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16 WHITTON ROAD,
TWICKENHAM, TW1 1BJ

TRANSPORT STATEMENT

November 2024

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Ref: File path P:\ P3009 16 Whitton Road Transport Statement November 2024

I.0 INTRODUCTION

- I.1 Paul Mew Associates is instructed on behalf of Ivo Kay in relation to the S106 agreement at Flat 3, 16 Whitton Road, Twickenham, TW1 1BJ.
- I.2 The application site's location is presented on a map in Figure 1 of this report.
- I.3 The local planning and highway authority is the London Borough of Richmond upon Thames.
- I.4 The site is located on Whitton Road, a short walk to the junction with London Road.
- I.5 The application site has a public transport accessibility level (PTAL) score of 4 which is a 'good' accessibility rating as defined by Transport for London (TfL).
- I.6 The site is located within controlled parking zone (CPZ) 'C'. CPZ 'C' is subject to parking restrictions from Monday to Friday, 08:30-18:30.
- I.7 The area immediately adjoining the site comprises of a mixture of residential dwellings and shops at the junction of Whitton Road and London Road.
- I.8 The proposal seeks to remove the condition that imposes a parking permit restriction currently associated with the dwelling.
- I.9 This Transport Statement (TS) has been prepared for submission with either an s73 or s106A application to the local planning authority to remove the condition. The TS includes the results of a parking survey undertaken in November 2024 according to the Richmond Parking Survey Methodology, which is outlined in Appendix A.
- I.10 The following chapter sets out the results of the existing parking conditions on the roads adjoining the site.

2.0 SURVEY OF EXISTING PARKING CONDITIONS

2.1 The first stage of assessing the parking impact of the proposed variation of the S106 agreement is to survey the existing baseline conditions on the adjoining road network.

Parking Survey Inventory

2.2 The first stage of the parking assessment is to map out the parking survey area. All kerb space largely within a 200-metre distance of the application site has been measured using a measuring wheel and the on-street parking opportunities have been recorded to-scale onto OS mapping.

2.3 This parking survey has been conducted in accordance with the Richmond Parking Survey Methodology.

2.4 The parking study area has been curtailed or extended where it has been deemed appropriate as it is unlikely that someone seeking a parking spot would simply stop at an imaginary 200-metre line, surveyor discretion has therefore been applied. The full extent of the area included within this parking study is presented in Figure 2.

2.5 The survey area comprises of the following roads:

- Cole Park Road
- Grimwood Road
- Latham Road
- London Road
- March Road
- Whitton Road

2.6 The site is located within CPZ 'C', which is subject to parking restrictions from Monday to Friday, 08:30-18:30. There is also a small amount of pay by phone

parking, which itself is also part of CPZ 'C'. This is effective Monday to Friday, 08:30-18:30, with a maximum stay of two or four hours.

2.7 All vehicle crossovers and kerb space within 7.5 metres of junctions have been eliminated from the surveys. The remainder of the parkable kerb space within the survey area has been measured on-site; the total distance of kerb space between crossovers / junctions has been recorded and split into increments of 5 metres in accordance with Richmond Council's parking survey methodology.

2.8 The parking survey inventory is presented in Table I as follows (additionally refer to Figures 3 a-c):

Table I. Parking Survey Inventory

Road or Section of Road	PARKING STUDY INVENTORY	
	Length of parallel kerb side parking PHO 'C' (metres)	Total PHO 'C' kerb side parking spaces
Cole Park Road	107.2	19
Grimwood Road	121.7	24
Latham Road	123.7	22
London Road	46	8
March Road	102.9	18
Whitton Road	93	17
Total	605.2	108

Notes:

Permit Holders Only (PHO) 'C' 08:30-18:30 Monday to Friday, including "resident or business permit holders only", "resident permit holders only", "PHO 'C' and pay by phone max. stay 2 hours", and "PHO 'C' and pay by phone max. stay 4 hours".

All areas of kerb side parking have been counted. To calculate parking capacity each length of parking bay has been measured and converted into parking spaces by dividing the length by 5m and rounding down to the nearest whole number in accordance with the Richmond methodology.

Source: PMA Survey

2.9 The parking survey inventory in Table I shows that there is a total of 108 safe and legal Permit Holder Only (PHO) 'C' parking opportunities within the survey area.

