

MALTINGS PLAZA

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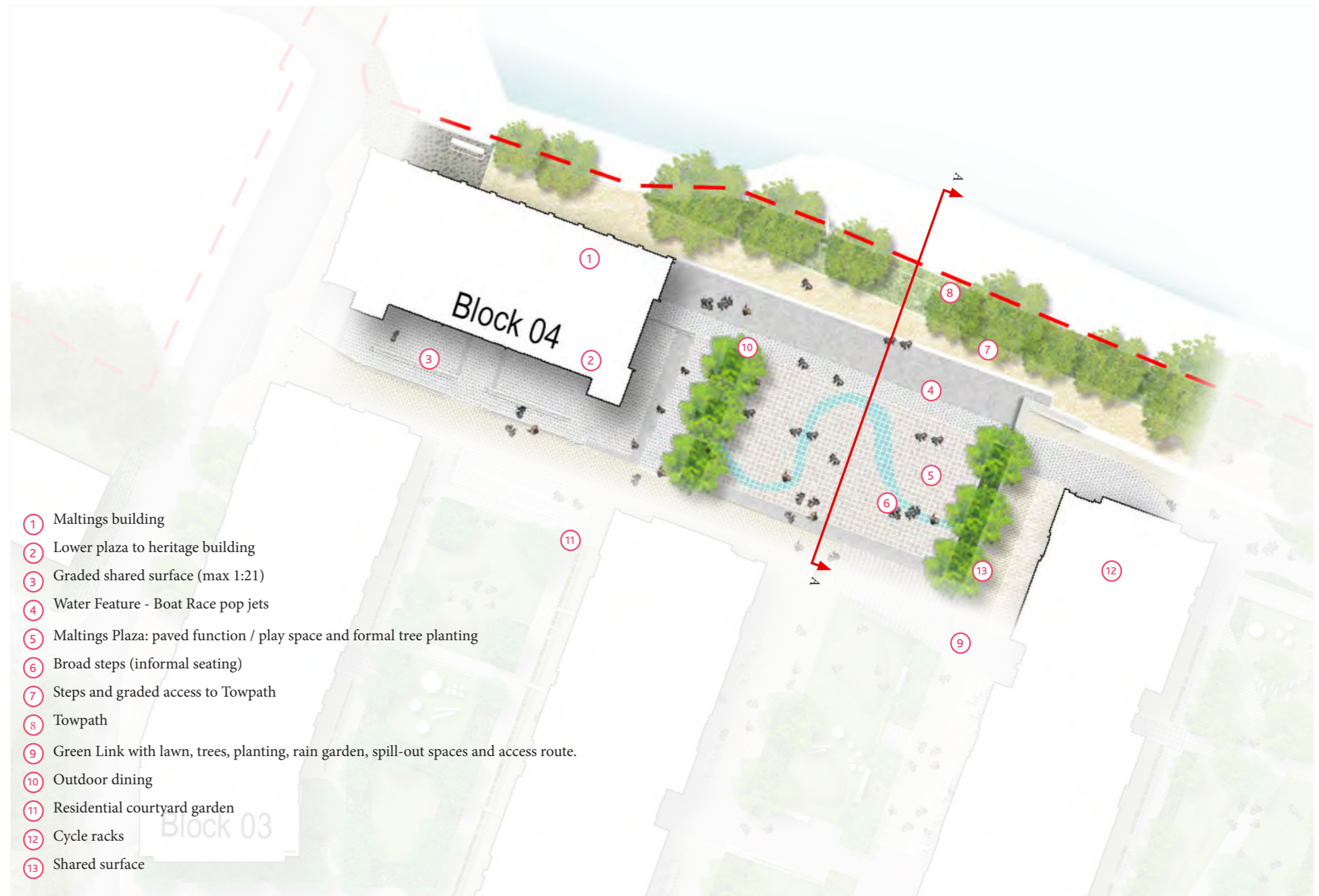
This more formal space forms the main public open space adjacent to the River Thames. Maltings Plaza supports a variety of community-based and retail activities as well as recreational functions. The plaza is primarily hard paved to cater for a range of functions at various times of the year with a water feature proposed within the space. The proposed water feature consists of a series of pop-jet fountains along a stone etched segment of the River Thames, relating to the famous boat race between the University's of Cambridge and Oxford which finishes adjacent to the site. This feature will assist in activating the space and provide a desirable attraction for children and adults alike, with the potential to create a range of effects day and night. Alternatively, the feature can be closed down, allowing a full range of other activities to occur in the paved plaza.

Views to the historic Boat Race finish line, adjacent to this end of the site can be facilitated within the new Maltings Plaza with temporary seating, together with other seasonal community activities and regular markets or displays.

A number of level changes are facilitated within the design of the square, providing separation of various function areas and accessible connection to the Towpath and river edge with a combined stepped and graded access. Connection to the Maltings Building and various function areas and access points have also been incorporated.



