectural and urban design standard. Proposals should demonstrate a thorough understanding of the site and how it relates to its existing context. The following will be considerations when assessing proposals:
- 1. compatibility with local character including the relationship to existing townscape, development patterns, views, local grain and frontages as well as scale, height, massing, density, landscaping, proportions, form, materials and detailing;*
 - 2. sustainable design and construction, including adaptability, subject to aesthetic considerations;*
 - 3. layout, siting and access, including making best use of land;*
 - 4. space between buildings, relationship of heights to widths and relationship to the public realm, heritage assets and natural features;*
 - 5. inclusive design, connectivity, permeability (as such gated developments will not be permitted), natural surveillance and orientation; and*
 - 6. suitability and compatibility of uses, taking account of any potential adverse impacts of the colocation of uses through the layout, design and management of the site.*
- 3.14 Policy LP 2 'Building Heights' requires new buildings to respect and strengthen the setting of the borough's valued townscapes and landscapes, through appropriate building heights, by the following means:
- 1. require buildings to make a positive contribution towards the local character,*

townscape and skyline, generally reflecting the prevailing building heights within the vicinity; proposals that are taller than the surrounding townscape have to be of high architectural design quality and standards, deliver public realm benefits and have a wholly positive impact on the character and quality of the area;

2. *preserve and enhance the borough's heritage assets, their significance and their setting;*
3. *respect the local context, and where possible enhance the character of an area, through appropriate:*
 - a) *scale*
 - b) *height*
 - c) *mass*
 - d) *urban pattern*
 - e) *development grain*
 - f) *materials*
 - g) *streetscape*
 - h) *Roofscape and*
 - i) *wider townscape and landscape;*
4. *take account of climatic effects, including overshadowing, diversion of wind speeds, heat island and glare;*
5. *refrain from using height to express and create local landmarks; and*
6. *require full planning applications for any building that exceeds the prevailing building height within the wider context and setting.*

3.15 Policy LP 3 'Designated Heritage Assets' requires development to *conserve and, where possible, take opportunities to make a positive contribution to, the historic environment of the borough.* Great weight should be given to the conservation of heritage assets when considering the impact of a proposed development on the significance of the asset.

3.16 Policy LP 4 'Non-Designated Heritage Assets' states that the Council will seek to preserve, and where possible enhance, the significance, character and setting of non-designated heritage assets, including Buildings of Townscape Merit, memorials, particularly war memorials, and other local historic features.

4.0 BASELINE CONDITIONS

- 4.1 This section provides an analysis and overview of townscape character areas within the vicinity of the site and identifies key viewpoints from which the proposed development has been assessed.
- 4.2 Character areas have been identified based on land uses, built form, layout and history within the vicinity of the site and are shown on Figure 6.

Townscape Context

- 4.3 The proposed development is located in Twickenham in the London Borough of Richmond upon Thames. The development site forms part of the Station Yard that fronted the original Twickenham Station, opened in 1848. The original buildings were demolished when the current railway station was opened in 1954.
- 4.4 The surrounding area is subject to a major regeneration scheme, comprising the redevelopment of Twickenham Station which will provide a mix of uses including retail shops, residential units and updated public transport facilities.
- 4.5 The Borough is broadly residential in nature, with a number of town and local centres, and is characterised by urbanised areas which connect former villages, divided by the River Thames. LBRuT have identified twelve distinctive character areas within the borough which are set out in the 2006 Design Quality SPD. Character areas are defined by their cohesive identity or by the location of natural and manmade barriers such as the river, open space and railways.
- 4.6 The site sits within the character area known as 'Twickenham and St. Margaret's', a long-established settlement with a strong tradition of riverside uses and an important commercial centre. The centre comprises shops, flats and offices within continuous rows of Victorian and 20th century mixed use parades, based on Heath Road, King Street, York Street and London Road.
- 4.7 The southeast portion of the character area provides a contrast to the centre with a village-like character comprised of winding lanes, small cottages and a semi-rural edge with large villas to the riverside. The streets around St. Margaret's and East Twickenham form a picturesque and leafy landscape containing many fine Victorian and Edwardian villas and interwar homes.
- 4.8 The site is located centrally in Twickenham to the south of the railway line which divides the settlement along a north-south and east-west axis. London Road further divides the settlement east-west and forms the main commercial town centre towards the south. The streets off London Road to the east and west are mainly residential with a varying scale and character.

- 4.9 The urban grain south and west of the site is broadly characterised largely by semi-detached and terraced housing which establishes a relatively regular street pattern around Sherland Road, Queen's Road and Grosvenor Road. There is a marked contrast, however, along London Road and near Twickenham Station where buildings are much larger in scale, both in footprint and height. Station Yard forms an interface between the two character areas, fronted by three storey modern terraces, flats and maisonettes to the south and the more substantial office buildings and apartment blocks to the north and east.

Figure 1: Aerial View of Immediate Context



Source: Google Earth (2019)

- 4.10 There are a mix of land uses in the immediate vicinity of the site including offices, public transport, community/leisure, hotel and restaurants/pubs. Beyond the immediate surroundings, building types are predominantly residential though with the retail and associated uses in the town centre extending south eastwards.

Designations

- 4.11 This assessment considers any statutory and local designations within the vicinity of the site including conservation areas, listed buildings and local designations, details of which are set out below.

Conservation Areas

- 4.12 There are four conservation areas within 250m of the site: Queen's Road, Amyand Park Road, Cole Park Road and Twickenham Riverside. Most pertinent to the proposed development is Queen's Road Conservation Area which is located some 50m to the west. The remaining conservation areas do not form part of the visual envelope of the site, and therefore have not been assessed in respect of the proposals.
- 4.13 The Queen's Road Conservation Area is a late 19th and early 20th century residential infill behind the 18th century King Street frontages. The area comprises late Edwardian houses, mostly semi-detached which incorporate a variety of styles and include modern additions. There are also areas containing small Victorian cottages, but these have been extensively altered. The eastern edge of the conservation area has a strong character, containing several large Victorian buildings which incorporate unusual polychromatic brickwork and attractive terraces.
- 4.14 The conservation area contains a number of Statutory Grade II Listed Buildings including The George Public House, Grosvenor House, and Nos. 10, 12, 54, 60 and 62 King Street and Nos. 29 and 31 Holly Road. The George is an important building of late 17th century origins, whilst the garden setting of Grosvenor House serves as an important entry building into the area from London Road. The area also contains the locally listed Albany Pub which is a focus to the north end of Queen's Road.

Figure 2: Property on Queen's Road



(Source: Union4 Planning)

Figure 3: Houses on Queen's Road



(Source: Union4 Planning)

Figure 4: Houses on Queen's Road showing polychromatic detailing



(Source: Union4 Planning)

Figure 5: Queen’s Road Conservation Area boundary and site location



(Source: LBRuT, annotated by Wimshurst Perreti)

Listed Buildings

- 4.15 There are no statutory listed buildings within or adjacent to the site.
- 4.16 There are a number of Locally Listed properties nearby which are set out below.
- Nos. 1-16 Marys Terrace;
 - Albany Public House, Queen’s Road;
 - Nos. 2-8 (even) and nos. 3-7 (odd) Queen’s Road;
 - Cabbage Patch Public House, London Road;
 - Nos. 1-4 Railway Cottages;
 - Disused Industrial Building Southwest of Railway Cottages.

