

PADDOCK COTTAGE HAMPTON COURT

DESIGN, ACCESS + HERITAGE STATEMENT

REV B / FINAL

FOR MR THOMAS + MRS BERARDI

TO BE READ IN CONJUNCTION WITH PROPOSED DRAWINGS

50°North

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1.0 INTRODUCTION

THE PROPOSALS, LOCATION, PLANNING AUTHORITY + DETAILS

1.0 THE PROPOSALS, LOCATION, PLANNING AUTHORITY + DETAILS

The Proposals

Our proposal is to create a new opening in a recently repaired boundary wall facing the Hampton Court Road to facilitate parking for the host property. The property situated along a narrow strip of houses and their associated gardens between Bushy Park (to the North) and Home Park (to the South).

The existing arrangement consists of a dwelling house extended recently to the east on the plot and a courtyard garden to the south as shown opposite. The property, unlike most detached and semi-detached houses along the road, does not have dedicated parking.

Nearby parking arrangements are unsatisfactory and not safe for a modern home with occupants and visitors of a range of ages. Parking is not near the property and so everything from travel to shopping and visitor access is compromised.

The focus of the proposal is to achieve a safer environment for the occupants. The road and pavement immediately outside the southern wall (hatched opposite), has been the site of serious accidents and near misses for some time and the blend of cyclists, vehicles and pedestrians has left the occupants feeling exposed and very unsafe when exiting the garden or using their car. We must change this situation.



Fig 01 A view of the road immediately outside the property showing cycling lanes and traffic (often very busy)

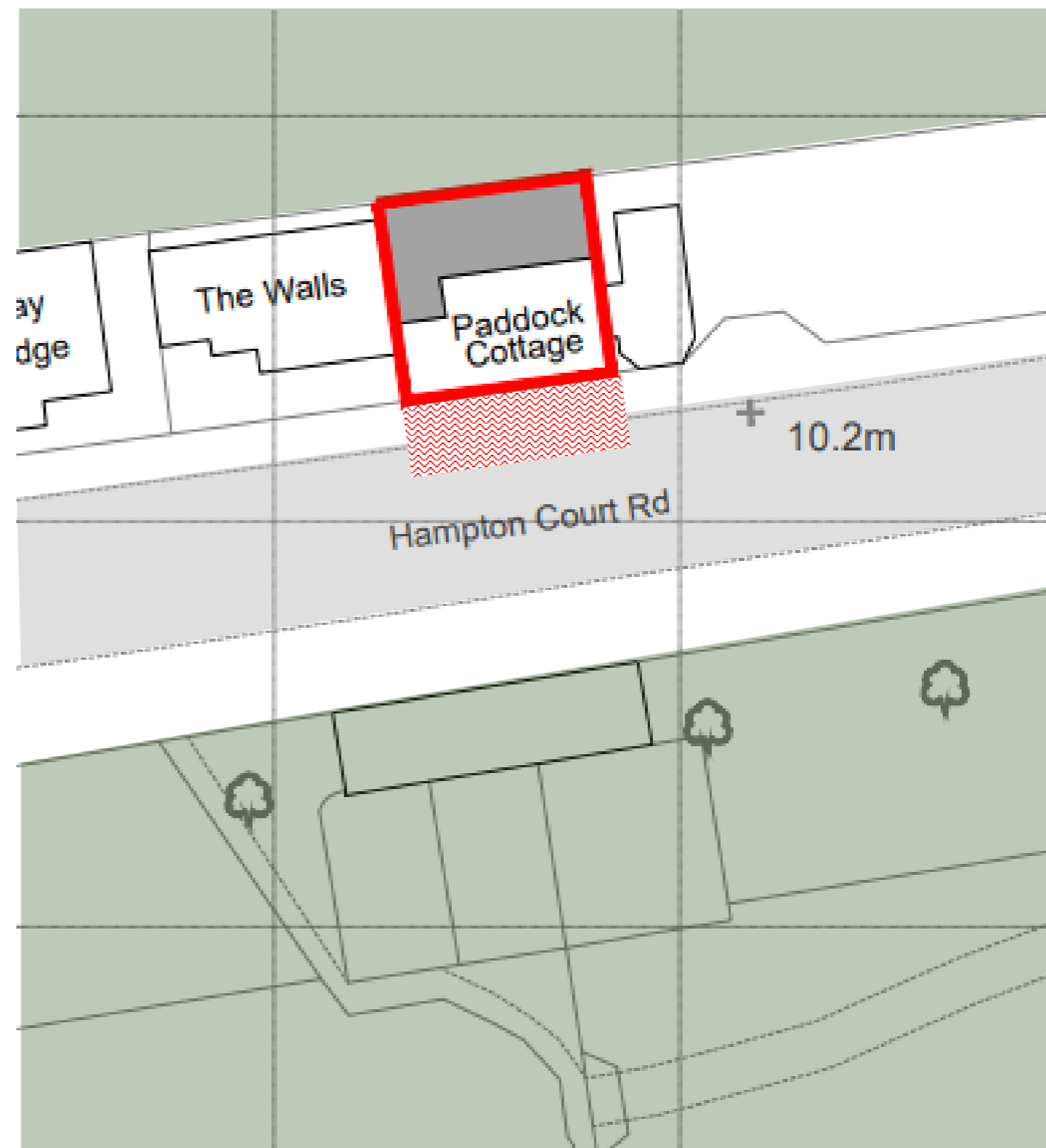


Fig 02 Site location plan (NTS)

THE PROPOSALS, LOCATION, PLANNING AUTHORITY + DETAILS

The Proposals continued

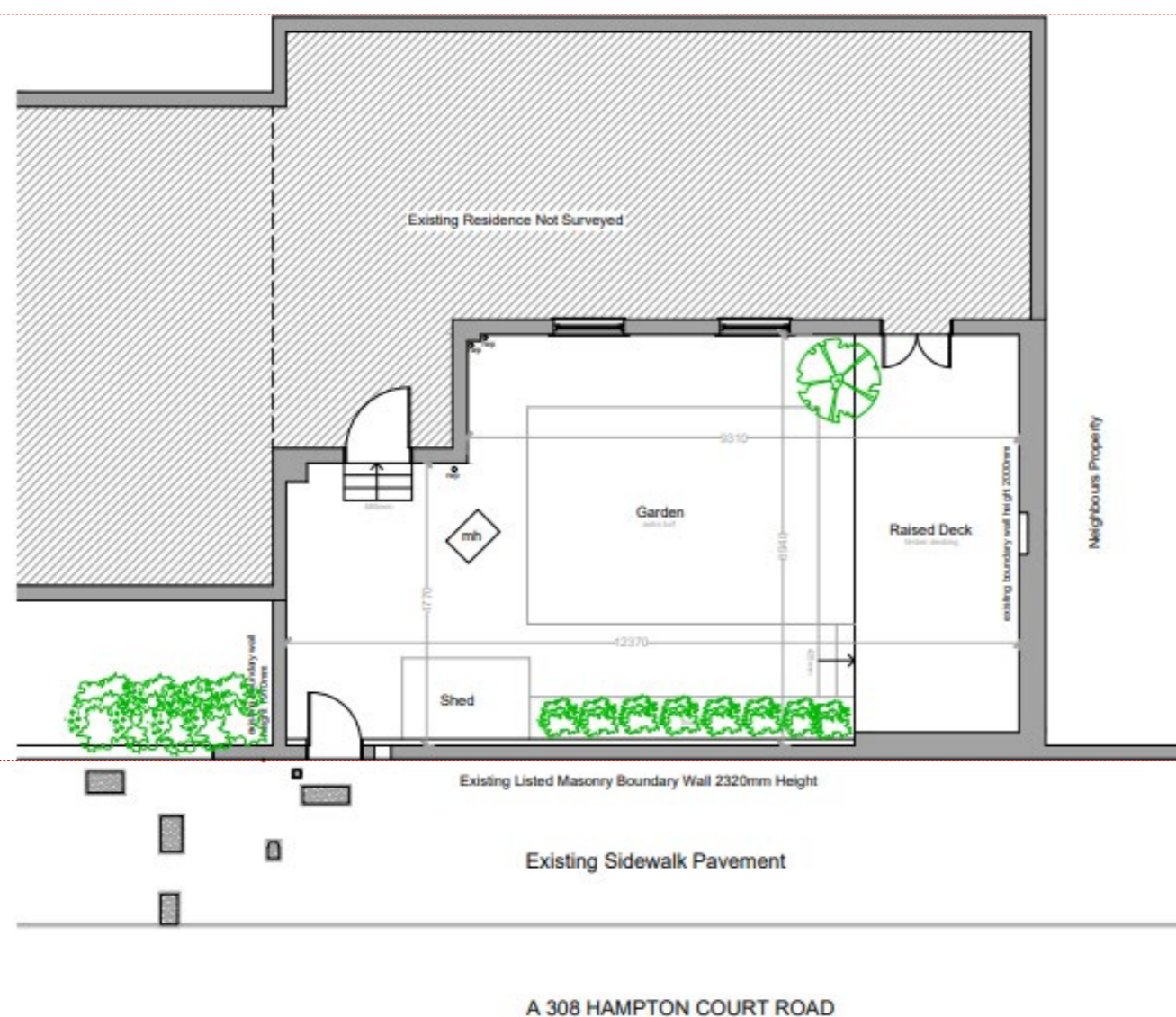
The proposal is situated in the **London Borough of Richmond on Thames** and is at the southern end of the borough. It is surrounded by Royal Parks estate land and faces the very busy A road - Hampton Court Road. The parking restrictions and part time parking arrangements on this road have led to a very unsuitable access and parking provision for this property and it is the exception along this thoroughfare.