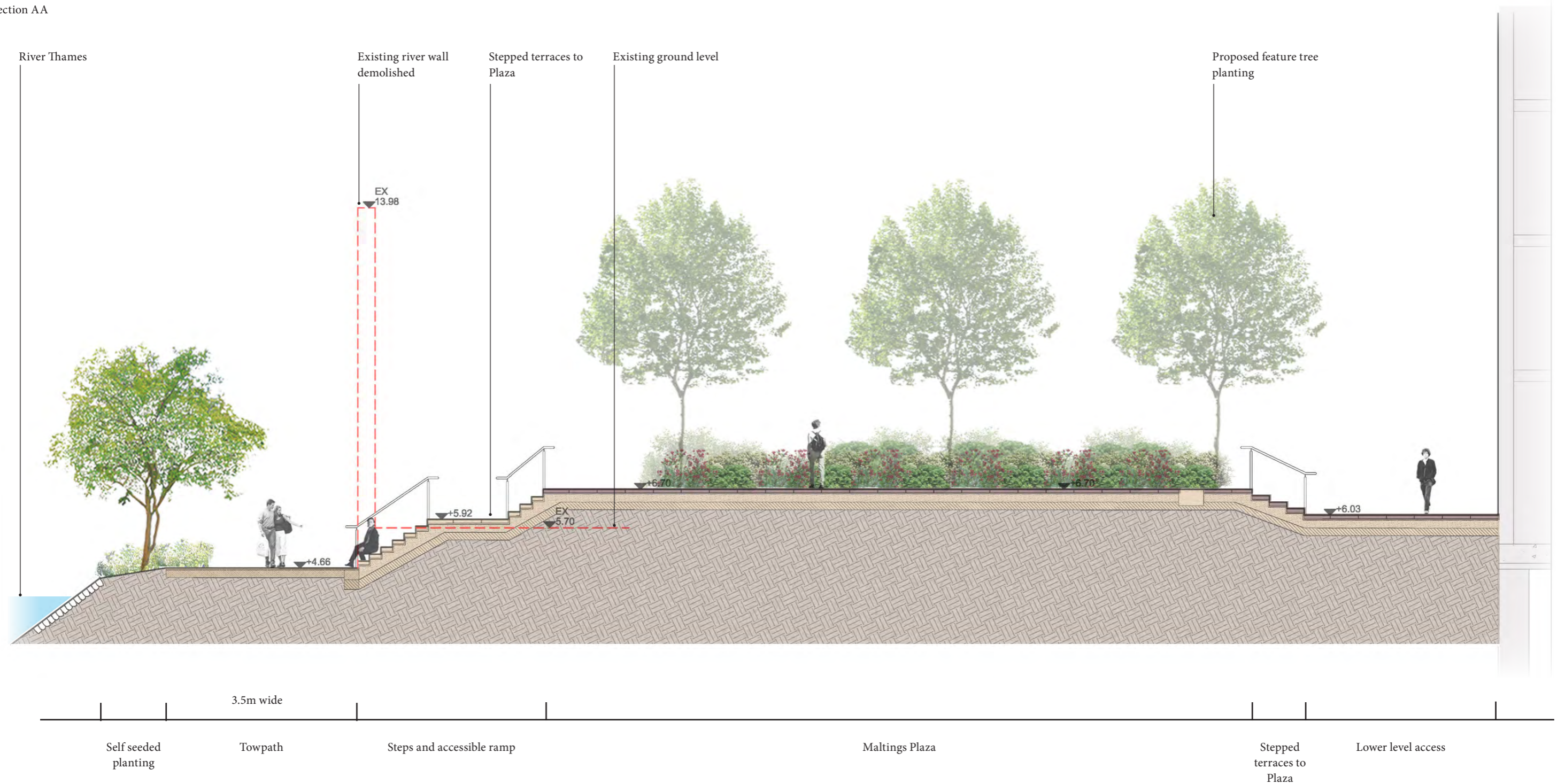
Location plan



- ① Maltings building
- ② Lower plaza to heritage building
- ③ Graded shared surface (max 1:21)
- ④ Water Feature - Boat Race pop jets
- ⑤ Maltings Plaza: paved function / play space and formal tree planting
- ⑥ Broad steps (informal seating)
- ⑦ Steps and graded access to Towpath
- ⑧ Towpath
- ⑨ Green Link with lawn, trees, planting, rain garden, spill-out spaces and access route.
- ⑩ Outdoor dining
- ⑪ Residential courtyard garden
- ⑫ Cycle racks
- ⑬ Shared surface