Parking Survey Results

- 2.10 The next stage of the on-street parking assessment is to carry out a series of parking surveys. The Richmond methodology states that three total surveys between the hours of 01:00-05:30 must be undertaken on two separate weekday nights (i.e. Monday, Tuesday, Wednesday or Thursday) and one on Sunday night. Overnight parking surveys are designed to capture the peak resident demand for on-street parking in a given area.
- 2.11 The overnight surveys were undertaken on Wednesday 20th, Thursday 21st and Sunday 24th November 2024, at 03:30, 01:30 and 02:00 respectively.
- 2.12 The results of each overnight parking survey are presented in Appendix B and have been produced to the standards prescribed within the Richmond methodology.
- 2.13 Table 2 presents the average results from all overnight surveys for total parking opportunities within the study area.

Table 2. Average Overnight Parking Survey Results

Street Name	Total PHO 'C' kerb side parking spaces	Total no. cars parked in PHO 'C'	PHO 'C' parking stress (%)
Cole Park Road	19	7	37%
Grimwood Road	24	22	92%
Latham Road	22	22	98%
London Road	8	7	92%
March Road	18	16	89%
Whitton Road	17	15	82%
Total	108	88	81%

NB: Minor arithmetic errors are due to rounding in the spreadsheet

Source: PMA Survey

- 2.14 The total observed average overnight parking stress of PHO 'C' (including pay by phone parking bays also) parking within the survey area was 81%. Of the 108

permit holder parking opportunities within the study area, an average of 88 cars have been observed to be parked, leaving 20 available spaces.

- 2.15 The Richmond methodology prescribes a threshold of 85% stress level for when a parking survey area is deemed to suffer from undue parking stress. The average overnight parking stress of permit holder parking opportunities within the survey area is 81%, which is 4% lower than the prescribed threshold. With the addition of one resident parking permit, the parking stress only increases to 82%. The results of the parking surveys demonstrate that the uptake of kerb side parking in proximity to the application site is not at a level where parking stress is overly high or problematic.