Our proposals (outlined below) are seeking several key changes to this arrangement - To create a dropped kerb over the existing pavement; a new opening and sliding gate in a new boundary wall; and turning circle provision in line with the boroughs SPG for parking and highways.

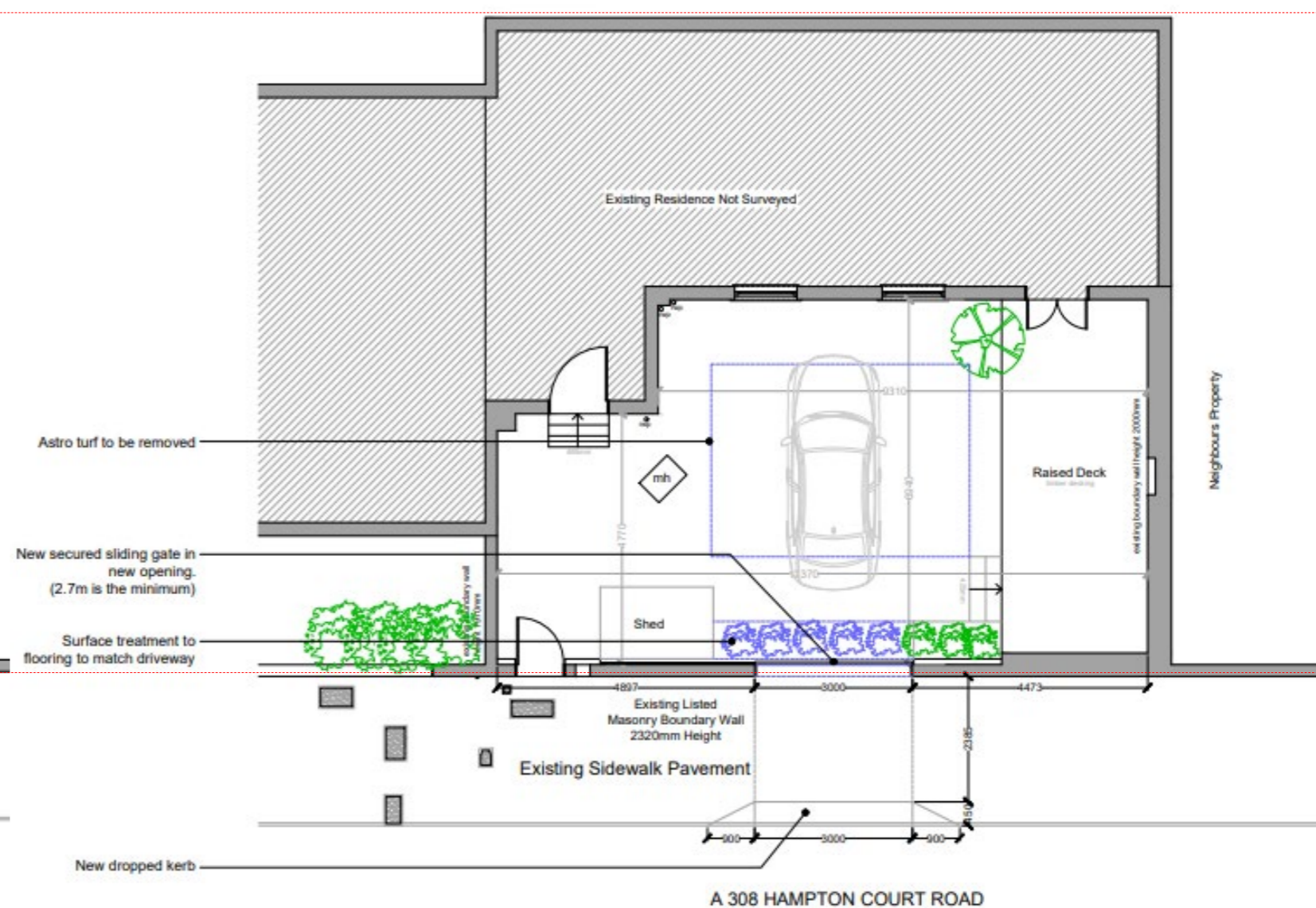
This proposal will create a safe parking arrangement close to the host property allowing safe egress and ingress to the building and change the existing very dangerous situation. Crashes involving visitors' vehicles nearly resulting in serious injury and the constant threat to passing cyclists having to avoid parked cars in the bike lane have led to a very untenable situation.

We would like to conduct a dialogue with the planning / highways team to achieve to desirable outcome.

BUSHY PARK



BUSHY PARK



ELEVATION FACING HAMPTON COURT ROAD

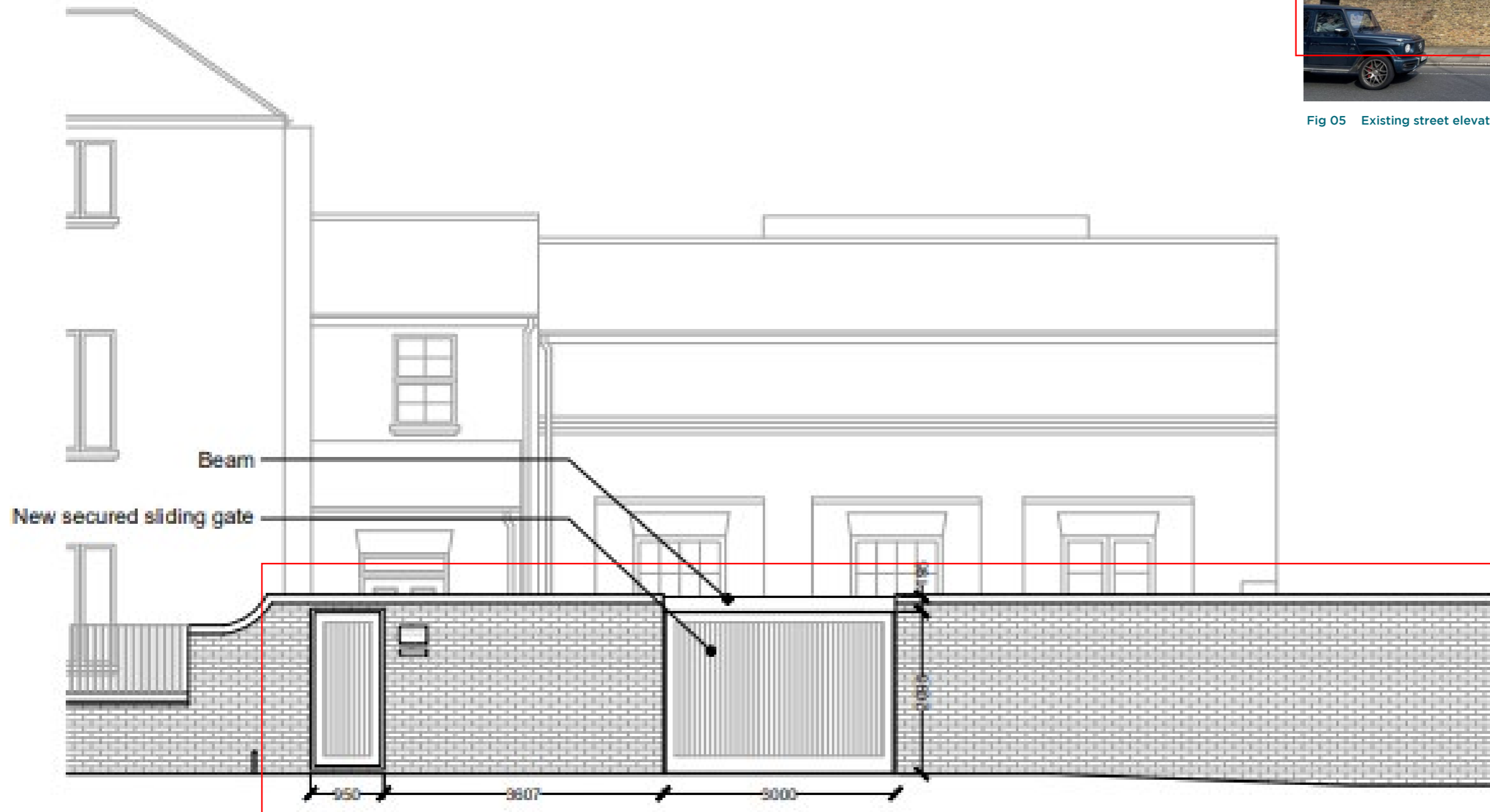


Fig 05 Existing street elevation of the site

EXISTING PHOTOS OF THE SITE



Fig 06 Existing wall (to the left-hand side), and the garden/entrance space



Fig 07 The existing garden arrangement (Wall shown to the RHS)