MALTINGS PLAZA

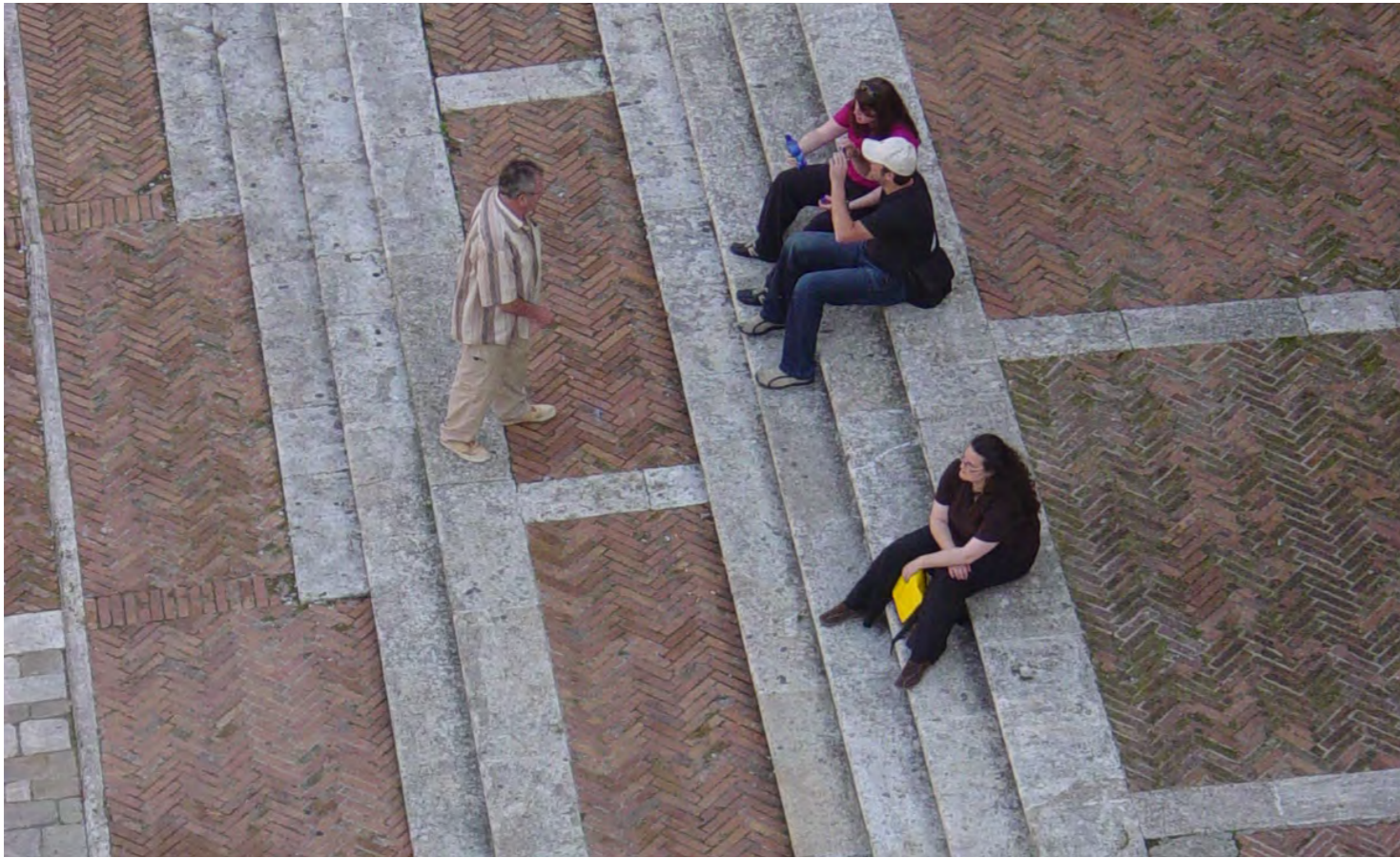
Section AA



MALTINGS PLAZA LOOKING SOUTH - ILLUSTRATIVE VIEW



MALTINGS PLAZA - PRECEDENT IMAGES



RIVERSIDE TERRACE

RIVERSIDE TERRACE

This public open space along the riverfront forms a connective link along the site frontage at the higher site level, creating an alternative access parallel with the Towpath.

- ① River terrace walk with feature paving band
- ② Riparian planting
- ③ Towpath resin bound gravel footpath
- ④ Steps to Towpath
- ⑤ Existing granite setts retained
- ⑥ Seating
- ⑦ Pruning of understorey vegetation to open key views
- ⑧ Life-saving equipment - locate as directed
- ⑨ Rediscovered railway track - express within new pavement
- ⑩ Courtyard garden
- ⑪ Existing Public Draw Dock
- ⑫ Maltings Plaza
- ⑬ Steps and slopes to Towpath
- ⑭ Retaining part of boundary wall with historical value
- ⑮ Existing slipway
- - - Site application boundary