3.0 IMPACT ON LOCAL PARKING AVAILABILITY

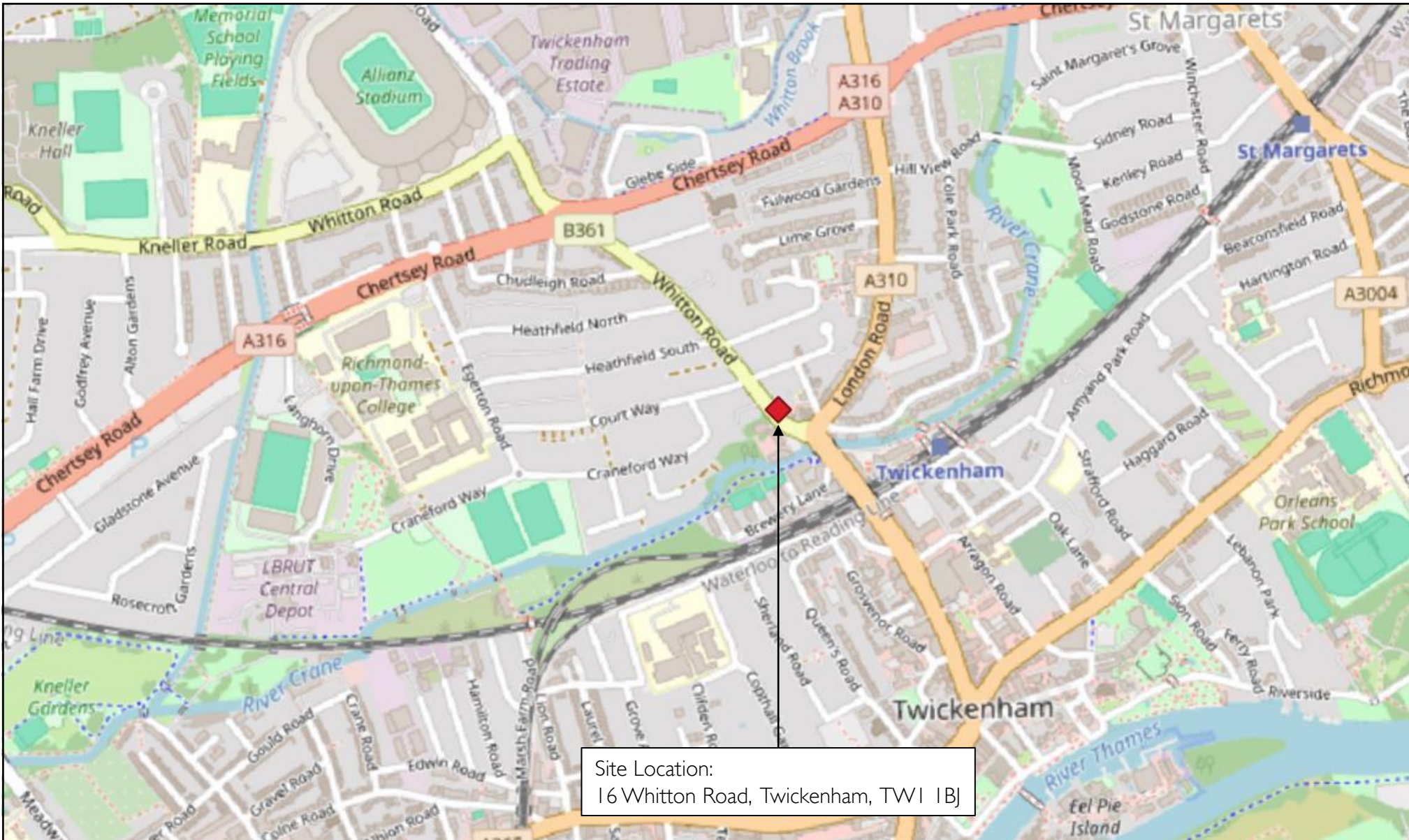
Application Impact

- 3.1 The total observed average overnight parking stress of PHO 'C' (including pay by phone parking bays also) parking within the survey area was 81%. Of the 108 permit holder parking opportunities within the study area, an average of 88 cars have been observed to be parked leaving 20 available spaces.
- 3.2 An additional one car parking within the PHO zone 'C' would increase the parking stress by 1%, from 81% to 82%, which is still below the prescribed 85% threshold that the London Borough of Richmond upon Thames has set for parking stress.
- 3.3 The impact of application is therefore anticipated to be acceptable and insignificant.

4.0 SUMMARY

- 4.1 This report has been prepared in relation to a planning application to the London Borough of Richmond upon Thames.
- 4.2 The proposal seeks to remove the S106 parking permit restriction currently associated with the dwelling.
- 4.3 A parking survey in line with the Richmond Methodology has been undertaken to assess the current on-street parking levels, and in order to determine the impact of the proposed development in relation to current highway capacity, highway safety, and neighbouring amenity.
- 4.4 The average overnight parking stress of permit holder parking opportunities for future occupants of the site is currently 81%. The results of the parking surveys demonstrate that the uptake of kerb side parking in proximity to the application site is not at a level where parking stress is problematic.
- 4.5 An additional one car parking within the local CPZ would increase parking stress by 1% from 81% to 82%.
- 4.6 The Richmond methodology prescribes a threshold of 85% stress level for when a parking survey area is deemed to suffer from undue parking stress. When applying the application impact, parking stress levels of permit holder parking will be 82%, which is 3% lower than the prescribed threshold.
- 4.7 The proposal will therefore have an insignificant impact on the adjoining highway in terms of parking capacity, road safety, and neighbouring amenity.
- 4.8 The development will have an insignificant effect on highway capacity, safety or neighbouring amenity.