2.0

URBAN CONTEXT + SETTING

2.0 URBAN CONTEXT AND SETTING

Maps + Aerials



Fig 08 Aerial photo of the site location showing its orientation and proximity to Hampton Court Palace



Fig 09 Excerpt from the aerial showing the immediate area around the site

Host property

Extent of wall under consideration

Dedicated bike lane (with combined restricted parking)

Main highway (Hampton Court Road)



Wide footpath (which often features cyclists avoiding traffic)



Fig 10 Top, the wall facing the Hampton Court Road where the gate would be installed
 Fig 11 Bottom, the adjacent property to the west

Fig 12 Top, the adjacent property to the east
 Fig 13 Bottom, a view along the footpath to the west of the gated location

3.0 PLANNING POLICY

RELEVANT POLICY TO OUR PROPOSALS

3.0 PLANNING POLICY

This document and the associated drawings seek to demonstrate that the proposal is in accordance with the following applicable policies and guidance:

- The Local Plan - LBRUT
- SPD -
- Residential Design Standards
- The National Planning Policy Framework (NPPF).
- Hampton Court Green Conservation Area (CA) planning statement (11)

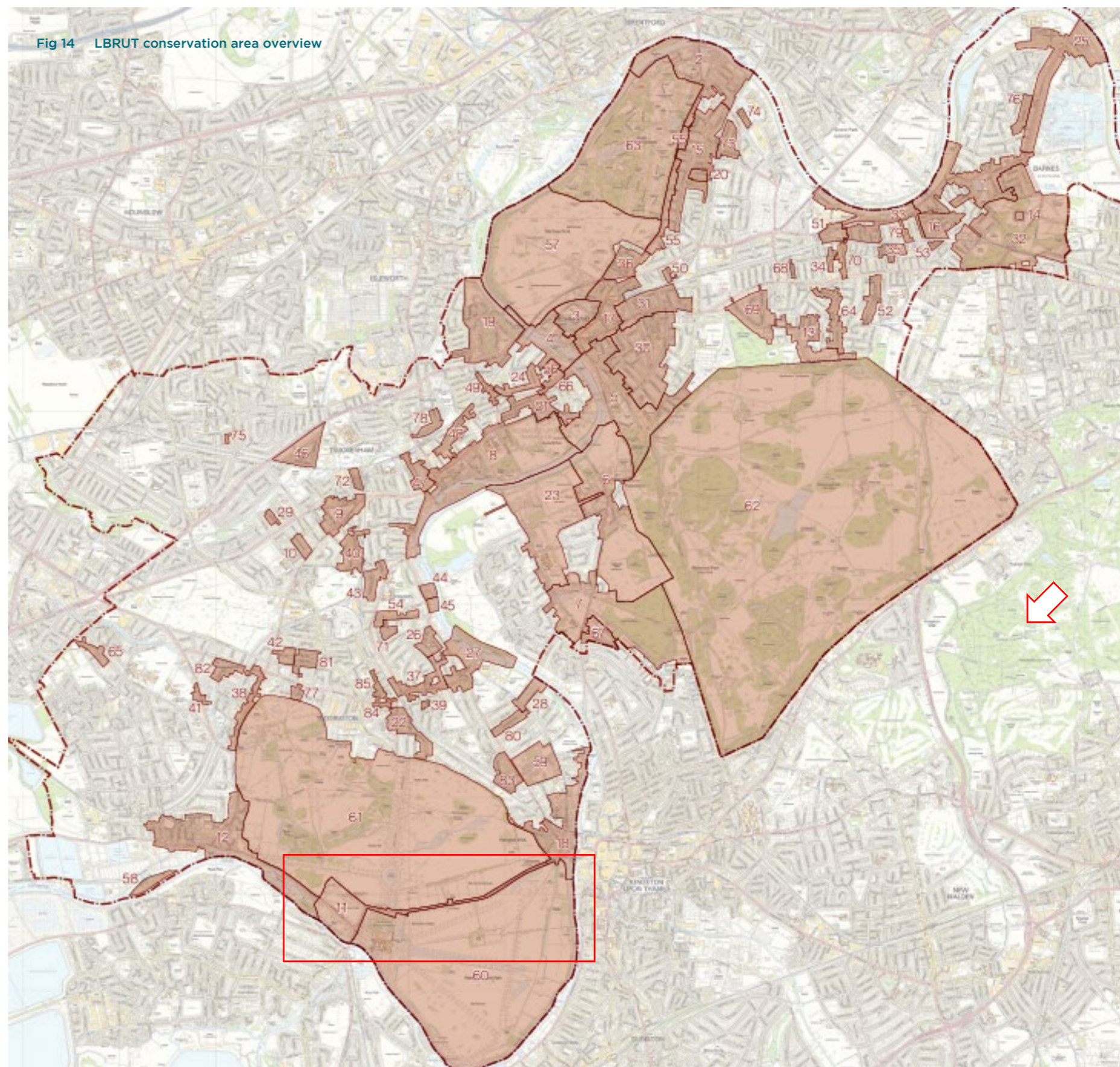
The map opposite illustrates the conservation areas within the borough – the CA we are working within is highlighted by the red box. In addition, on the following page, we show the specific CA map for area 11 with our property shown by the arrow symbol. Linear development pattern with close proximity to the Hampton Court Road.

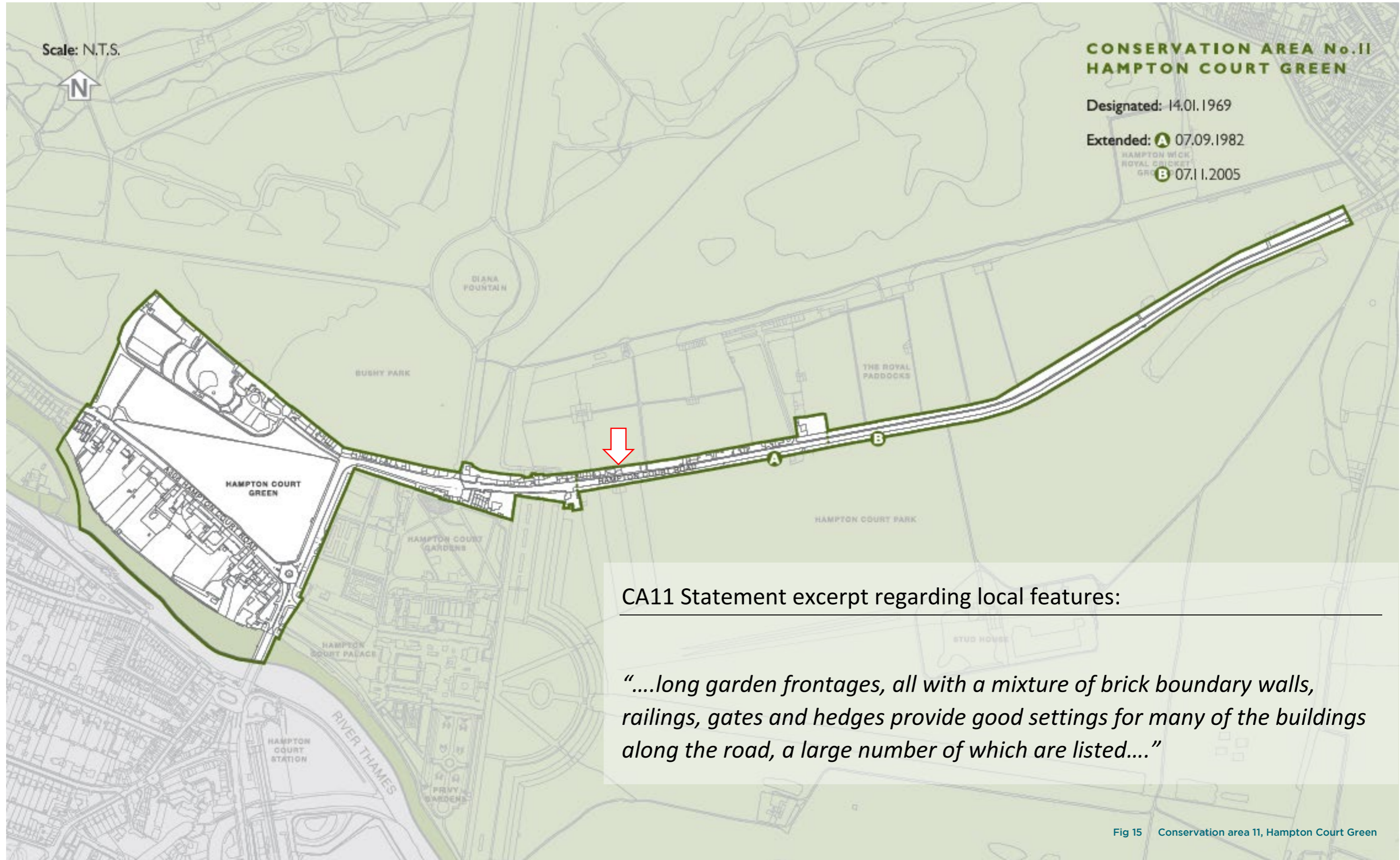
With reference to the CA statement listed above, we note the following:

“..... Hampton Court Green conservation area is a linear development along approach and perimeter roads to Hampton Court Palace. It is contained by Hampton Village (12), Hampton Wick (18), Hampton Court Park (60) and Bushy Park (61) conservation areas....The area is closely associated with the Tudor Palace, with further high-quality development, which continued into the 18th century....”

The host dwelling can be described as follows: Paddock Cottage is a Grade II Listed Building located along Hampton Court Road bounded by an existing high garden wall. It has limited high level views to the front to Home Park and to the rear to the Royal Paddock and Bushy Park beyond. The property was probably originally one house which was later divided into two allowing for a flying freehold at first /second floor levels to Paddock Cottage, and various extensions were added for the stairs and additional living accommodation. The single storey element is understood to be a Victorian addition.

The property is a family home and has in recent years been upgraded to meet modern day living with the works undertaken subject to Listed Building Consent, ref: 07/1617/LBC, consent granted 21.09.07. In conjunction with these works consent was obtained (08/0117/LBC) for the reinstatement of the chimney to the rear of the single storey part of the property to service the grand fireplace in the open plan ground floor living area although these works weren't implemented.





4.0

DESIGN FACTORS + CONSIDERATIONS

4.0 DESIGN FACTORS + CONSIDERATIONS

How we designed the proposals for the site

Design Approach

We have carefully reviewed the existing site and compared to diagram opposite. This shows how a 5.4 metre turning circle can be accommodated within part of the existing garden allowing a car to freely turn and face the forward direction to exit the site.

The proposed new gate will be a sliding timber type with remote opening and closing. There is also capacity for camera and other vision aids to facilitate entry and exiting. The gate is situated to be in keeping with the street elevation and compliment the nearby pedestrian gate. The gate opening is framed by a black painted lintol design found nearby on similar gate accesses.

In line with the SPG and highways advice we have proposed a standard dropped kerb design. The footpath is almost 3 metres wide which facilitates excellent vision splays along the footpath and road. A dropped kerb also serves to highlight the access point.

This diagram also highlights the cycle lane and relationship of the centre of the lane to the footpath (green dotted line), and the yellow dotted line that cyclists often take when faced with a blocked cycle lane. This is a very sub-optimal arrangement.

We have taken care to develop a simple clear aesthetic for the new element and sited it so that it doesn't interfere with existing services while giving maximum visibility to the access point. There are no dedicated bus or other public transport elements to consider.

The proposals seek to replace the astro area with a different landscape space creating a softer area for a family. Enhanced ecological footprint will be delivered as a result. This proposal seeks to combine safety, amenity and practicality and is in no way purely to deliver a park spot.

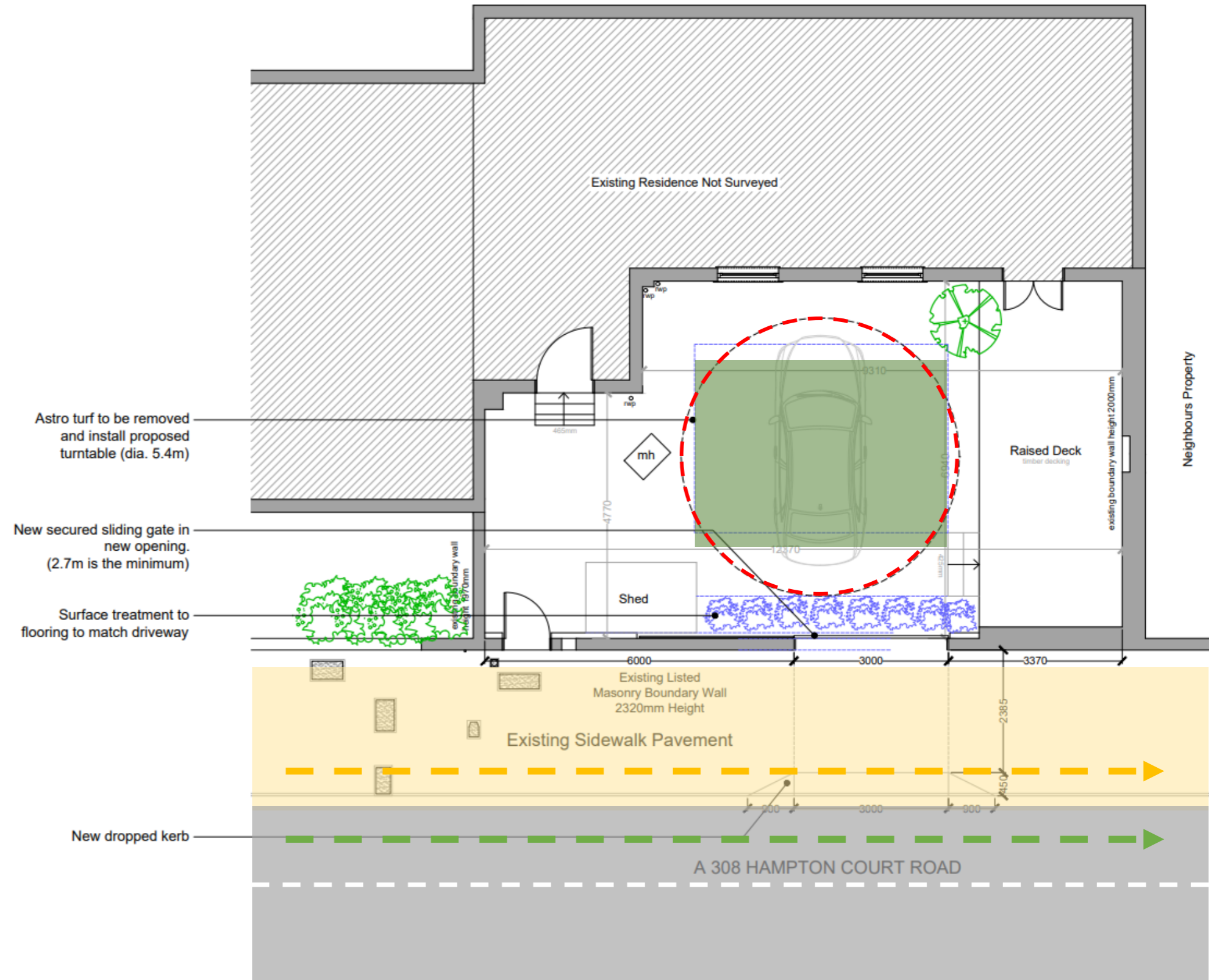


Fig 17 Proposed plan of the new measures focussing on the external spaces.



Fig 18 Existing parking arrangements to the west of the site for reference showing dropped kerbs and parking spaces



Fig 19 A details of current parking restrictions



Fig 20 Parking arrangements and general views of access for properties to the east



Fig 21 Yet, further, parking arrangements with more spacious parking facilities as a guide

5.0 – 9.0

**RELEVANT PLANNING HISTORY
PRIVACY OUTLOOK + AMENITY
FLOOD RISK ASSESSMENT
ACCESSIBILITY
WASTE DISPOSAL + RECYCLING**

5.0 RELEVANT PLANNING HISTORY

Relevant planning history for the property is listed below:

We note that the property has featured a range of changes and extensions over the years and now sits as a very smart and in keeping part of the conservation area abutting both Bushy Park and Hampton Court Way.