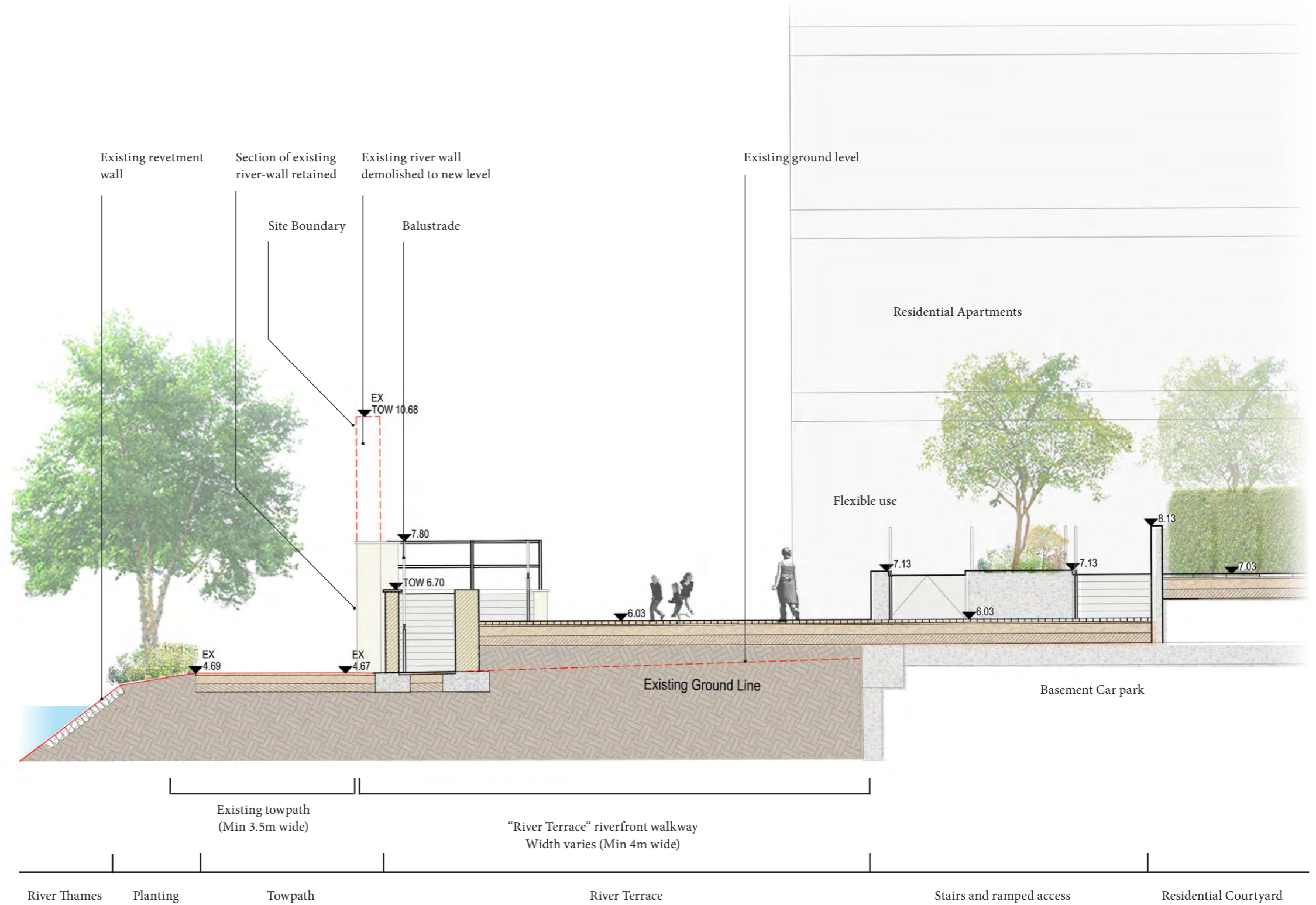


Location plan



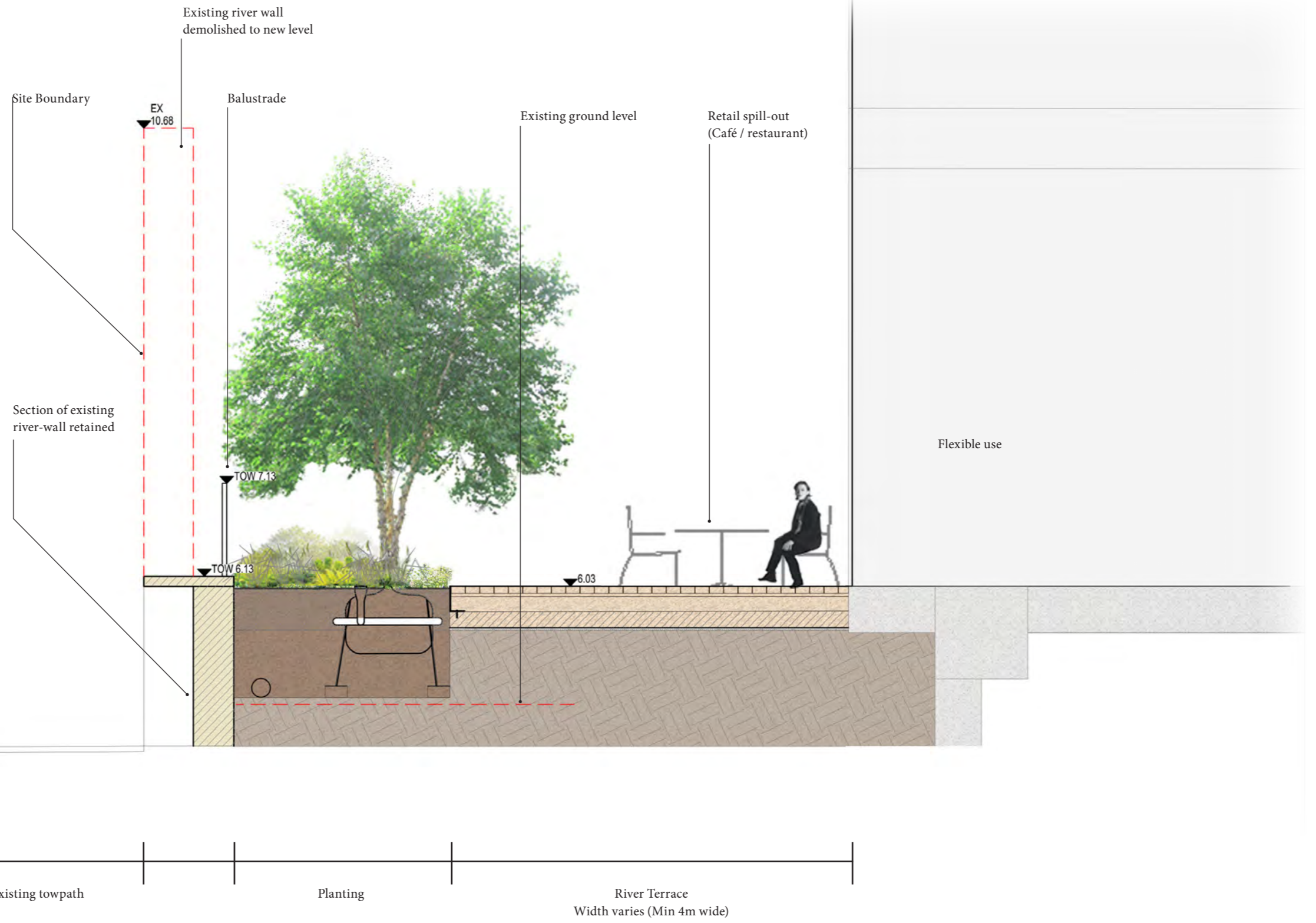
RIVERSIDE TERRACE

Section AA

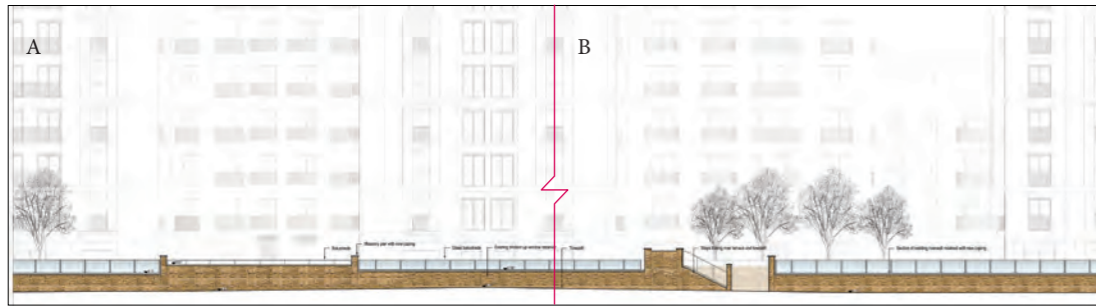


RIVERSIDE TERRACE

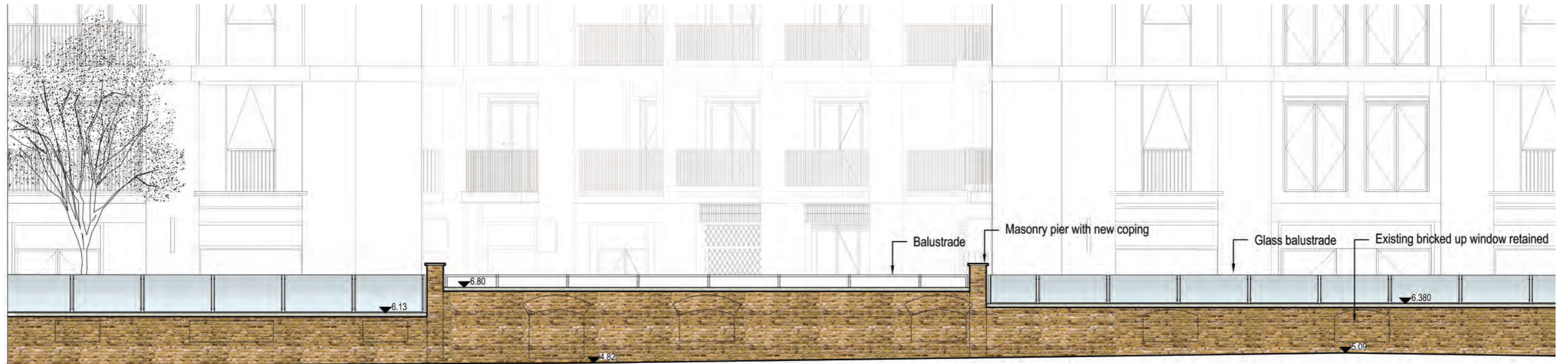