FIGURES



Site Location:
16 Whitton Road, Twickenham, TW1 1BJ

Date: 28-November-2024
Scale: NTS
Source: OpenStreetMap
Drawing No: P3009/TS/1



P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ

Figure 1
Site Location



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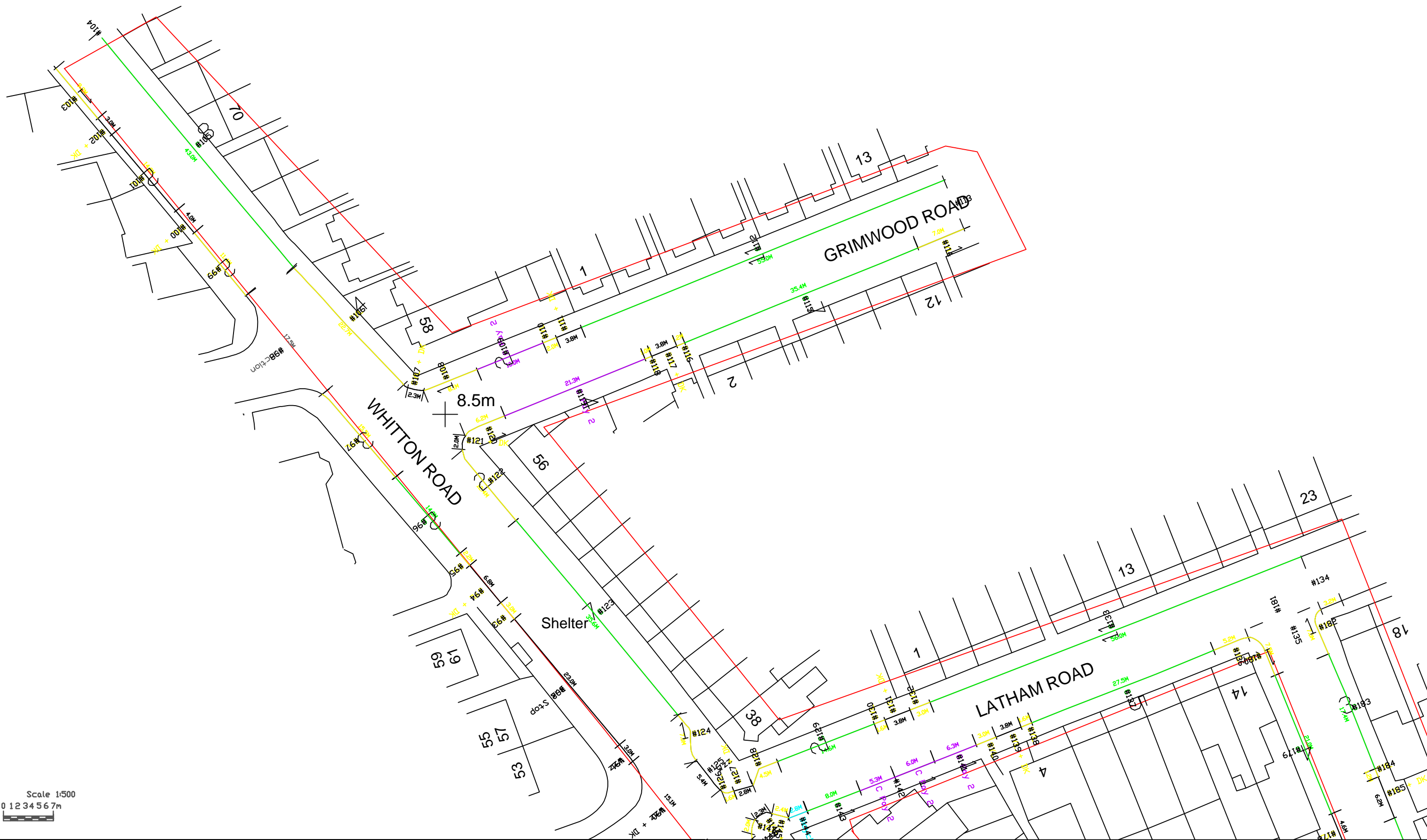
Site Location:
16 Whitton Road, Twickenham, TW1 1BJ

Date: 28-November-2024
Scale: NTS
Source: OpenStreetMap
Drawing No: P3009/TS/2

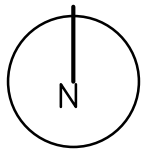


P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
Figure 2
Parking Survey Area