- 4.17 The most relevant to the proposed development is the Albany Public House, a three-storey stucco building on an island site which is situated to the west of the application site, closely framed by Station Yard and Station Road.

Figure 6: Albany Pub



(Source: Union4 Planning)

Character Areas

- 4.18 For the purposes of this study, the townscape context of Station Yard has been broken down into smaller areas, determined by their character and relationship to the site. An overview of each area is set out below.

Figure 7: Character Areas



(Source: Google Maps, annotated by Union4 Planning)

Brewery Lane Character Area

- 4.19 The Brewery Lane development sits opposite the railway tracks to the north of the site. It is the site of a former Royal Mail Sorting Office and is characterised by a mix of red, brown and yellow brick buildings comprised of recently constructed residential blocks and the Locally Listed former railway cottages and industrial building.
- 4.20 The scheme was designed referencing the former railway buildings in the area and responding to surrounding visual references including the houses along Station Road which adopt London stock brickwork and rendered detailing.
- 4.21 The development ranges from 3 to 6 storeys in height, rising towards the London Road and station frontage and the scheme is predominantly residential in nature. However, the area also incorporates two restaurants, a community theatre building and a public piazza.

Figure 8: Brewery Lane buildings



(Source: Google Maps)

Figure 9: View from Brewery Lane looking southeast towards the proposal site



(Source: Union4 Planning)

Figure 10: View towards the site from Brewery Lane car park/entrance off London Road



(Source: Google Maps)

Twickenham Station and London Road Character Area

- 4.22 Twickenham Station is located approximately 100m northeast of the site on the opposite side of London Road. It is currently undergoing major regeneration which will provide significant upgrades to the station whilst incorporating new homes, retail units, a new station plaza and a riverside walk along the River Crane. The development represents the changing nature and character of the area, modernising and enhancing outdated facilities whilst providing a mix of additional uses. As the scheme is currently under construction and shortly to open, the Station Yard proposals have been considered based on the future completed development.
- 4.23 The development rises to a maximum of 7 storeys with the use of brick to reflect the surrounding architectural materials and styles with glazing and more modern materials around the new piazza and station entrance. The station entrance will contrast with a glazed frontage and series of louvres and balconies above which will be predominantly clad in glass and metal.
- 4.24 There is an evident cluster of taller buildings focused around the railway station, and this extends south along London Road, which is the key route running north-south through the commercial centre of Twickenham. Adjacent to the application site, the road is defined by large buildings ranging between 5 and 10 storeys in height including Bridge House (5 commercial storeys), Regal House (10 commercial storeys) and the future Twickenham Station (7 storeys).

- 4.25 Of notable distance to the surrounding character within this area are the locally listed Nos. 1-16 Mary's Terrace, located within the eastern portion of Mary's Terrace. These are small two-storey 19th century terraced houses running parallel to and facing the railway line, bounded by low front walls. Whilst geographically these properties fall within this character area, they are situated at a lower level to their immediate surroundings, and thus relate more appropriately to the residential area to the east. Furthermore, they are segregated from the site via London Road which runs north-south above Mary's Terrace. Nonetheless, they have been included within this assessment given their proximity to the development site.
- 4.26 The houses form a consistent row along this stretch of Mary's Terrace and are easily distinguishable from other buildings within the area. The houses are not visible from London Road due to the surrounding large-scale developments, however they are visible from the pedestrian footbridge at Twickenham Station which provides access to the various platforms.

Figure 11: No. 1-16 Mary's Terrace



(Source: Union4 Planning)

- 4.27 Moving southwards within the character area, the Cabbage Patch public house, a building of townscape merit, is framed by more modern 20th century buildings which extend south along the road. London Road continues as the main retail and commercial core with a mix of more modern 20th century buildings and 19th century terraces which create a strong sense of the high street townscape pattern with building heights stepping down to an average of 3-4 storeys.

- 4.28 The portion of London Road nearest to the site forms a bridge over the railway line. To the south of the bridge, land uses along the road are predominantly commercial, retail, offices and pubs with buildings set in a linear pattern.
- 4.29 Whilst the majority of buildings along London Road comprise red and brown brick, Regal House incorporates contrasting white and black cladding which is considered a departure from the building styles in the area.

Figures 12 and 13: Bridge House (left) and Regal House (right)



(Source: Union4 Planning)

Figures 14 and 15: Twickenham Station Proposals



(Source: Solum Regeneration (Twickenham) LLP)

- 4.30 Regal House and Bridge House contain undercroft car parking at ground level, thus differentiating them from surrounding buildings which generally have active frontages. However, the sloping nature of the London Road bridge and the topography of the area largely obscures the view of ground level and creates the illusion of a typical ground floor level.
- 4.31 Overall, this character area is defined by large-scale development of a mixed nature. The majority of buildings have been constructed using brick. However, there has been an

introduction of more modern render and glazed elements, particularly with Regal House and the future Twickenham Station. The north generally comprises commercial and employment uses, with the introduction of retail and high street uses moving southwards.

Railway Approach, Station Yard and Surroundings Character Area

- 4.32 This character area incorporates the application site and its surroundings and is predominantly residential in nature. It adjoins the Queen’s Road Conservation Area, but has been assessed separately as it differs in architectural style and form.
- 4.33 The majority of houses in the area are three-storey modern townhouse-style developments with front garages. Brick remains the most dominant material in line with the surrounding areas. However, the buildings do not contain any notable details or distinguishable features being predominantly 20th century in origin.

Figure 16: View from Station Yard towards Grosvenor Road



(Source: Union4 Planning)

- 4.34 The character area also contains The Cabbage Patch Public House which is located on London Road at the beginning of Railway Approach and acts as an entrance feature leading towards the site from the main road. It is part one and part two storeys in height, sitting in contrast to the much larger buildings that surround it along London Road.
- 4.35 The remaining location within this character area is the application site itself. Given the lack of any built form on the site, it presents an apparent void in the townscape and character of the wider area. Station Road and Station Yard generally comprise residential properties facing the railway line. However, the application site creates a break in this pattern and leaves the street one-sided.

Viewpoints

4.36 Four viewpoints have been identified based on the visibility of the site from various locations and where the proposed development will significantly alter the existing view. These are supplemented by additional views set out within the Design and Access Statement and which are considered alongside these views. The existing and proposed verified views are explored in further detail in Section 6 of this TVIA.

- View 1: Railway Approach looking northwest;
- View 2: Station Yard/Queen's Road looking northeast;
- View 3: Station Road looking northeast;
- View 4: London Road looking southwest.

5.0 PROPOSED DEVELOPMENT

5.1 The proposed development can be described as:

"Redevelopment of existing car park to provide a new building of 5 to 6 storeys, comprising 46 no. residential units (Use Class C3), disabled car parking, cycle parking, landscaping, enhancement to public realm and associated works."