Section BB



RIVERSIDE TERRACE - EXISTING WALL RETAINED ELEVATION VIEW FROM TOWPATH A-A



Location plan



A

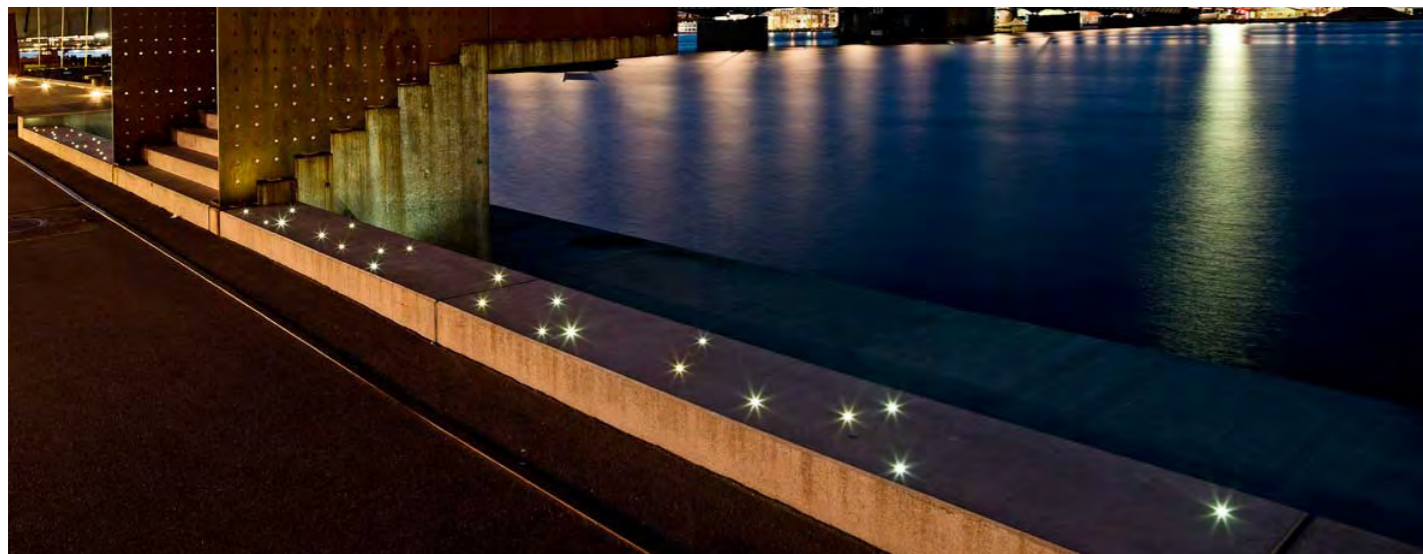
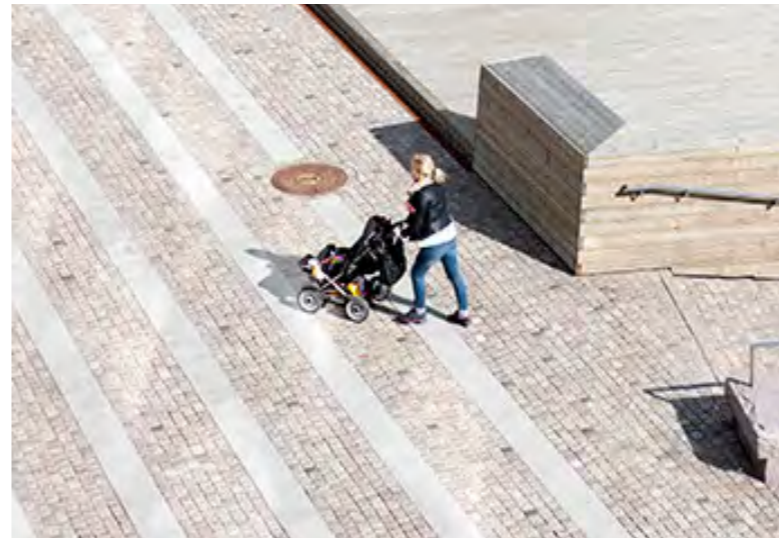