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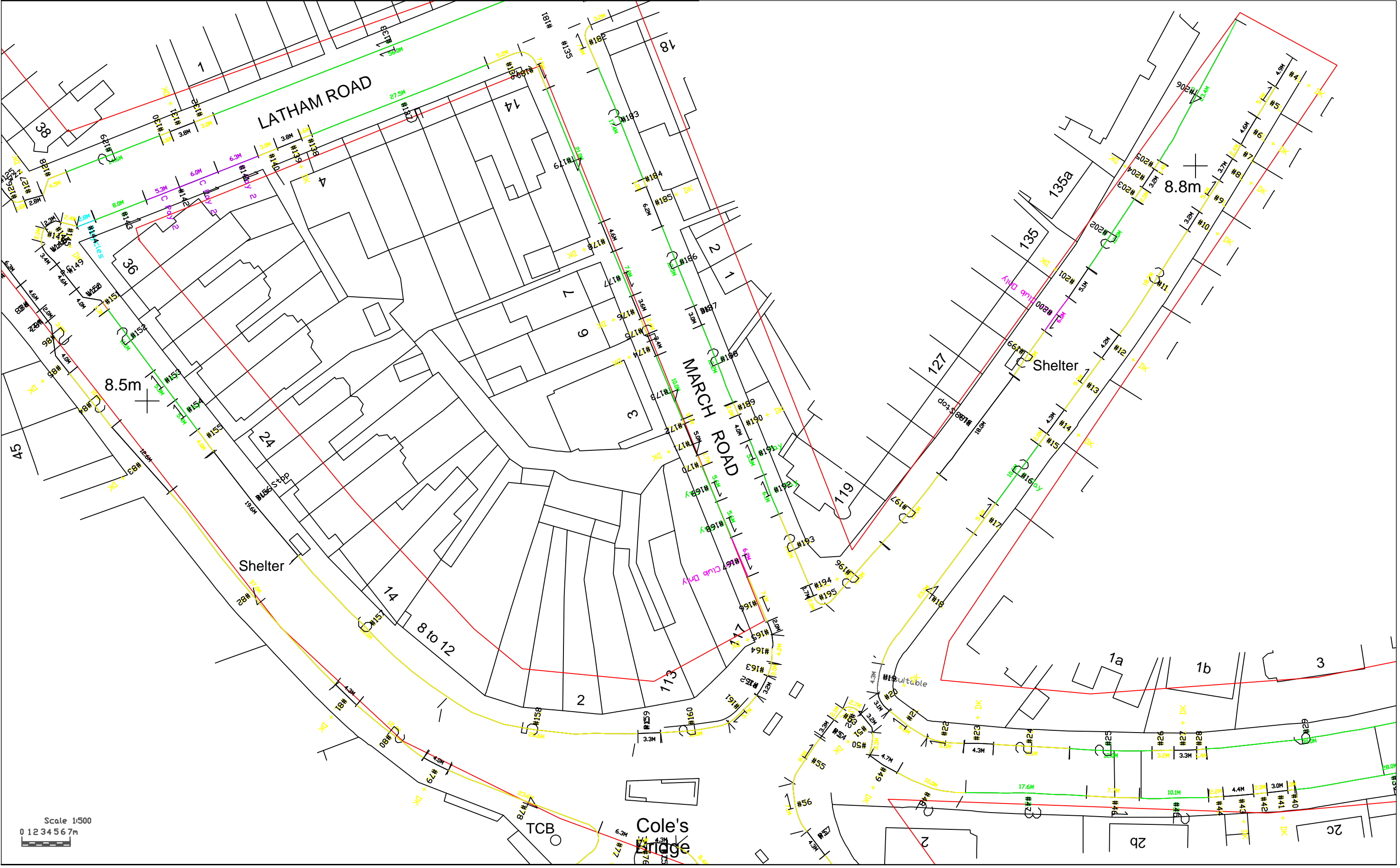


Date: 28-November-2024
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 Source: OS/PMA
 Drawing No. P3009/TS/3a

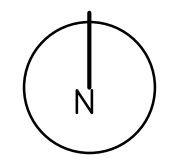


P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
 Figure 3a
 Parking Survey Inventory Map - North Extent