The property has also been landscaped to create a modest garden area with storage and seating area. This application is focussed on creating a more secure and safe environment with better amenity and parking arrangements that add to the property's facilities.

We are seeking to introduce access and parking so that the current occupants and those living there in the future feel safe and the building is able to be used by a wider variety of people such as those with families, the elderly and those with disabilities. This site is unique in some ways.

As part of our research we have also reviewed nearby applications looking at the scale and type of additions and alterations proposed and believe our work to be in keeping with the general pattern of development along this stretch of the road.

16/1065/DD01
Details pursuant to condition U04571 (Details of foundations) and U04572 (Details of brickwork , ne...
Granted Permission 13/07/2017

16/1849/FUL
Revised rear elevation to show 2no. windows in place of French doors previously approved.
Granted Permission 12/06/2017

16/1850/LBC
Revised rear elevation to show 2no. windows in place of French doors previously approved
Granted Permission 12/06/2017

16/1080/LBC
Amendment to Application 14/4179/LBC approved on Appeal for - Roof extension over existing single s...
Granted Permission 05/05/2016

16/1039/HOT
Demolition of boundary wall facing Hampton Court Road and rebuild new
Granted Permission 03/05/2016 (highlighted for relevance)

16/1065/LBC
Demolition of boundary wall facing Hampton Court Road and rebuild new
Granted Permission 03/05/2016 (highlighted for relevance)

16/0795/HOT
Amendment to Application 14/3752/HOT approved on Appeal for - Roof extension over existing single ...
Granted Permission 05/05/2016

14/3752/DD02
Details pursuant to condition 4 of appeal decision re external surfaces
Granted Permission 21/03/2016

14/4179/DD01
Details pursuant to condition 4 of appeal decision - re external surfaces
Refused Permission 03/02/2016

14/3752/DD01
Details pursuant to condition 4 of appeal decision re external surfaces
Refused Permission 03/02/2016

15/3378/LBC
Proposed basement and access serving side extension.
Granted Permission 12/11/2015

15/3404/LBC
Amendment to listed building consent (ref 14/4179/LBC) for additional accommodation in roof level e...
Refused Permission 29/10/2015. Appeal Dismissed on 20/04/2016

15/3307/HOT
Amendment to planning permission (ref 14/3752/HOT) for additional accommodation in roof level extension.
Refused Permission 29/10/2015. Appeal Dismissed on 20/04/2016

15/3048/HOT
Proposed basement and access serving side extension.
Granted Permission 12/11/2015

14/4179/LBC
Roof extension over existing single storey part of the property to include accommodation within room...
Refused Permission 10/11/2014. Appeal Allowed on 07/05/2015

14/3752/HOT
Roof extension over existing single storey part of the property to include accommodation within room...
Refused Permission 28/10/2014. Appeal Allowed on 07/05/2015

12/1043/LBC
Increase height over single storey part of property to provide rooms within new roofspace including...
Granted Permission 22/05/2012

12/1042/HOT
Increase height over single storey part of property to provide rooms within new roofspace including...
Granted Permission 22/05/2012

08/0117/LBC
Construction of chimney to rear of building
Granted Permission 11/03/2008

08/0116/HOT
Construction of chimney to rear of building
Granted Permission 11/03/2008

07/1619/DD01
Details pursuant to condition U15882 -larger scale plans-
Granted Permission 21/09/2007

07/1619/LBC
Internal alterations and replacement of windows with door on front elevations.
Granted Permission 04/07/2007

04/0106/TCA
T1 - Magnolia spp. - Cut down.
Granted Permission 13/09/2004

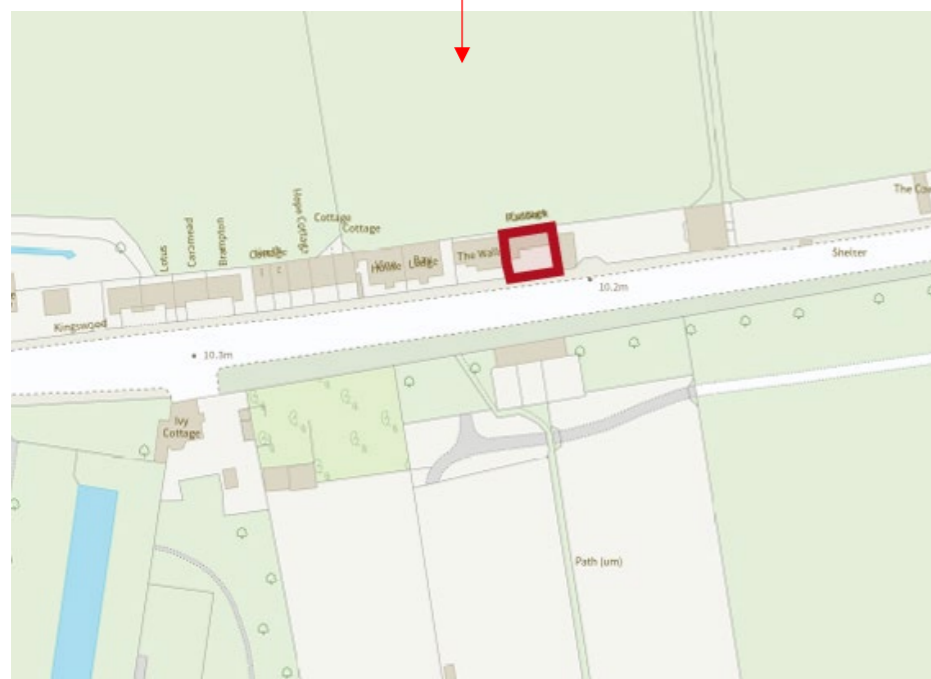
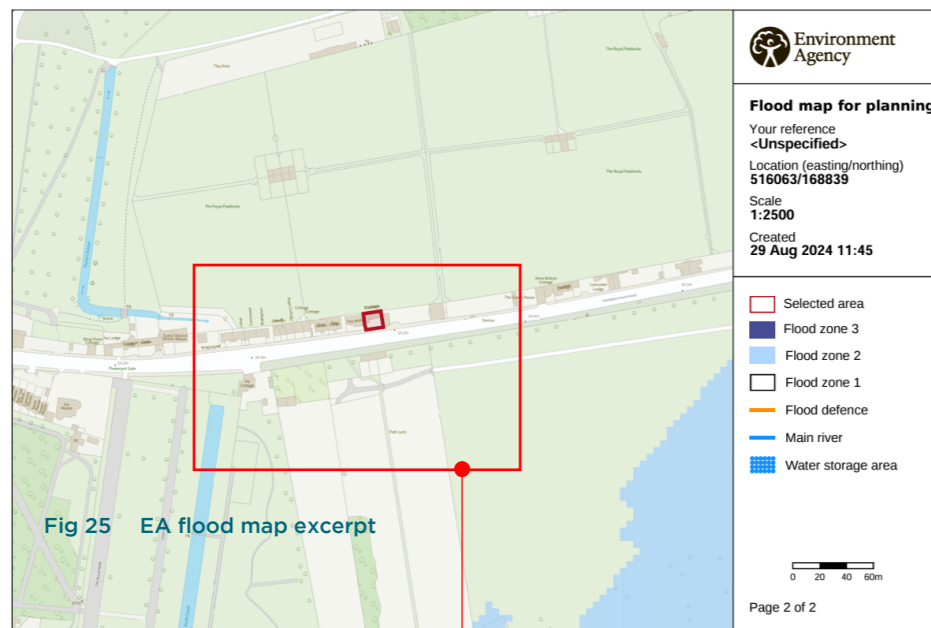
The highlighted applications above show the previous submissions relating to this wall which has been part demolished and rebuilt. This was due to the structure being unsafe. To this end, the walls aesthetic is one of new and old patterns of brick as shown on the attached photo study.



6.0 FLOOD RISK ASSESSMENT

We have downloaded the relevant flood map below:

The site is not in a Flood Zone which means the site has no significant risk of flooding. This information is taken from the [Flood Map](#) for Planning on government website.



Figs 23-24 Taken from the govt flood map site, the maps above show the property in relation to flood zones. The excerpt illustrates the lack of flood risk.

7.0 ACCESSIBILITY

Flush thresholds and level access

The property is currently accessed via the front door and garden facing French doors which lead to the entrance hall and living areas. This proposal doesn't seek to alter or compromise these arrangements indeed the layout of the property lends itself to the proposed parking and access changes.

8.0 WASTE DISPOSAL + RECYCLING

Recycling, refuse and design

Household waste and recycling will continue to be carried out as per council requirements. The property has been designed to accommodate storage and recycling including a utility room, kitchen recycling facilities and external bin storage. There is no change to the requirements for council refuse collection, indeed it is enhanced by the proposals due to increased access from the private areas to the roadside.