5.2 The proposal involves clearance of the site and construction of a new residential building comprising 38 no. one-bed apartments and 8 no. two-bed apartments.

5.3 The development will comprise a single building rising to a maximum of 5 to 6 storeys. The building will be stepped back at 6th floor level to provide a shoulder height of 5 storeys.

Figure 17: Southern Elevation of Proposed Development



(Source: Wimshurst Pelleriti)

5.4 A total of 55 no. cycle parking spaces will be provided at ground floor level and accessed via the rear of the building adjacent to the railway line.

5.5 Landscape planting is proposed along the frontage of the building onto Station Yard to provide a soft edge to the street and complement the limited street trees already present.

5.6 Whilst not part of the proposals, it is understood that LBRuT are undertaking public realm improvements around the Albany Pub and Station Yard area which have informed the application proposals and formed a basis for the additional hard and soft landscape works proposed to further enhance the public realm.

5.7 It is proposed to rebuild a section of the railway boundary wall adjoining the site as part of the application proposals.

- 5.8 The design of the building contains both vertical and horizontal articulation to break up the form and provide a finer grain, responding to the local context. The overall approach is to provide a solid form, reflective of the railway heritage, using predominantly brick facades with complimentary use of recesses, detailing and metalwork.
- 5.9 The proposed building will be predominantly constructed using red/buff brick, reflecting the surrounding buildings and the former use of the site associated with the original Twickenham Railway Station. It will also incorporate a mix of materials including steel channels, glass, metal infill panels and balustrades.

6.0 IMPACT ASSESSMENT

- 6.1 This section assesses the impact on the Queen’s Road Conservation Area, the character areas and the viewpoints that have been identified due to their relationship to the application site. The assessment has been undertaken using the methodology set out in Section 2.0 and considers each individual area and view based on the magnitude, significance and nature of the impacts of the proposed development.
- 6.2 In order to assess the impact of the proposed development four verified views were commissioned from FoundationCGI. The methodology and results are included at Appendix A. The verified view locations are identified on the figure below. These verified views were also used to assess the additional views and illustrations presented in the Design and Access Statement prepared by Wimshurst Pelleriti which are also considered in this section and provide an understanding of the wider context.

Figure 18: Verified View Locations



(Source: FoundationCGI)

- 6.3 A range of views were considered in undertaking this assessment including those from within the conservation area on Grosvenor Road and Queens Road, but the development was not considered to be visible from within those locations.

Queen’s Road Conservation Area

- 6.4 The Queen’s Road Conservation Area and the Albany Pub have been key considerations throughout the progression of the planning submission as they are considered sensitive to current and future development proposals. There are no direct impacts on the conservation area as the development lies beyond its boundaries. Equally, there are no direct impacts on

national heritage assets, including listed buildings and scheduled ancient monuments, or on local buildings which are considered of townscape merit.

- 6.5 Hence the assessment of impacts focusses on indirect impacts arising from development in the wider setting of the conservation area.
- 6.6 The majority of the conservation area is located to the south and west and is generally formed by streets that are tightly enclosed by residential properties, thus containing its character and setting. The proposed development is considered to be largely screened from the majority of the conservation area and a sufficient distance from these assets to be unaffected.
- 6.7 The view from the northern edge of the conservation area towards the site is the key interface with the development. The conservation area appraisal notes this as an area where there is significant scope for enhancement and references the poor quality of the urban environment resulting from the TfL bus stand and open areas of car parking. Views are currently across these open areas and are dominated by Brewery Lane, Bridge House and the emerging Twickenham Station buildings. The site currently represents a gap in this view characterized by various elements of street clutter.
- 6.8 The proposed development aims to fill in the current void, enhancing the view from the conservation area, by completing an element of this streetscape and helping to frame the residential environment of Station Yard. By enclosing the northern edge of the street it helps to create a more appropriate residential street pattern.
- 6.9 This is reinforced through the use of consistent footway materials and the completion of the pavement crossover at the end of Mary's Terrace and extending towards the junction with Station Road. Consistent hard and soft landscaping is proposed to connect with the Council's realignment of Station Road and Station Yard junction, creating footways and an enhanced pedestrian environment outside the Albany. Additional street trees are proposed at the end of Mary's Terrace to supplement those proposed by the Council outside the Albany.
- 6.10 The scale of the development aims to mediate between the taller buildings of London Road to the east and the smaller 2, 3 and 4 storey scale of those extending south and west. The height of the building at 6 storeys is set just below that of Bridge House, which frames Station Yard and Railway Approach to the east and the proposals adopt a set back top floor so that the building appears as 5 storeys to the shoulder when viewed from Station Yard and the immediate surroundings south and west. The form is further broken up using brick reveals and set backs, with inset balconies to give the building a human scale.

- 6.11 Consequently, although there is no overall impact on the conservation area itself, the proposed development is considered to have a positive impact on the wider setting of the northern edge of the Queen's Road Conservation Area and the Albany Pub.

Assessment of effects

Magnitude: Moderate

Significance: Moderate

Nature: Positive

Brewery Lane Character Area

- 6.12 The Brewery Lane development sits directly opposite the site to the north. It is separated by the high railway boundary wall which creates a strong division between the two sites. The character area as a whole is bound by the railway to the south, River Crane to the north, London Road to the east and an area of dense trees to the west. Due to these distinct boundaries, the character and setting of the area is contained and therefore unlikely to be significantly impacted by the development proposals.
- 6.13 The view towards the site from within the Brewery Lane development is largely blocked by the railway fence as shown in Figure 8. The site is more easily visible from the car park and entrance off London Road as there is an upward slope in the topography at this point. The Albany Pub is clearly visible (see Figure 9) and residential properties along Station Yard.
- 6.14 The proposed development will alter the view but will reflect to a large degree the approach of Brewery Yard, which presents a taller more substantial form facing London Road and steps down to the west. The Albany Pub will remain clearly visible, remaining a landmark building from this viewpoint and ensuring this key view of the conservation area is retained.
- 6.15 Whilst the proposed development will be well separated from the character area by the railway line, the overall design and use of brickwork is considered to strongly reflect the Brewery Lane buildings, retaining the heritage-led approach within the area and continuing to reference the former railway buildings. It also remains in keeping with the residential uses in this area to the west of London Road.
- 6.16 Overall, it is considered that the Brewery Lane Character Area will not be affected by the development proposals and will retain its character and setting.