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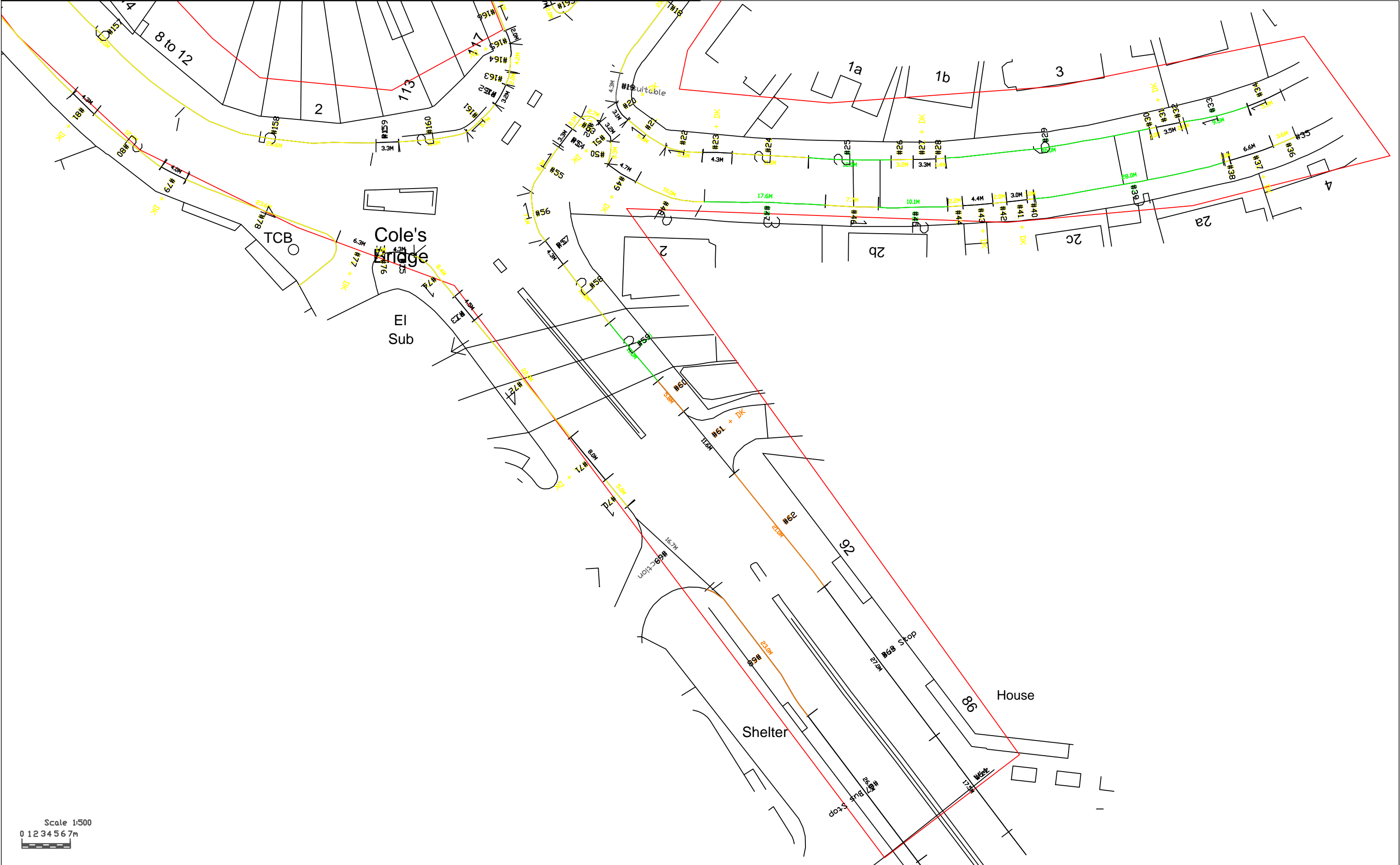


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 Source: OS/PMA
 Drawing No. P3009/TS/3b

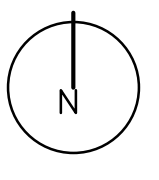


P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
 Figure 3b
 Parking Survey Inventory Map - Central Area


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Date: 28-November-2024
 Scale: 1:500@A3
 Source: OS/PMA
 Drawing No. P3009/TS/3c



P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
 Figure 3c
 Parking Survey Inventory Map - South Extent


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APPENDIX A

Richmond Parking Survey Methodology

Richmond parking survey methodology

The Council has set maximum parking standards for developments in Their Local Plan and these are expected to be met, unless it can be shown that there will not be an adverse effect on on-street parking. Where there is a shortfall of parking on site, a parking survey of the surrounding streets will be required. The Council will use an independent survey company; however applicants may provide their own surveys as long as they follow the methodology outlined below.