Current recycling route is shown below on our proposed floor plan (NTS)

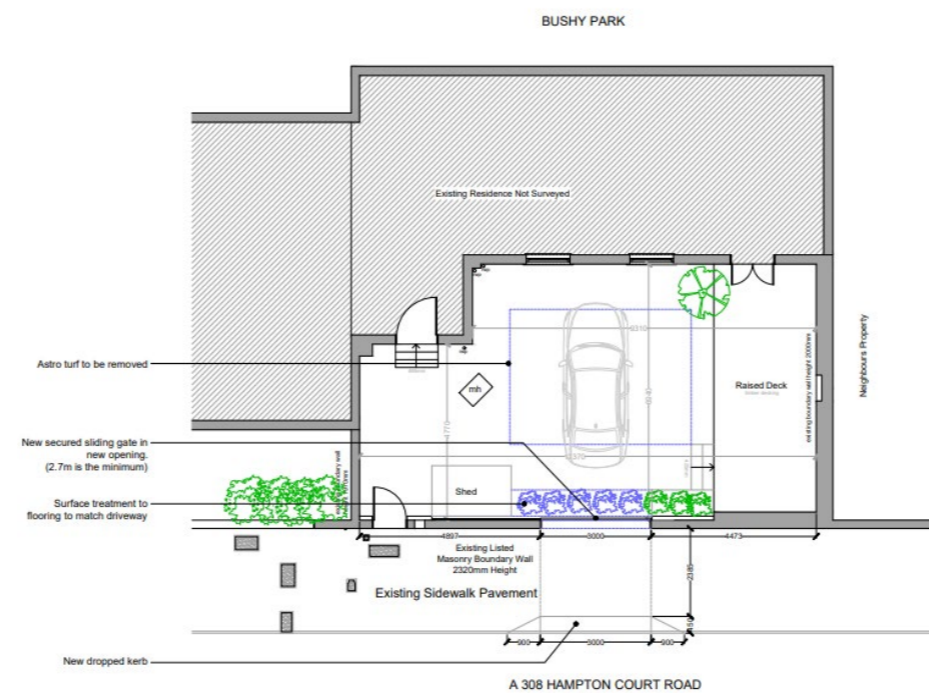


Fig 25 Excerpt from proposed drawings showing site layout, access and recycling arrangements.

9.0 PRIVACY OUTLOOK + AMENITY

The design and privacy

It is considered that issues of privacy, outlook and amenity are not undermined by our proposals due to the nature of the works, this application is firmly about access and parking arrangements.

By seeking approval for a new gate and access off the road we will not detrimentally affect adjacent homes and their access which, as is visible from our photos and drawings, already features vehicular access, dropped kerbs and gates along the road.

The privacy and amenity of adjacent homes is unchanged, this proposal seeks a safer environment for adjacent homes through better access, reduced likelihood of accidents and associated issues.

We believe this proposal benefits refuse collection, parking, cycle lane safety and pedestrian safety.



Fig 26 Streetview image to illustrate the general setting and adjacent entrances and parking arrangements to other properties.

10.0

PARKING + ROAD SAFETY

10.0 PARKING + ROAD SAFETY

New gate, dropped kerb, and parking proposals

Using Transport SPG June 2020 and current CPZ mapping we make the following observations for discussion with the case officer and highways department:

This scale of development does not require and transport assessment to be supplied. A parking survey is also not required as we do not intend to affect define parking bays or spaces.

We understand looking at 12.1 of the SPG that more dropped kerbs are undesirable, but we believe there are mitigating factors for this application which require discussion and consideration.

We understand that if granted, works to the footpath and highway will be carried out by the councils' approved contractors and a license will be required for the works to the wall.

The spatial requirements outlined on page 15 of the SPG have been met by our design.

Our design makes provision for a turning table under paragraph 5.5 of the SPG.

Fig 27 CPZ details closest to the site for reference

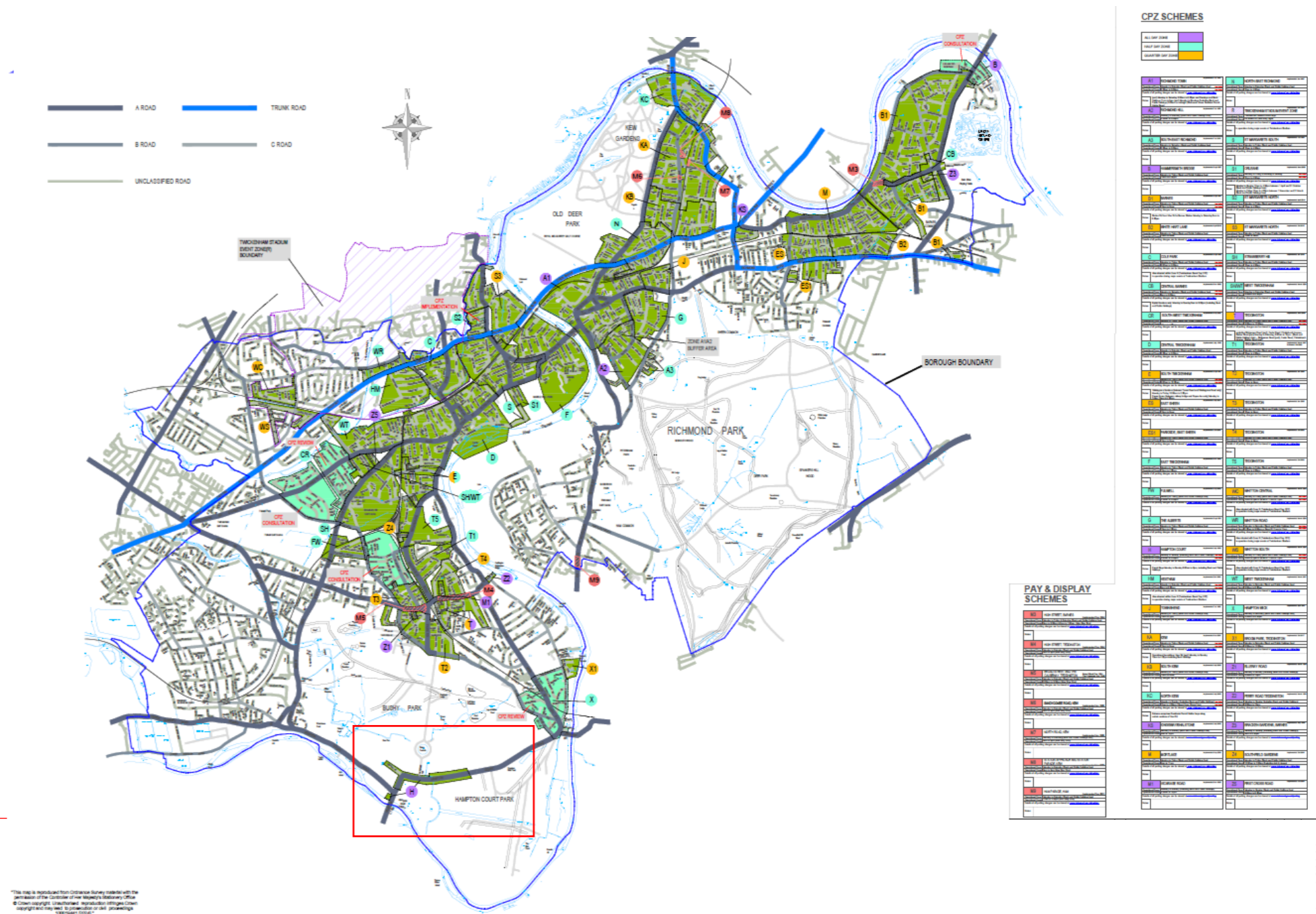
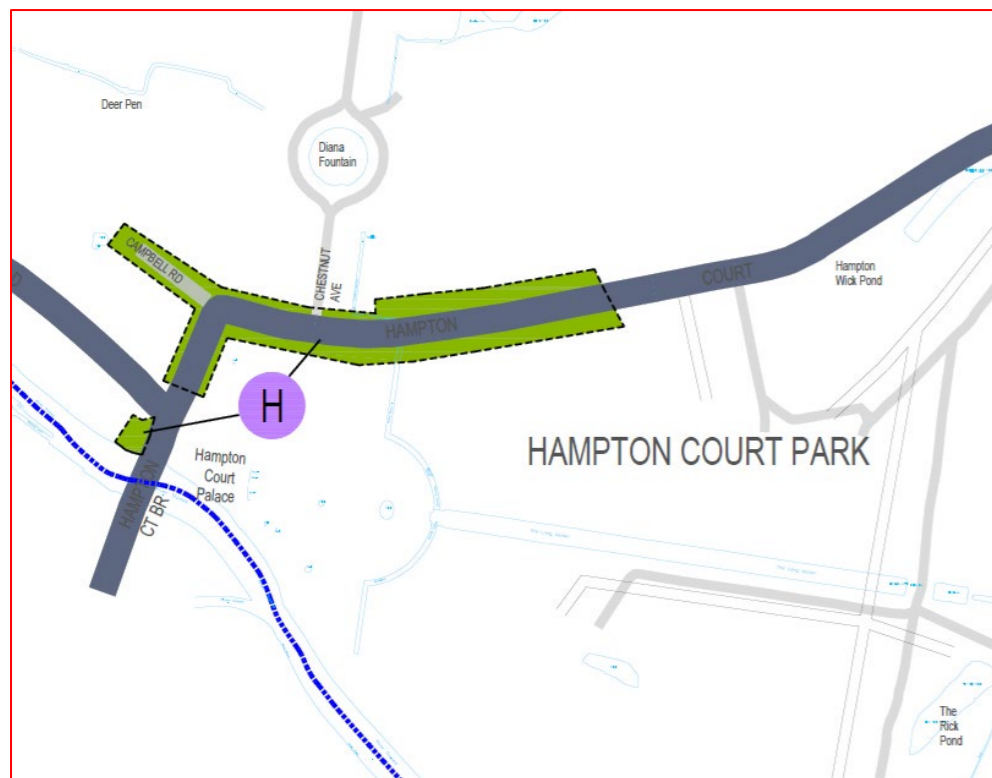


