



Planning and Heritage Statement

Erection of a carport and associated landscaping at

**Land Opposite Wigsell House, Manor Road,
Teddington, TW11 8AB**



PREPARED BY

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DECEMBER 2024



1.0 INTRODUCTION

- 1.1 This statement has been prepared in support of a full planning application for the erection of a car port on land opposite Wigsell House, Manor Road in Teddington which is being developed for the erection of 1no. dwellinghouse. This follows a positive pre-application submission for a similar scheme.
- 1.2 The statement describes the site and proposal and sets out how the proposal complies with the policies and guidance as set out in the National Planning Policy Framework, the London Plan and the Development Plan for London Borough of Richmond upon Thames. We therefore suggest that planning permission should be granted.

2.0 SITE AND SURROUNDINGS

- 2.1 The site comprises a parcel of land situated opposite Wigsell House, Manor Road, off the access way which leads along the rear of Twickenham Road properties. The location of the car port is an under-used piece of land which includes a hedge which collects rubbish which is blown there by the wind.

- The site is not located within a conservation area, although there are several surrounding Buildings of Townscape Merit (BTM). The Teddington Lock Conservation Area is adjacent.
- The site is in flood zone 1 (low probability of flooding) but is within a site with "Potential for groundwater flooding of property situated below ground level"
- Terrace of four storey houses nearby (37-49 Twickenham Road) are designated BTMs.
- Large detached dwellings to south and west Nos. 28 and 30 Manor Road and 25-45 Manor Road are designated BTMs.
- The site has a Public Transport Accessibility Level (PTAL) of 2 (Poor) and on the border of PTAL 1 (very poor).
- The area is residential in nature in walking distance to the river.

3.0 RELEVANT PLANNING HISTORY

- 3.1 In 2018, planning permission was granted under reference 17/0788/FUL for “Demolition of lock up garages to provide 1 no. detached 4 bedroom dwellinghouse with associated parking, cycle and refuse stores, new boundary fence and hard and soft landscaping”.
- 3.2 This was implemented and then on 5th June 2024, permission was granted for a “Variation of condition U34062 - Approved Drawings - of planning permission ref: 17/0788/FUL dated 08.01.2018 and condition BD12 (Details - Materials) approved under ref. 17/0788/DD01 to allow minor material amendments that include changing the exterior finish to top floor to brick from render, replace the metal eaves supports to plaster ones to match neighbours, change position of GF staircase, new timber sash windows on the GF bay on front elevation, all other windows to change to timber sash windows from timber casement windows, rear rooflight omitted, replace the three timber bifold doors to rear garden with alitherm heritage bifold windows painted Dark Grey RAL 9017” under reference 23/2839/VRC.

Pre-application advice

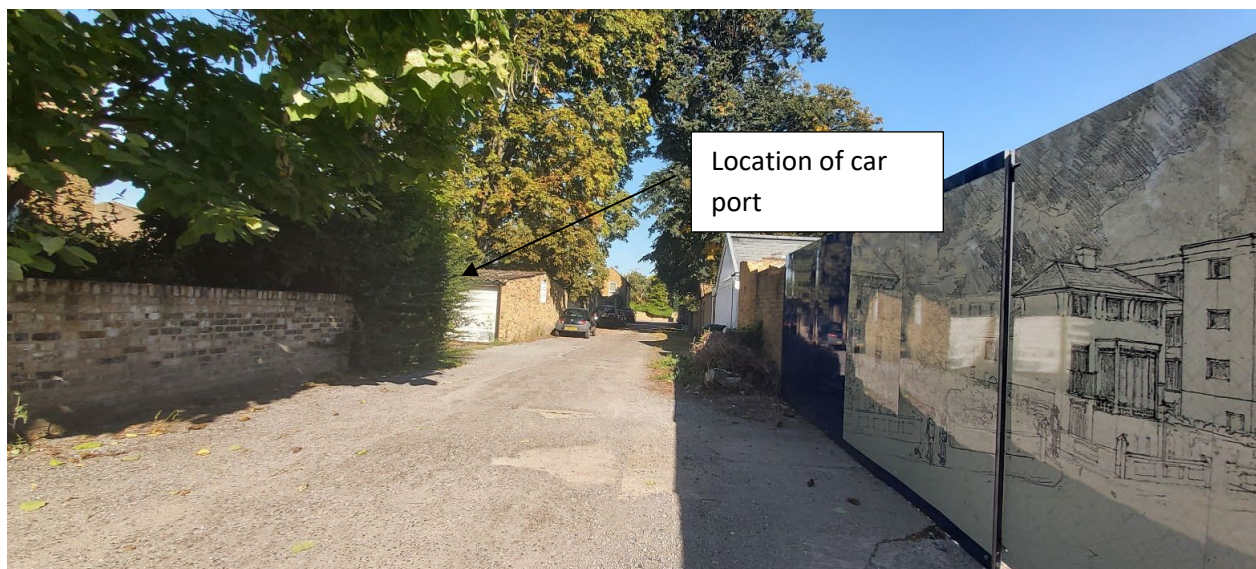
- 3.3 On 2nd August 2024, a pre-application response was received by the LPA for this proposal. The advice was as follows:

The carport would be next to the existing BTM at 28 Manor Road, sited in the Conservation Area CA37 High Street Teddington, however it would appear to be well screened by existing vegetation in the garden of that property. Furthermore, this would present an open and have a single storey nature resulting in the carport to be subordinate to the surrounding BTMs and residential properties. It is also noted that the private road, where the carport would be located, hosts garages and therefore such carport is considered to be in keeping with the character of this private road. As such, the proposals are considered to have a neutral impact over the settings of the Conservation Areas and BTMs.

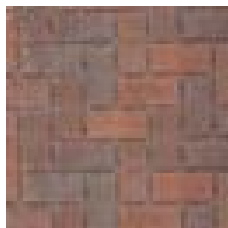
This would be a single storey and open structure that does not appear to cause unacceptable amenity issues in terms of loss of light, sense of enclosure and overlooking.

4.0 PROPOSAL

- 4.1 The proposal would be for a carport to be utilised by the owners of the new house which is to be known as Wigsell House, Manor Road under reference number 17/0788/FUL and amended by ref. 23/2839/VRC. Following permission for this car port an application will be submitted to amend the proposed dwelling known as Wigsell House, Manor Road to remove the parking space and use that space as habitable space.



- 4.2 The car port would be 2.1m high at its lowest height rising up to 2.69m. The structure would be made from timber with 5 posts acting as the sides and timber slatted panels. It would measure 3.4m wide by 4.9m long. It would be located behind the boundary garden of no. 28 Manor Road on land which is underused off the access road which leads to garages and parking spaces behind Twickenham Road properties.
- 4.3 The car would be parked upon a permeable paved area under the shelter. It would be paved with permeable paving - Marshalls Driveline Piora Brindle Block Paving Pack 200mm x 100mm x 60mm. The specifications are submitted with the application.



5.0 PLANNING POLICY

5.1 The development plan comprises the Richmond upon Thames Local Development Local Plan (July 2018) and the London Plan (2021). The National Planning Policy Framework (NPPF) from December 2024 and the Hampton Wick and Teddington Village Planning Guidance (2017) are also material considerations.

National Planning Policy Framework 2024 (NPPF)

5.2 The NPPF requires, at section 12, high quality, well designed places which is key aspect to sustainable development. Paragraph 131 states that development should be visually attractive as a result of good architecture and layout and are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change.

5.3 Paragraph 203 states that local planning authorities should take account of:

- d) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- e) the wider social, cultural, economic and environmental benefits that conservation of the historic environment can bring;
- f) the desirability of new development making a positive contribution to local character and distinctiveness; and
- g) opportunities to draw on the contribution made by the historic environment to the character of a place.

- 5.4 Paragraph 215 states that “Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.”
- 5.5 Paragraph 216 states that “The effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset”.