Extent of survey area

The area to be surveyed must cover a 200m/2 minute walking distance around the site. This area can be extended/amended in the following ways:

- 1 If the survey reaches the middle of a street at 200m, the survey area could be extended to the next junction or curtailed to the previous junction with agreement of Transport Planning officers
- 2 If there are areas within 200m where parking is restricted due to on street restrictions or undesirable (for which justification must be given) the area is to be curtailed
- 3 Areas outside of Richmond will be excluded
- 4 Roads in CPZ's adjacent to the site, for which the site would not be able to access parking permits, may be excluded depending on CPZ start time and these roads are to be agreed with Transport Planning officers prior to the survey being undertaken

The Council may require amending of surveys which reveal anomalies or require further investigation once scrutinised.

Survey times

Surveys must only be undertaken during term time and not within public/school holidays/half term or the week before/after to take into account independent school holidays. It is best to contact the Council to confirm acceptable survey dates and dates which coincide with an event in the area, which must also be avoided as these could impact on the results.

For residential surveys 2 x weekday surveys (Monday to Thursday) and one weekend survey on a Sunday between 01h00 and 05h30 are required. This will capture the residential peak parking time.

Commercial and other land use applications will require surveys at other times which are to be agreed with the Council in advance of the survey being undertaken. Similarly, times may be amended for residential surveys where the site is within close proximity to commercial uses or a town centre in which case morning and early evening surveys may also be requested. More detailed surveys may be required if the operational times clash with nearby restaurants, in which case 15 minute interval surveys between 18h00 and 22h00 will also be required. In order to assess commuter parking morning and evening

peak hour surveys will be required for sites within close proximity to railway stations. These should be undertaken between 06h30 – 08h00 and 17h30 – 19h00.

Required information

Surveys must be provided in map form, examples are included at the end of this appendix.

One map shows the inventory for the area and notes all individual bay lengths and types.

Another shows x's as parked cars and s's as empty spaces exactly where they are parked on the night. This will give us a snapshot of exactly how cars are parked in that area, rather than a calculated assumption, which is often incorrect. S's can only be shown where each space represents 5.0m.

Noted on the survey maps should be the date and time the survey was undertaken as well as whether the area is within a Controlled Parking Zone (CPZ) or not. All parking restrictions on street must be noted Double/Single Yellow Lines (D/SYL's), bus lay-by's, zig-zags, kerb build outs, legal footway parking, dropped kerbs, disabled/doctors/loading bays, suspensions/temporary restrictions, skips and road works, narrow roads, where parking is not possible or subject to flooding etc. Disabled bays can be shown but will not be included in the general inventory as they require a permit to park therein. If there are marked bays on street these must be shown and dimensioned on the map. The space between crossovers should also be dimensioned although areas of less than 5.0m should not be included in the calculations.

The first 7.5m of a junction is to be omitted, but cars parked within will be considered in the calculations as contributing to on street stress. Illegally parked cars must be shown on the plan and these will be included in the stress calculation.

Surveys undertaken within CPZ's during CPZ hours will need to clearly define various types of bays (Resident permit holders/shared use bays/Business Bays etc).

Where restrictions start early in the morning we may not consider these areas for overnight parking if the surveys show that residents do not park there as they will have to move their cars before the restriction commences. This includes single yellow lines.

The above information can be tabulated, but this table must reflect the information on the inventory map in terms of the available bay numbers i.e. individual lengths of bays divided by 5.0m.

The stress figures must be taken from the results maps and illegally parked cars should be counted. If spaces are noted and tabulated these must only be included if each space represents at least 5.0m. Tabulated results should be by road and include a 'Total' column.

Results

In order to assess the parking stress the tabulation must calculate the number of parked cars shown on the results map of each survey, against total available space calculated from the inventory survey and add the shortfall anticipated from the development using the Council's parking standard. In addition committed development parking overspill should also be included.

If stress levels are calculated at 85% stress* or more LBRuT will raise an objection on the grounds of saturated parking, highway safety and undue harm to neighbour amenity.