Fig 28 LBRUT CPZ overview

11.0

CONCLUSION

11.0 CONCLUSION

We believe our proposal should be received and considered favourably because for the following reasons:

The Wall and Streetscape

The proposal is to carefully alter the boundary wall in the same style and approach as many nearby properties with similar settings and relationships to the highway and footpath. The design has been developed to be simple and clear with practical aims. See below for details of this design and the effect on the garden of the property.

Safety

If these works are favourably received and determined, they will transform the property and lives of its inhabitants with regard to safety and amenity. The key driver of this application is the safety of the occupants and their family. There are numerous and regular accidents immediately outside of the property on Hampton Court Road. Some have directly impacted our clients (written off vehicles), and regular occurrence of pedestrian / cyclist near misses which is particularly worrying for elderly visitors and children. This aspect is real and visceral to the owners.

Our proposals will enhance the safety of passing cyclists and pedestrians and could be seen as part of a package of measures to make this stretch of the road safer for all. Currently the situation is suboptimal and widely seen as untenable by our clients and their neighbours. There are two reasons for this - The parking restrictions timings and the change in speed limits along the road. There appears to be confusion amongst road users and this, along with general speeding, makes this a blackspot for accidents.

Wholistic Approach

In proposing these changes we have sought the input of 3 key advisors - A planning consultant, a highways consultant, and the web for planning case history and policy which is referenced above. Our aim is to put forward a case for an off-street space in place of part of the existing astro area greatly enhancing the property and giving a lot of comfort to the clients.

Parking and Speed restrictions

The photos on the next page show the proximity of parking to the property with dedicated parking over 100m from the entrance to the building which is at best inconvenient but has created a part time parking arrangement that has led to widespread confusion for visitors. This in turn creates opportunity for safety issues as we have described.

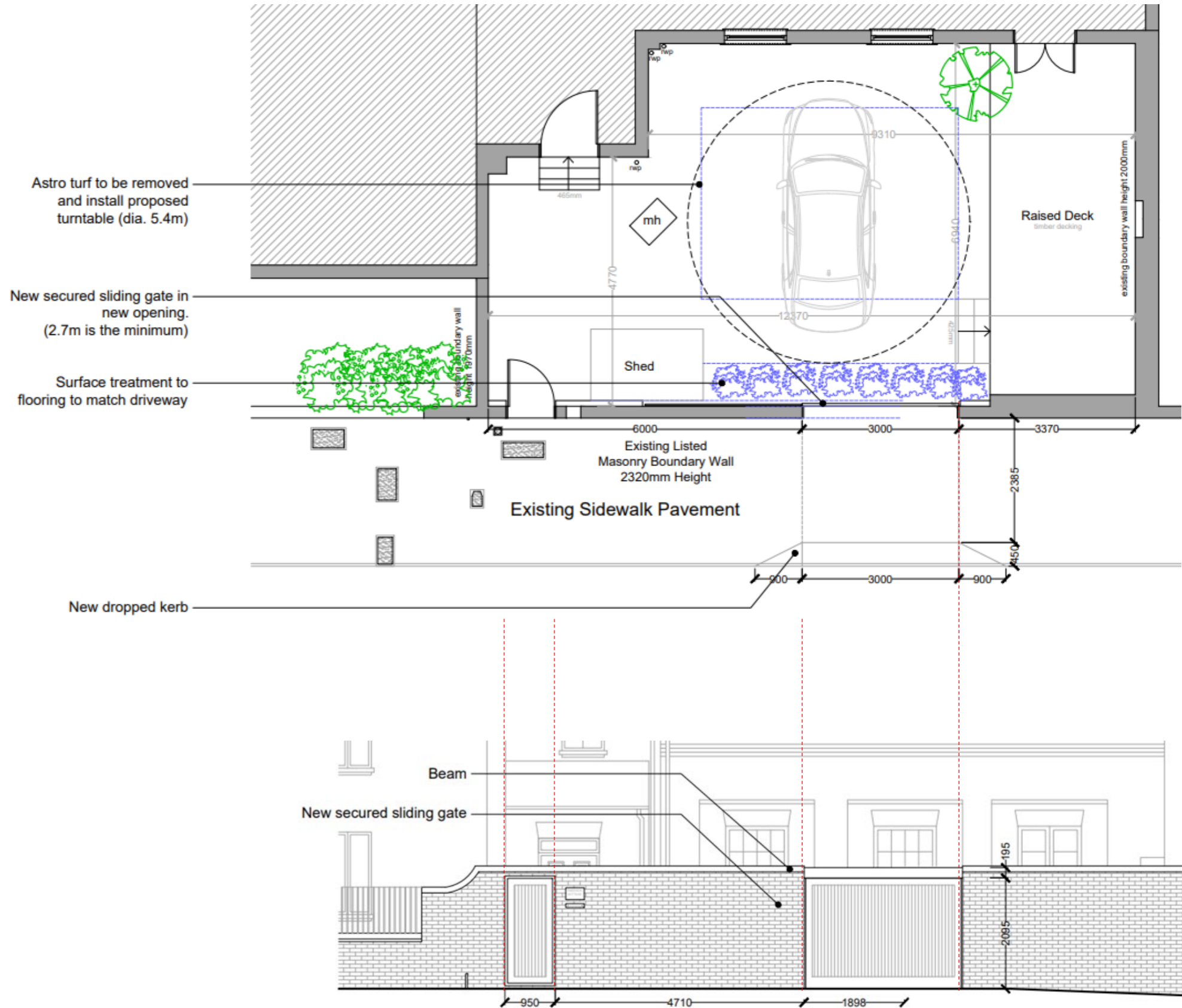


Fig 29 Proposed plan + street elevation illustrating an 'in-keeping' and simple gate arrangement with the parking plan above - Combining the parking with other amenity elements

Our Aims

It is hoped that the proposals demonstrated within this Design and Access Statement and the accompanying proposed drawings will be supported by the London Borough of Richmond Upon Thames and we would welcome any opportunity to discuss them with the case officer in due course.

We have extensive site knowledge containing photographs and survey information we can supply on request.



Fig 30 The property is 110 metres from the nearest parking and parking is restricted up until 6:30 each day giving rise to confusion amongst drivers.