Assessment of effects

Magnitude: Low

Significance: Low

Nature: Positive

Twickenham Station and London Road Character Area

- 6.17 The Twickenham Station and London Road Character Area generally comprises a mix of larger-scale buildings with varied architectural styles and a mix of uses, with the exception of Nos. 1-16 Mary's Terrace. This area is largely dominated by views of Regal House, the tallest building in the area which incorporates a much different architectural style than the typical brick buildings within its surroundings.
- 6.18 Whilst the site is highly visible from the bridge on London Road, it is easy to go unnoticed as it forms part of the void between Bridge House and The Exchange. In this respect the site does not contribute to the townscape and in some senses detracts from this character area, forming part of its immediate surroundings which comprise a mix of hardstanding, car parking and the railway line.
- 6.19 Due to the mixed nature and scale of buildings surrounding the station and along London Road, and changing nature of the area, this character area is not considered to be particularly sensitive to future neighbouring development.
- 6.20 The redevelopment of Twickenham Station will partly fill in the area to the east of London Road, leaving the west side underdeveloped. The proposed development would partly fill in this gap, completing the linear pattern along London Road and contributing to the cluster of larger buildings in this area. While the scale is smaller than the adjoining buildings of Bridge House and The Exchange, the form proposed relates to both these and the smaller scale to the east.
- 6.21 Locally Listed Nos. 1-16 Mary's Terrace represent a significant variation in terms of the surrounding architectural styles. However these properties are largely obscured from view by surrounding large-scale development and are only visible from Mary's Terrace itself or from the pedestrian footbridge at Twickenham Station. Whilst geographically they are within this character area, they relate more appropriately to the residential area to the east. They are also located a substantial distance from the application site and are segregated from the rest of the character area by the London Road bridge and railway line. The development would therefore not have any impact on their setting.

Assessment of effects

Magnitude: Major

Significance: Neutral

Nature: Positive

Railway Approach, Station Yard and Surroundings Character Area

- 6.22 The site lies within this character area and therefore the proposals have greater potential to impact significantly on the character and nature of the area.
- 6.23 The area is predominantly residential in nature and generally comprises 3 storey houses fronting Railway Approach and Station Yard with the exception of the 1-2 storey Cabbage Patch Pub. The properties fronting Station Yard and Railway Approach are of modern 20th century origin with 3 storey brick facades and pitched roofs. These face onto the development site, which would form the opposing street edge and help frame the street pattern, marking the junction with the western end of Mary's Terrace towards the junction with Station Road.
- 6.24 The site at present is in conflict with the rest of the area as it presents a noticeable void in the townscape which generally follows a traditional residential street pattern. Consideration has therefore been given to the existing pattern and architectural form within this area to determine the potential impacts of the development proposal.
- 6.25 The proposed development represents a modern addition to the townscape, using materials that reflect the surrounding buildings and drawing on the difference brick tones of the railway heritage.
- 6.26 It would fill part of the existing void, allowing Station Yard to better reflect the surrounding residential street pattern and frame views within the character area. It would provide an appropriate end point to views along Railway Approach, Station Road, Mary's Terrace and the northern end of Queens Road, looking towards Bridge House.
- 6.27 The proposal would present a new view from the locally listed Cabbage Patch Pub. The change in this view is considered positive as the new building will provide a sense of enclosure within the character area and relate to its residential surroundings.

Assessment of effects

Magnitude: Major

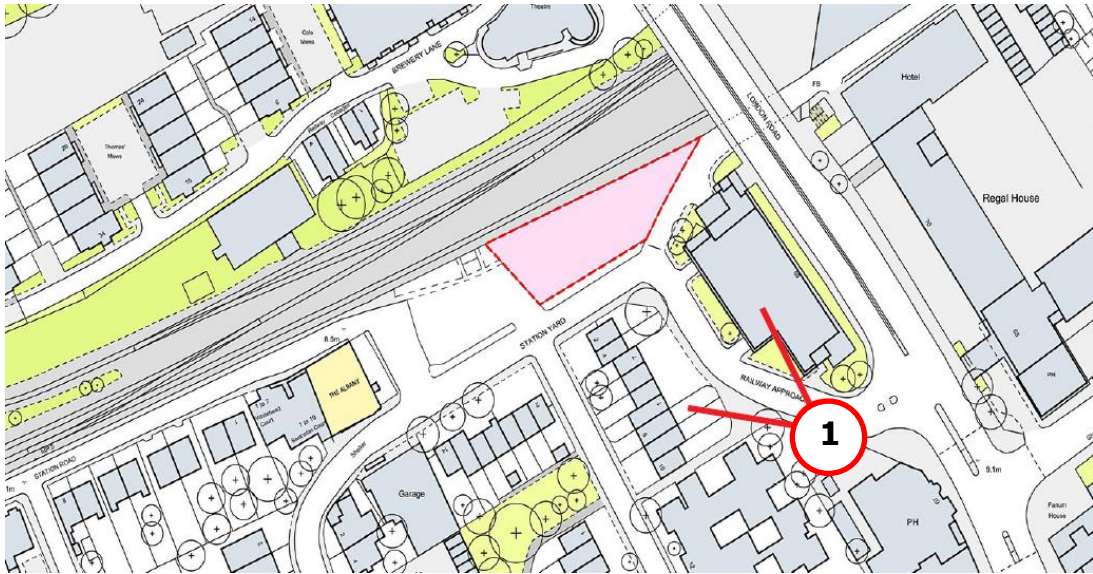
Significance: Moderate

Nature: Positive

Viewpoints

View 1: Railway Approach looking northwest

Figure 19: Map showing location of View 1



(Source: Wimshurst Pelleriti)

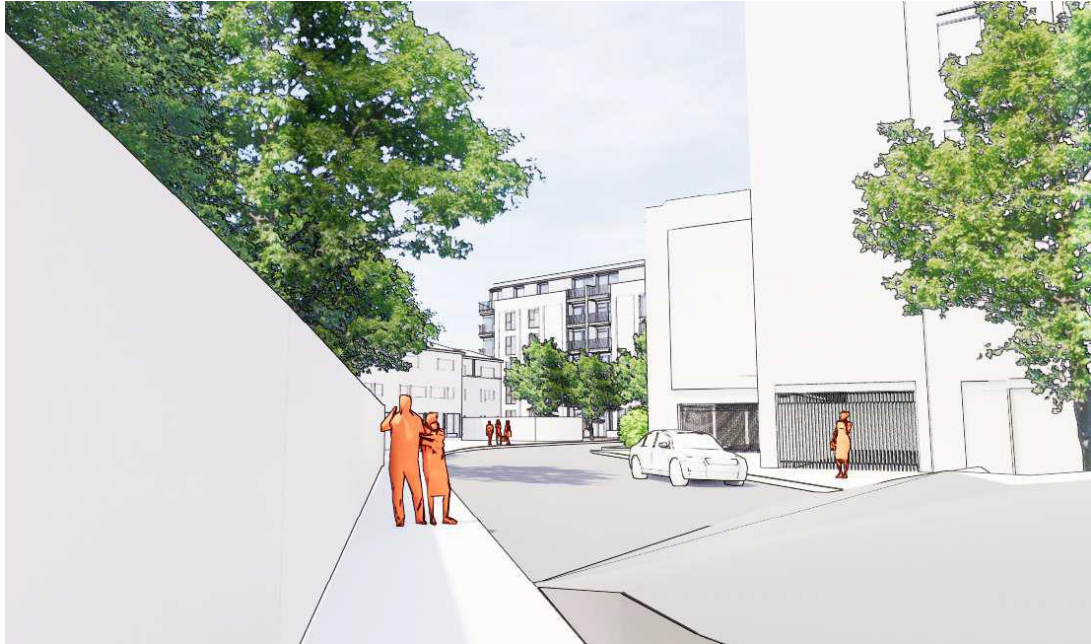
Figure 20: Existing View 1



(Source: FoundationCGI)

6.28 The existing view is dominated by Bridge House to the east. The site is relatively indistinguishable from the surrounding roads and pavements from this viewpoint.