Example of survey inventory sheet and results maps

Road Name	No Bays	17/6/14 @ 5am	19/7/14 @ 5am	Ave		
	43	37	45		41	
	16	20	21		20.5	
	28	28	28		28	
	34	29	26		27.5	
	22	19	19		19	
	21	13	15		14	
	11	14	11		12.5	
	16	19	19		19	
TOTAL	191	179	184	181.5	All % stress	95.02617801
plus anticipated shortfall of proposal plus x cars from approved applications yet to be implemented within the survey area	191	192	197	194.5	plus x cars stress%	101.8324607
	191	195	200	197.5	plus another x cars stress%	103.4031414

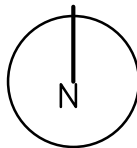
APPENDIX B

Parking Survey Results



Scale 1:1250
0 5 10 15 20m

Date: 28-November-2024
Scale: 1:1250@A3
Source: OS/PMA
Drawing No. P3009/TS/Bi



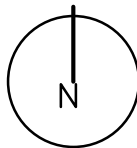
P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
Appendix Bi
Parking Survey Results - Survey 1 20/11/2024 03:30


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Scale 1:1250
0 5 10 15 20m

Date: 28-November-2024
Scale: 1:1250@A3
Source: OS/PMA
Drawing No. P3009/TS/Bii



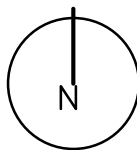
P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
Appendix Bii
Parking Survey Results - Survey 2 21/11/2024 01:30


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Scale 1:1250
 0 5 10 15 20m

Date: 28-November-2024
 Scale: 1:1250@A3
 Source: OS/PMA
 Drawing No. P3009/TS/Biii



P3009: 16 WHITTON ROAD, TWICKENHAM, TW1 1BJ
 Appendix Biii
 Parking Survey Results - Survey 3 24/11/2024 02:00


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P3009 I6 Whitton Road

Appendix B. Parking Survey Results

Sum of 20/11/2024 03:30	Kerb Type									
Road Name	Car Club Only	Pay At Machine (Mon-Fri 8.30am- 6.30pm Pay By Phone Max Stay 2 Hours)	PHO C (Resident or Business Permit Holders Only)	PHO C (Resident Permit Holders Only At all times)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 2 Hours)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 4 Hours)	Single Yellow Line (Mon-Fri 8.30am-6.30pm)	Single Yellow Line (Mon- Sat 8.30am- 6.30pm)	Taxi (7am- 7pm)	Grand Total
Cole Park Road			2	4			0	0		6
Grimwood Road				16		6	1			23
Latham Road				20		1	0			21
London Road	0			6	1		0	0	0	7
March Road	1			12	4		0			17
Whitton Road		1		14			0	0		15
Grand Total	1	1	2	72	5	7	1	0	0	89
Sum of 21/11/2024 01:30	Kerb Type									
Road Name	Car Club Only	Pay At Machine (Mon-Fri 8.30am- 6.30pm Pay By Phone Max Stay 2 Hours)	PHO C (Resident or Business Permit Holders Only)	PHO C (Resident Permit Holders Only At all times)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 2 Hours)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 4 Hours)	Single Yellow Line (Mon-Fri 8.30am-6.30pm)	Single Yellow Line (Mon- Sat 8.30am- 6.30pm)	Taxi (7am- 7pm)	Grand Total
Cole Park Road			2	5			0	0		7
Grimwood Road				16		6	1			23
Latham Road				20		2	0			22
London Road	0			6	2		0	0	0	8
March Road	1			12	3		0			16
Whitton Road		2		13			1	0		16
Grand Total	1	2	2	72	5	8	2	0	0	92
Sum of 24/11/2024 02:00	Kerb Type									
Road Name	Car Club Only	Pay At Machine (Mon-Fri 8.30am- 6.30pm Pay By Phone Max Stay 2 Hours)	PHO C (Resident or Business Permit Holders Only)	PHO C (Resident Permit Holders Only At all times)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 2 Hours)	PHO C and Pay A Machine (Mon-Fri 8.30am-6.30pm Pay By Phone Max Stay 4 Hours)	Single Yellow Line (Mon-Fri 8.30am-6.30pm)	Single Yellow Line (Mon- Sat 8.30am- 6.30pm)	Taxi (7am- 7pm)	Grand Total
Cole Park Road			3	5			0	0		8
Grimwood Road				15		6	0			21
Latham Road				20		2	0			22
London Road	0			6	1		0	0	0	7
March Road	1			13	4		0			18
Whitton Road		1		14			0	0		15
Grand Total	1	1	3	73	5	8	0	0	0	91