B

RIVERSIDE TERRACE - ILLUSTRATIVE VIEW



RIVERSIDE TERRACE PRECEDENT IMAGES

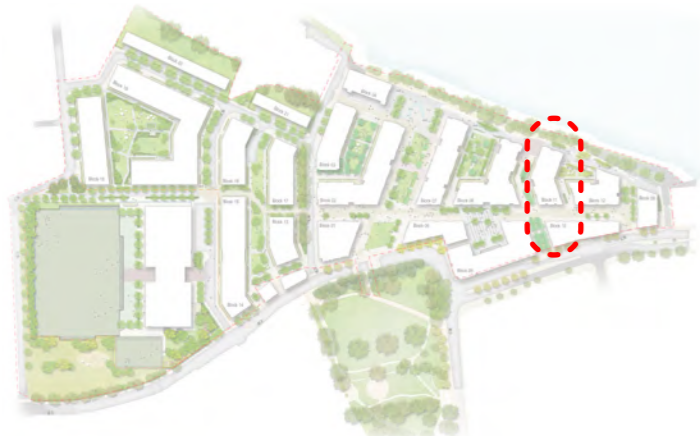


RESIDENTIAL STREETS

RESIDENTIAL STREETS

The more traditional spaces between buildings offer public open space, planting and seating opportunities as well as catering for access, and circulation of service and emergency vehicles, pedestrians and cyclists.

The streets allow for visual links through the site to the river and feature trees and understorey planting provide softening, seasonal colour and interest within the streetscape.



Location plan - Residential Street Example

RESIDENTIAL STREETS

Section AA



LOWER RICHMOND ROAD

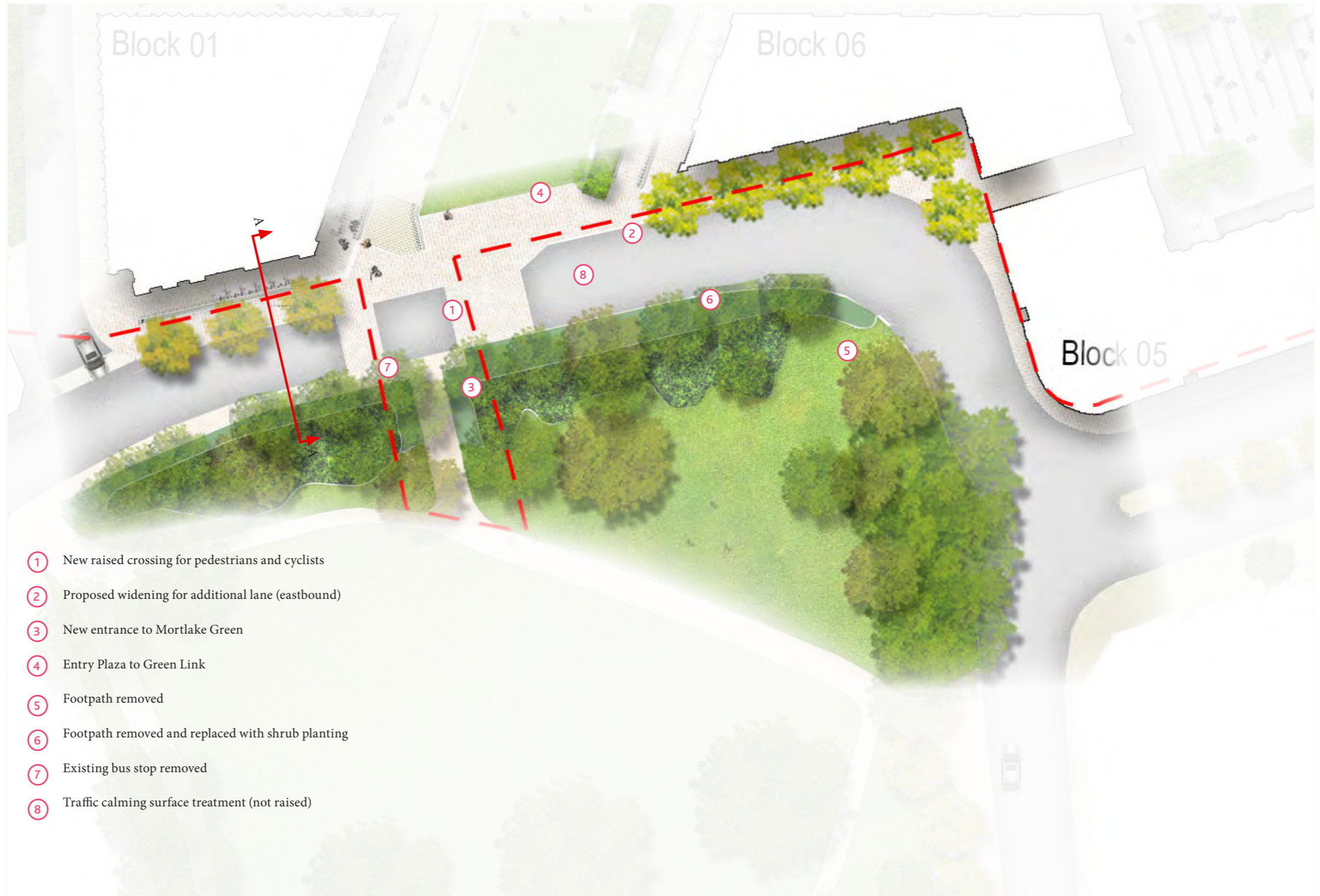
CONNECTION TO MORTLAKE GREEN

The major pedestrian connection from Mortlake to the site occurs at the Green Link entrance across from Mortlake Green. A raised pedestrian / cycle crossing is proposed, aligning with the western pedestrian pathway within the Green Link and entrance to the proposed Cinema complex.