Figure 21: Proposed View 1



(Source: Wimshurst Pelleriti)

Figure 22: Verified View of View 1



(Source: FoundationCGI)

- 6.29 The proposed view remains dominated by Bridge House to the east. The proposed development provides a sense of enclosure and its orientation naturally leads one from the end of Railway Approach to the west along Station Yard.
- 6.30 The proposed building height presents an appropriate stepping down between Bridge House and the residential properties to the west.

Assessment of effects

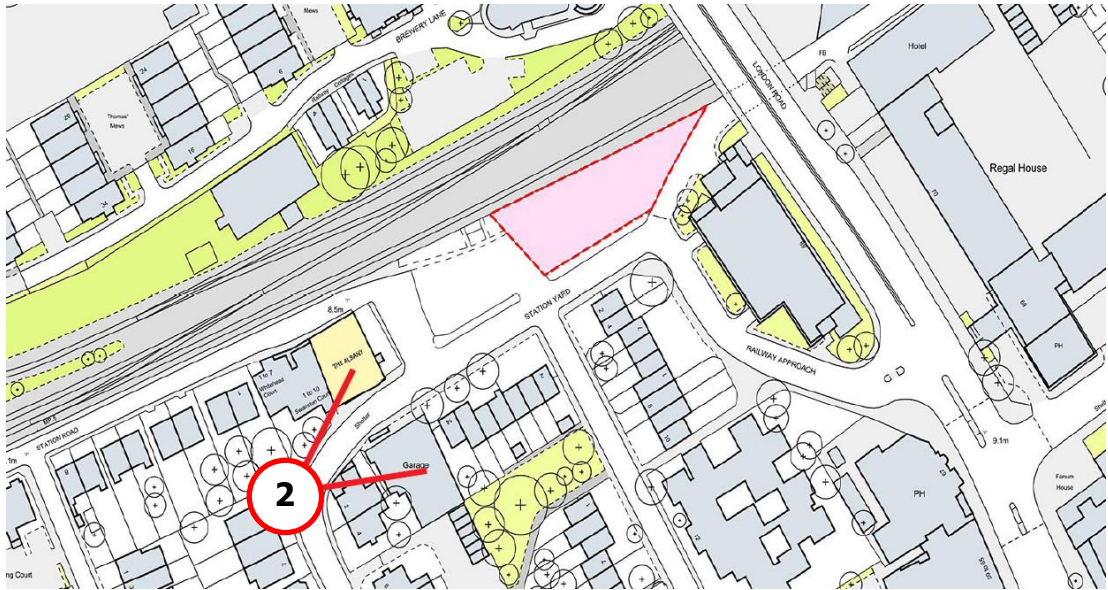
Magnitude: Major

Significance: Moderate

Nature: Positive

View 2: Station Yard/Queen's Road looking northeast

Figure 23: Map showing location of View 2



(Source: Wimshurst Pelleriti)

Figure 24: Existing View 2



(Source: FoundationCGI)

- 6.31 The existing view is enclosed to the north and south by the Albany Pub and a commercial building respectively. Looking towards the site, the view appears empty and lacks any character, buildings or distinguishing features. Bridge House is clearly visible in the distance,

however not dominant from this perspective. The townscape is an unorganised mixture of houses to the south, with hardstanding and fencing to the north.

Figure 25: Proposed View 2



(Source: Wimshurst Pelleriti)

Figure 26: Verified View of View 2



(Source: FoundationCGI)

- 6.32 The proposed view presents a legible and distinct street pattern with the proposed building visually prominent in the distance. Importantly, the Albany Pub remains a key feature in the foreground and acts as an entry point from Queen's Road.

Assessment of effects

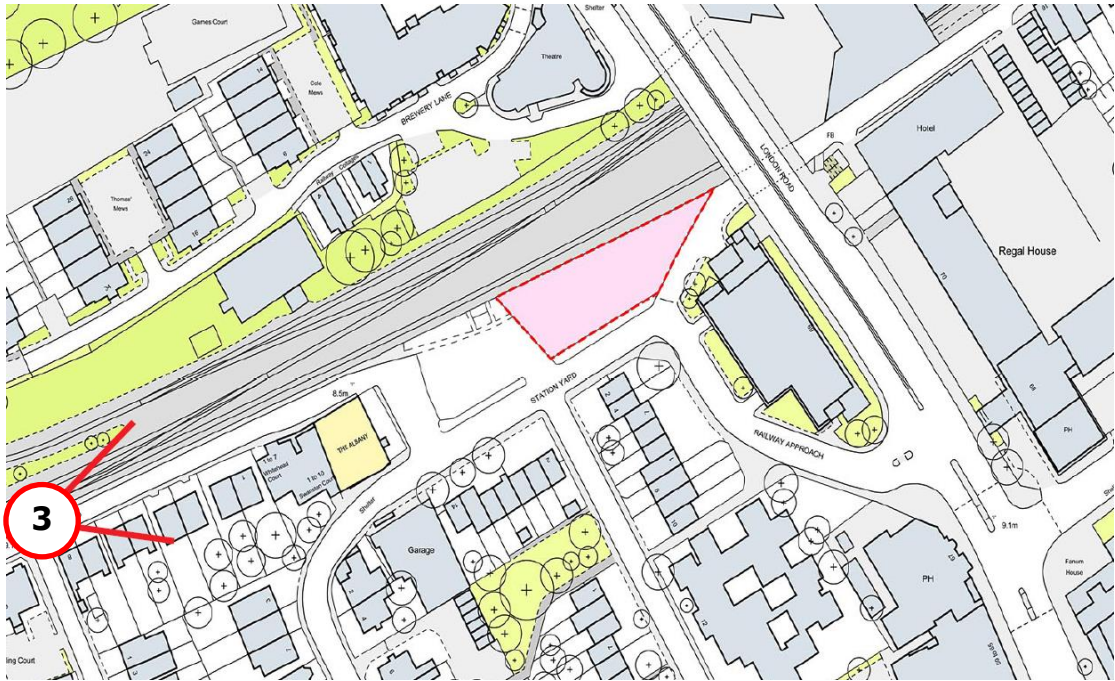
Magnitude: Major

Significance: Moderate

Nature: Positive

View 3: Station Road looking northeast

Figure 27: Map showing location of View 3



(Source: Wimshurst Pelleriti)