Fig 31 Detail showing the nearest dedicated parking spaces over 100m from the property leaving part time parking adjacent to the property where traffic is prone to speeding

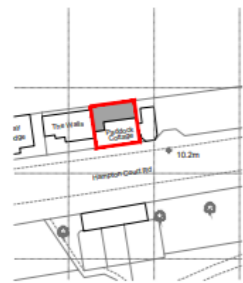
12.0

APPENDIX

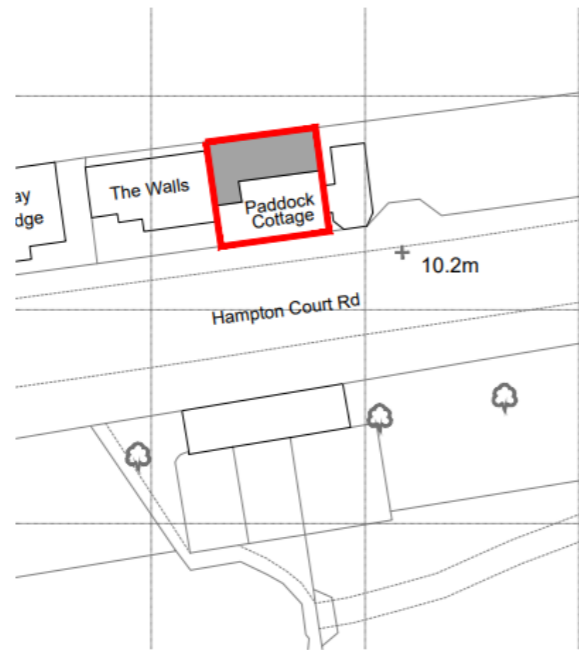
ADDITIONAL INFORMATION TO SUPPLEMENT THE APPLICATION

12.0 APPENDIX 1

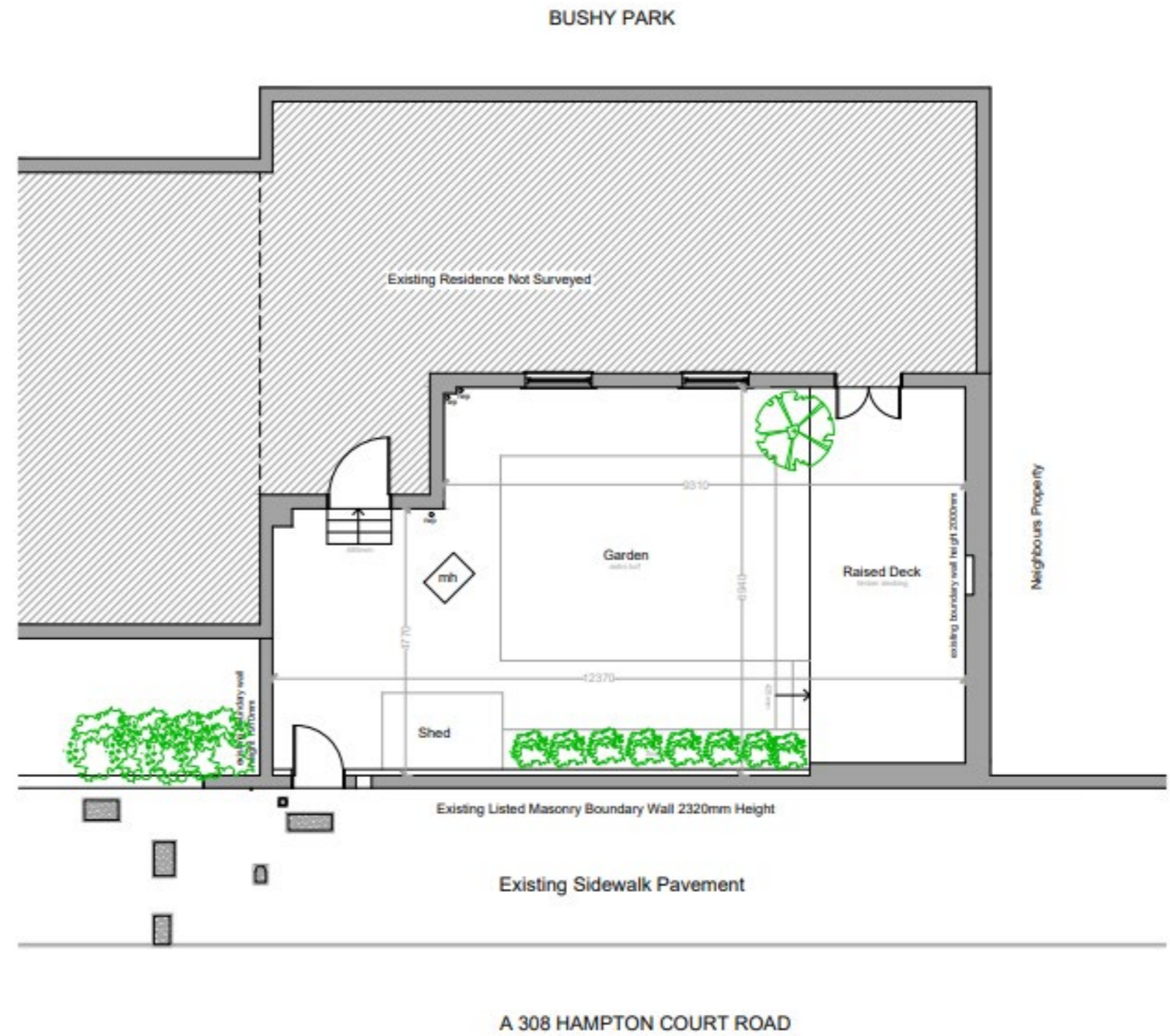
Existing plans + elevations



01 Existing Location Plan
1:1250 @ A3



02 Existing Site Plan
1:500 @ A3

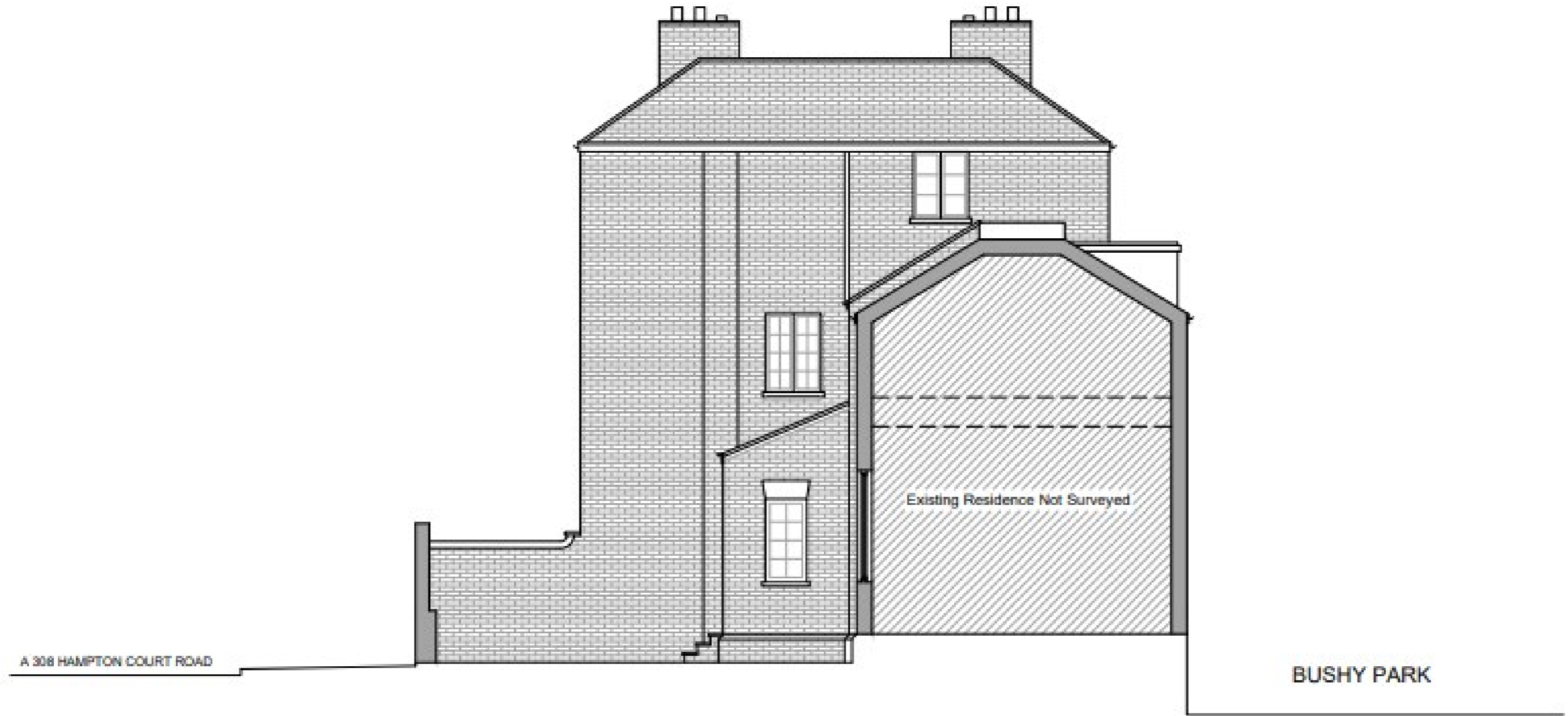




01 Existing Front Elevation
1:100 @ A3



02 Existing Boundary Wall Elevation
1:100 @ A3



END

THANKS FOR YOUR TIME

Please contact us with any queries and comments. Additional information may be requested by the case officer. Please contact info@50degrees.co.uk and ask for the project architect for the project if you require more information.

50° North