The proposed connection across Lower Richmond Road into the park will connect with the existing pathway network. Some selective removal of shrubs and pruning will be necessary, although no existing trees will be affected in these works. Special pavement treatment will be specified for the pathway within the Root Protection Zone of the existing trees and this will be constructed in resin bound gravel or similar material to restrict excavation for the new pavement.

Additional understorey planting either side of the new path will be provided to integrate the path with the existing parkland. New planting is also proposed to replace the existing pathway along the edge of Lower Richmond Road towards the roundabout, to dissuade pedestrians attempting to cross at locations away from the safe crossing point.

Future works in Mortlake Green to extend and upgrade this pathway in order to enable the safe sharing of the space with cyclists has been discussed in detail with LBRuT and it is envisaged council will integrate these works in their planned general upgrade of the park.



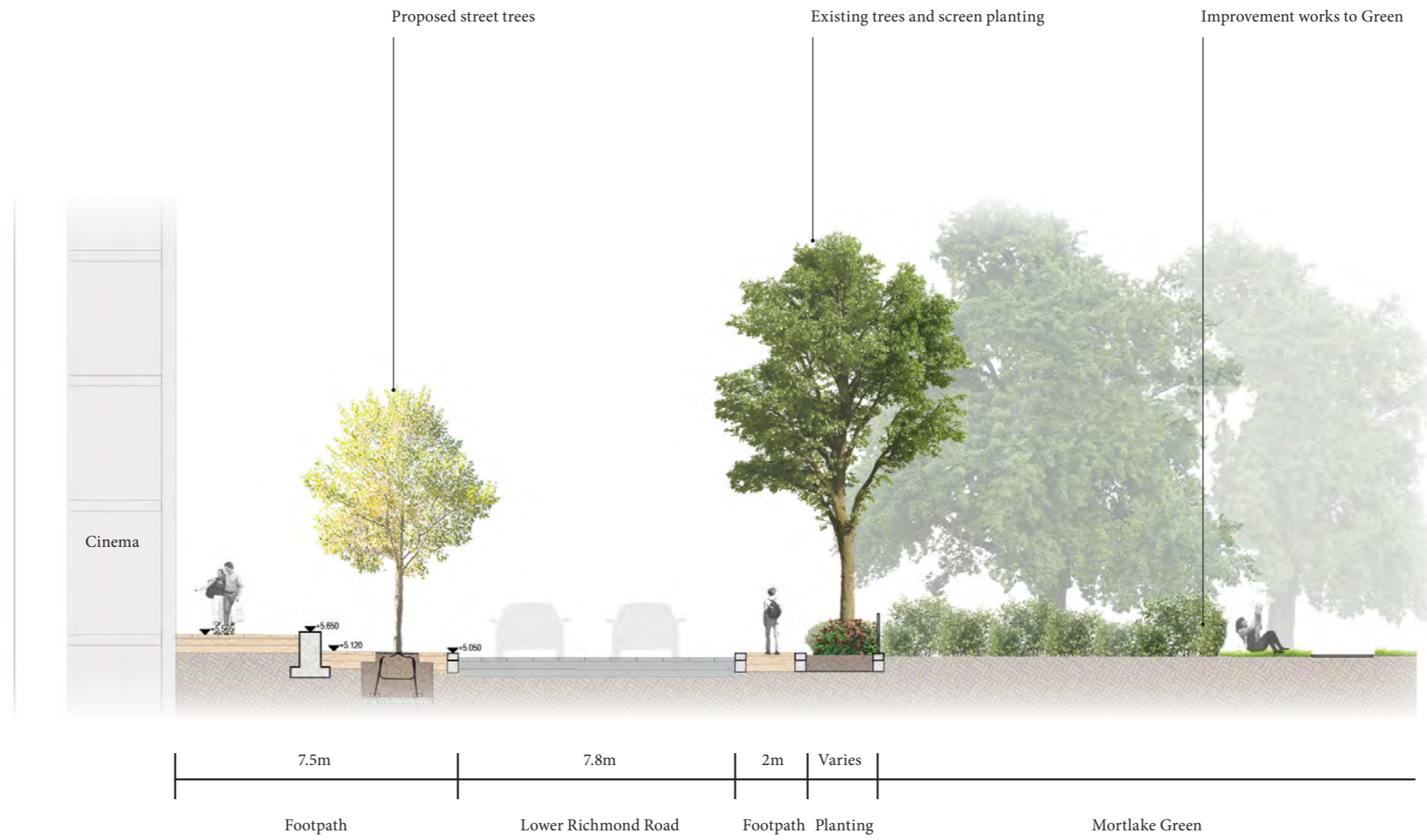
- ① New raised crossing for pedestrians and cyclists
- ② Proposed widening for additional lane (eastbound)
- ③ New entrance to Mortlake Green
- ④ Entry Plaza to Green Link
- ⑤ Footpath removed
- ⑥ Footpath removed and replaced with shrub planting
- ⑦ Existing bus stop removed
- ⑧ Traffic calming surface treatment (not raised)



Location plan

LOWER RICHMOND ROAD

Section AA



SHIP LANE UPGRADE

SHIP LANE WORKS

Landscape works include removal of the existing brick walls along the western side of the street and retention of all existing trees. A new paved footpath is proposed along the eastern side adjacent to the detailed part of the site development and an interim gravel path provided around the western side of the group of existing trees. Following development on the western part of the site, this footpath will be installed with a permanent finish where outside the Root Protection Zone of existing trees.

Provision of a raised pedestrian and cycle crossing at the end of Thames Street will be included in the works associated with the first stage of development and will cater for extension of the cycle connection when the outline area is developed.