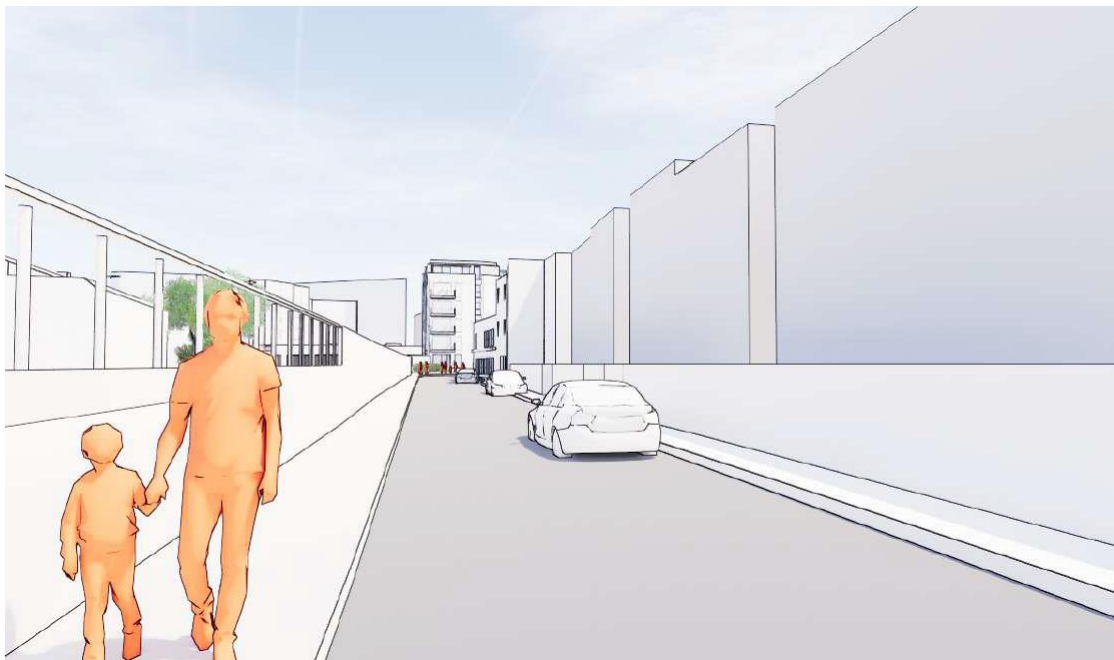
Figure 28: Existing View 3



(Source: FoundationCGI)

- 6.33 The existing view is enclosed peripherally to the north and south by the railway boundary and Albany pub respectively. The railway boundary runs through the centre of the view, with the Brewery Lane development forming the main view to the northeast and Bridge House forming the main view to the east. Regal House is also visible in the distance beyond Bridge House.
- 6.34 There is a noticeable gap between Brewery Lane and Bridge House where no other buildings are present. The London Road bridge over the railway sits centrally within this view, sitting much lower than the adjacent buildings.

Figure 29: Proposed View 3



(Source: Wimshurst Pelleriti)

Figure 30: Verified View of View 3



(Source: FoundationCGI)

- 6.35 The proposed view closes part of the gap between Brewery Lane and Bridge House, with the remainder of the gap closed by the future Twickenham Station development. This provides a clear distinction between the immediate residential area and the larger commercial area beyond.

Assessment of effects

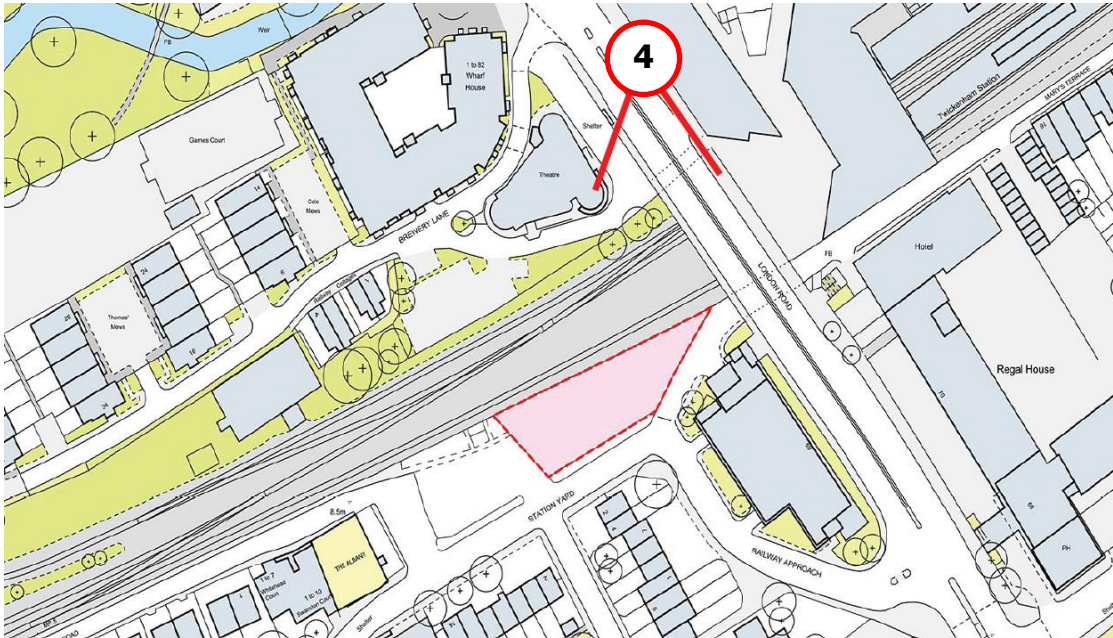
Magnitude: Moderate

Significance: Neutral

Nature: Positive

View 4: London Road looking southwest

Figure 31: Map showing location of View 4



(Source: Wimshurst Pelleriti)

Figure 32: Existing View 4



(Source: FoundationCGI)

6.36 Once again, there is a noticeable gap between Brewery Lane and Bridge House with three-storey residential buildings visible centrally in the distance. The London Road bridge is prominent in the foreground, acting as a connecting point between Brewery Lane and Bridge House.

Figure 33: Proposed View 4



(Source: Wimshurst Pelleriti)

Figure 34: Verified View of View 4



(Source: FoundationCGI)

- 6.37 The proposed development represents a significant change in the view towards the site from London Road, clearly filling in a substantial void in the townscape.
- 6.38 The building becomes a new focal point from London Road, situated centrally within the view. In comparison to The Exchange building on Brewery Lane and Bridge House, the scale and mass of the building appears moderate and the building is considered to sit appropriately within its surroundings.

Assessment of effects

Magnitude: Major

Significance: Moderate

Nature: Positive

7.0 SUMMARY AND CONCLUSIONS

- 7.1 This Townscape and Visual Impact Assessment is provided to assess the likely impacts of the proposed development on the surrounding visual amenity and townscape character. It has considered the proposed development in relation to surrounding buildings, heritage designations, character areas, viewpoints, emerging developments and any other relevant features in the area, including architectural styles and materials. It has drawn on information set out in the Heritage and Archaeology Assessment, Design and Access Statement, Landscape Statement and application plans and reports.
- 7.2 The proposals are for the redevelopment of the application site to provide a 5 to 6 storey building comprising 46 no. residential units, disabled parking, cycle parking, landscaping, public realm enhancements and associated works.
- 7.3 Overall, the TVIA has determined that the proposed development represents a moderate magnitude of change on the identified views, townscape and public realm. The development will help mend the current void in the townscape, reframe the street and will substantially improve the character and appearance of the area around the site and views within Station Yard, London Road, Railway Approach and Station Road. The significance impact is considered to be moderate in an area that can accommodate change and the nature of the impacts were found to be positive.
- 7.4 In conclusion, it is considered that the proposals will successfully integrate within the area and surroundings and provide positive enhancements to the landscape and visual amenity of the area. Furthermore, the development will not adversely impact upon any statutory or locally designated heritage assets. Consequently, the development meets the requirements of national, regional and local planning policy.

Appendix A: Verified Views Methodology Statement

1. VERIFIED VIEW VIEWPOINT SELECTION AND TYPE

- 1.1 Proposed viewpoints were identified by Union4 Planning and Foundation CGI used these as guide to find suitable positions located on site.
- 1.2 Verified View Level 0 was chosen to give the assessors a meaningful representation of the proposed.

2. PHOTOGRAPHY

- 2.1 The photography was undertaken on the 16th September 2019 using a Canon 5D Mark IV with a Canon 24-70mm f/2.8L II USM Zoom Lens.
- 2.2 The height to the centre of the lens was recorded for each view
- 2.3 A plumb line was hung from the lens's entrance pupil. The position on the ground was surveyed and recorded.
- 2.4 The camera was targeted at the centre of the proposed development, unless otherwise stated.
- 2.5 The photograph's lens distortion is removed using the Adobe Lens profile for the lens used.
- 2.6 The surveyor surveyed the camera locations and key points within each photograph. These are delivered as 3D CAD point cloud along with a table of their coordinates. All coordinates relate to the OS grid & datum via GPS observations.

3. 3D MODELLING

- 3.1 A 3D computer model was supplied by Wimshurst Pelleriti Architects.

4. VERIFICATION PROCESS

- 4.1 The surveyed camera positions and key points were imported into our 3D software.
- 4.2 To prevent numerical and display inaccuracies the surveyors coordinate system was moved so that the coordinates for "Position 1" (516086.249E 173669.797N) became our new origin (0,0 in the 3D software's x and y axes).
- 4.3 Wimshurst Pelleriti's 3D model of the proposed development was aligned to the surveyor's coordinate system using the CAD information supplied by Wimshurst Pelleriti Architects.
- 4.4 For each view a virtual camera is created in our 3D program using the camera's position supplied by the surveyor and the initial focal length is set using the data recorded in the photograph's metadata.
- 4.5 For each camera the camera's target, roll and focal length is refined so that the surveyed points in the 3D scene aligned with the corresponding points in the photograph.

5. IMAGE PRODUCTION

- 5.1 Buildings with a similar orientation to the proposed development within the photography were analysed to provide a reference of how the proposed should be lit.
- 5.2 Computer renders of each view are combined with the photographs using Adobe Photoshop.
- 5.3 The combined image is analysed to determine which elements of the proposed development will be visible and masking is applied to remove elements that would be hidden behind the context.
- 5.4 Further analysis of the photography provides us with tonal values to hue, saturation and brightness of the computer renders to set them into the photograph.
- 5.5 Finally any camera roll is removed to provide a horizontal horizon and converging verticals are minimised.

6. VERIFIED VIEW LIMITATIONS

- 6.1 Verified Views are intended to be used alongside site based assessment.
- 6.2 Whilst the position and scale of the proposed development within the photography is of verifiable accuracy, assuming accurate survey data, there is currently no way of depicting every eventuality that may affect the proposed development. Lighting, weather & atmospheric conditions along with the ageing of materials can all affect a viewers perception of the proposed. Therefore where the Verified View depicts proposed materials Foundation CGI worked with the Architect to ensure these materials are shown as intended under the lighting conditions shown in the photograph. This is a subjective, non-verifiable process.

7. GLOSSARY

- 7.1 Photomontage: The combination of a computer render of a 3D model with site photography. The computer render will typically have masking applied to remove elements that would be hidden behind any context.

- 7.2 Computer render: The 2D representational output from a 3D software application.

8. VERIFIED VIEWS PREPARED BY:

Foundation CGI
Studio 13, Claremont Business Centre
6-8 Claremont Road
Surbiton
KT6 4QU
Tel: 020 8549 3355

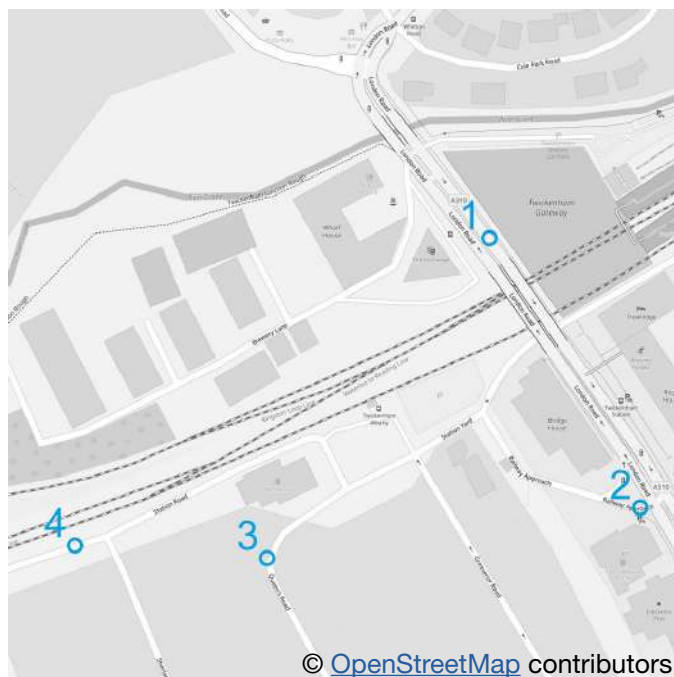
Position 1: A310 - London Road



Existing View

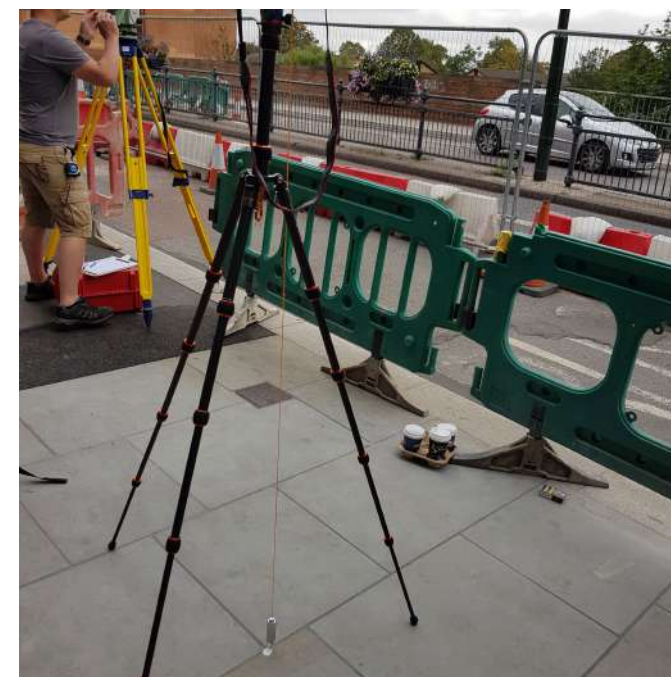


Verified Camera View: Annotated key points (red arrows) and their corresponding survey points (green crosses) seen through the re-created virtual camera demonstrating a verified view.