- ① Temporary planting
- ② Existing London Plane trees retained
- ③ On-street parking & Footway enhancements
- ④ New crossing for cycles
- ⑤ Thames Street
- ⑥ Existing Towpath
- ⑦ Pedestrian entrance to residential courtyard
- ⑧ Access to Maltings Plaza
- ⑨ Temporary gravel footpath
- ⑩ Public amenity space integrated with play elements and seating
- ⑪ Natural stone footpath



ZOOM PLAN - SHIP LANE (INTERIM)



ZOOM PLAN - SHIP LANE (FINAL)

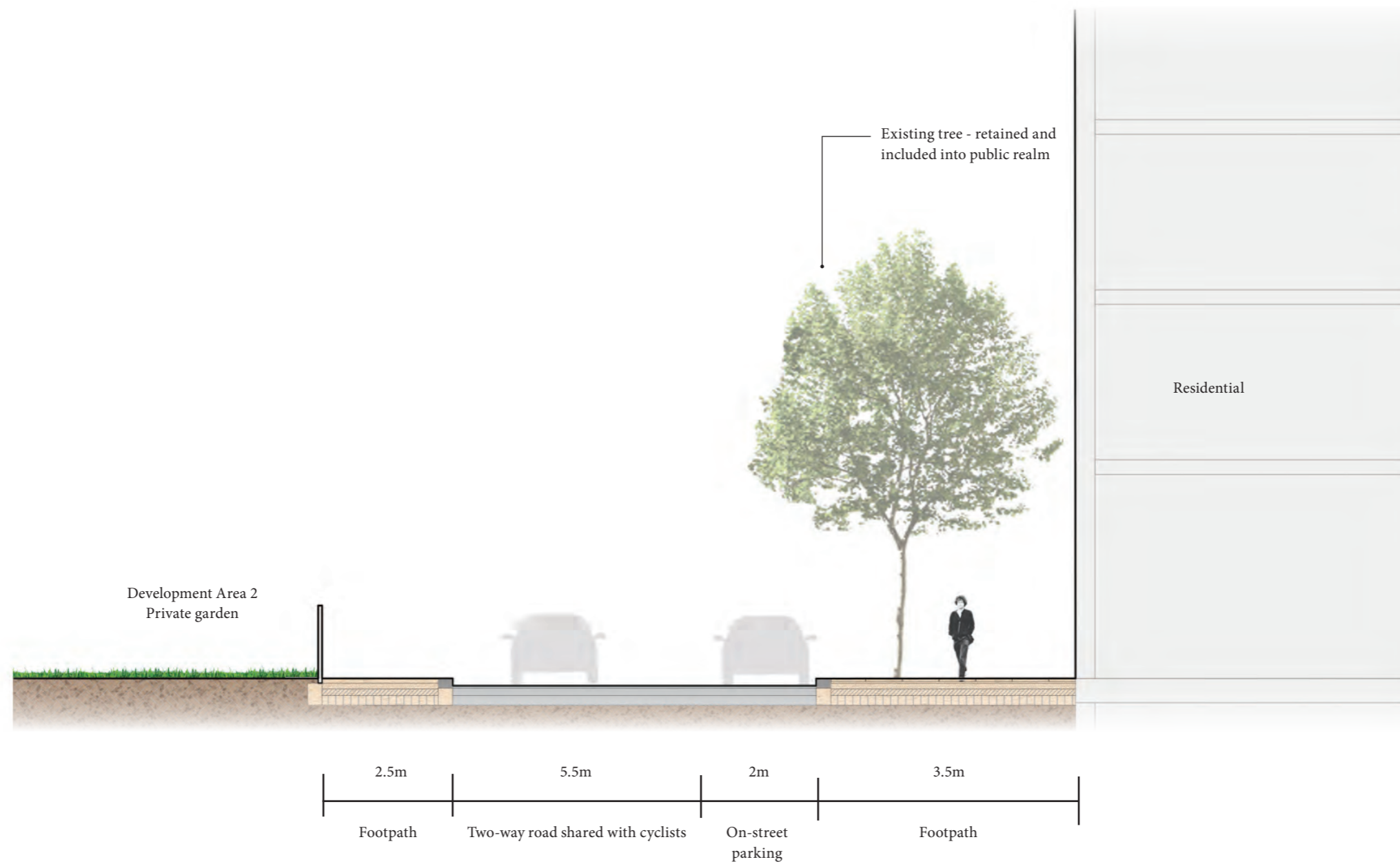
SHIP LANE UPGRADE

Section AA - 20m



SHIP LANE UPGRADE

SECTION BB - 14M



RESIDENTIAL COURTYARDS

RESIDENTIAL COURTYARDS

Courtyards spaces will be designed to accommodate both private terraces as well as publically accessible communal gardens for residents and visitors. Screening will be achieved by walls, fences and hedge planting, with some changes of level creating improved separation. Graded walkways (generally no steeper than 1:20.5) and steps from different entry points are provided to facilitate access for all user groups.

Playgrounds for different age groups will be located in the courtyards in the development, primarily 'Doorstep' Play for 0-5yrs age group - refer Play Strategy.

Small multi-stem tree planting, perennial, ground cover and Lawn areas will be used to provide formal and informal green space for the users.

Technical requirements have been considered to ensure successful design of courtyards built on podium level with basement underneath. Courtyard levels are typically 1.2m above structural basement slab to provide suitable space for drainage and growing medium.

- ① Natural play
- ② Doorstep play
- ③ Grass lawn
- ④ Small trees with perennial planting
- ⑤ Feature entrance paving
- ⑥ Hedge
- ⑦ Private terrace
- ⑧ Slopes to courtyard
- ⑨ Steps



SKETCH PLAN 1:500

RESIDENTIAL COURTYARDS

Section AA