Viewpoint Location

National Grid Reference: 516086.249E 173669.797N
Height of Camera Lens: 14.595m



Viewpoint Information

Height of Camera (above ground): 1690mm
Ground Marking: Surveyed in situ
Date Taken: 16th September 2019
Time Taken: 11:47am
Focal Length: 50.8mm
Field of View: 39°
Verified View Type: Level 0

Position 1: A310 - London Road



Existing View

Position 1: A310 - London Road



Proposed View

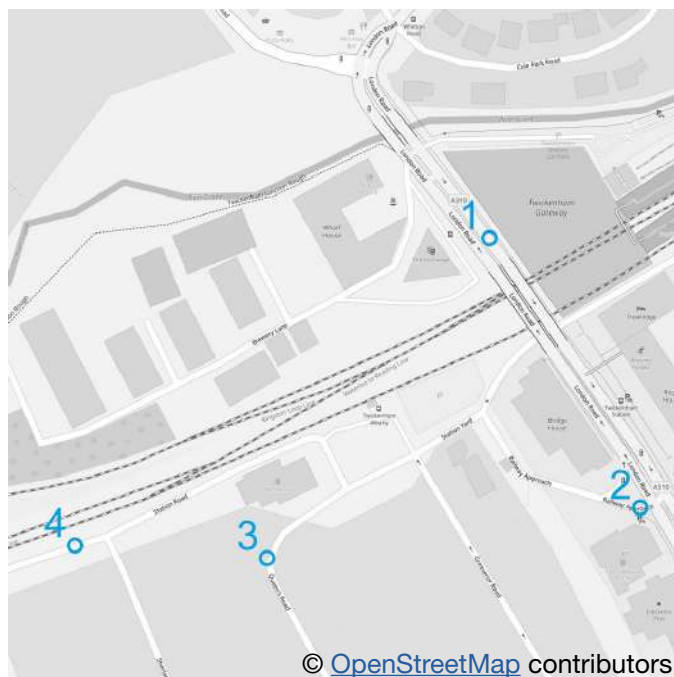
Position 2: Railway Approach



Existing View



Verified Camera View: Annotated key points (red arrows) and their corresponding survey points (green crosses) seen through the re-created virtual camera demonstrating a verified view.



Viewpoint Location

National Grid Reference: 516154.731E 173540.322N
Height of Camera Lens: 10.870m



Viewpoint Information

Height of Camera (above ground): 1685mm
Ground Marking: Surveyed in situ
Date Taken: 16th September 2019
Time Taken: 12:08pm
Focal Length: 49.5mm
Field of View: 39.6°
Verified View Type: Level 0

Position 2: Railway Approach



Existing View

Position 2: Railway Approach



Proposed View

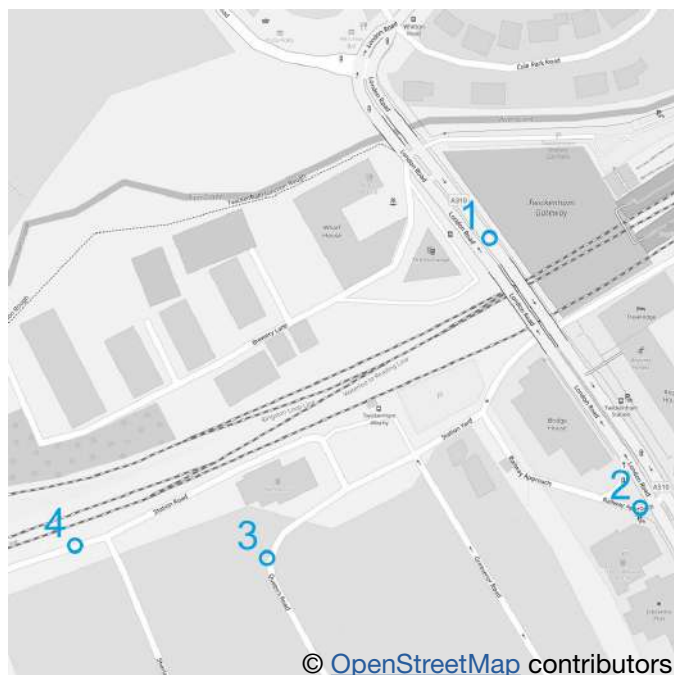
Position 3: Queen's Road



Existing View



Verified Camera View: Annotated key points (red arrows) and their corresponding survey points (green crosses) seen through the re-created virtual camera demonstrating a verified view.



Viewpoint Location

National Grid Reference: 515977.665E 173526.571N
Height of Camera Lens: 10.368m



Viewpoint Information

Height of Camera (above ground): 1670mm
Ground Marking: Surveyed in situ
Date Taken: 16th September 2019
Time Taken: 13:02pm
Focal Length: 49.6mm
Field of View: 39.9°
Verified View Type: Level 0

Position 3: Queen's Road



Existing View

Position 3: Queen's Road



Proposed View

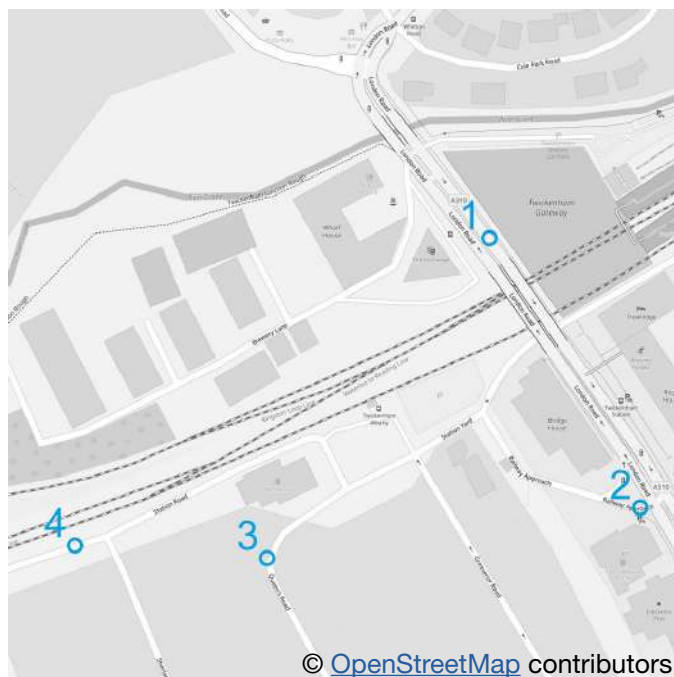
Position 4: Station Road



Existing View



Verified Camera View: Annotated key points (red arrows) and their corresponding survey points (green crosses) seen through the re-created virtual camera demonstrating a verified view.



Viewpoint Location

National Grid Reference: 515901.109E 173539.011 N
Height of Camera Lens: 10.729m



Viewpoint Information

Height of Camera (above ground): 1675mm
Ground Marking: Surveyed in situ
Date Taken: 16th September 2019
Time Taken: 10:40am
Focal Length: 49.4mm
Field of View: 39.63°
Verified View Type: Level 0

Position 4: Station Road



Existing View

Position 4: Station Road



Proposed View