London Plan

- 5.6 The main London Plan policies applying to the site are:
- Policy D4 Delivering good design
 - Policy HC1 - Heritage Conservation and Growth
 - Policy D12 Fire Safety
 - Policy T6.1 Residential parking

Richmond upon Thames Local Plan

- 5.7 The policies which are relevant to this proposal are:
- i. LP1 Local Character and Design Quality
 - ii. LP 3 Designated Heritage Asset
 - iii. LP4 Non-designated heritage assets
 - iv. LP 8 Amenity and Living Conditions
 - v. LP 15 Biodiversity
 - vi. LP 16 Trees, Woodlands and Landscape
 - vii. LP 21 Flood Risk and Sustainable Drainage
 - viii. LP 45 Parking Standards and Servicing

6.0 PLANNING CONSIDERATIONS

6.1 The main issues for consideration are as follows:

- 1) The impact on the character and appearance of the area, including the neighbouring conservation area.
- 2) The impact on neighbour amenity
- 3) The impact on trees
- 4) Parking
- 5) Flooding and drainage

Design and heritage issues

6.2 In terms of such a heritage asset there is a duty imposed by Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 requiring decision makers to have special regard to the desirability of preserving or enhancing the character or appearance of a Conservation Area.

6.3 The application site lies in a slither of land between the boundary of the Teddington Lock Conservation Area and High Street, Teddington Conservation Area. As such, the application site is considered to lie within the setting of the conservation areas. The site is also adjacent to a locally listed building (no.28 Manor Road) and thus the impact on its setting is considered below. The proposal would be made from natural materials and is a modest structure seen in the context of garages along the access road.

6.4 The pre-application response stated:

The carport would be next to the existing BTM at 28 Manor Road, sited in the Conservation Area CA37 High Street Teddington, however it would appear to be well screened by existing vegetation in the garden of that property. Furthermore, this would present an open and have a single storey nature resulting in the carport to be subordinate to the surrounding BTMs and residential properties. It is also noted that the private road, where the carport would be located, hosts garages and therefore such carport is considered to be in keeping with the character of this private road.

6.5 By virtue of the development's design and scale, the character and appearance of the Teddington Lock Conservation Area and High Street Teddington Conservation Area, within which the proposal car port would be adjacent to, would be overall preserved. As such, the proposal complies with Policy LP1, LP3 and LP4 of the Local Plan (July 2018) and the historic environment policies of the NPPF and Policy 28 and 29 of the draft Local Plan (June 2023).

Impact on Neighbour Amenity

6.6 Policy LP8 of the Local Plan advises that all development is required to protect the amenity and living conditions for occupants of new, existing, adjoining and neighbouring properties.

6.7 The pre-application response stated:

From an Amenity perspective...the carport, would be a single storey and open structure that does not appear to cause unacceptable amenity issues in terms of loss of light, sense of enclosure and overlooking.

6.8 The car port is a lightweight and modest sized structure which would not cause any harm to visual amenity. The parking of a vehicle in this location would be wholly in keeping with this residential area and with other garages and parking bays along this access road.

6.9 As set out above, the proposed development would not lead to any visual intrusion, overlooking, loss of privacy or daylight and sunlight implications when it comes to impact on the nearest neighbouring occupiers, in compliance with Policy LP8.

Impact on Trees and biodiversity

6.10 The proposal does not require the removal of any trees, however, the site is in the Root Protection Area of an Indian Bean tree. The application is supported with an Arboricultural

Impact Assessment which concludes: the principal arboricultural features within the site can be retained and adequately protected during development activities. Subject to precautionary measures including retaining the hard surface that exists, installing the sections of the car port within the RPA of T1 using hand tools only, mixing cement/concrete outside of the RPA, the proposal would not be injurious to trees to be retained complying with policy LP16 of the Local Plan.

- 6.11 In terms of Biodiversity Net Gain, as this proposal is less than 25sqm, it is exempt from needing to provide 10% Biodiversity Net Gain.

Transport and Parking

- 6.12 Policy LP45 of the Local Plan states that the Council will require new development to make provision for the accommodation of vehicles in order to provide for the needs of the development while minimising the impact of car-based travel including on the operation of the road network and local environment and ensuring making the best use of land.

- 6.13 The pre-application response stated:

The site for the carport is opposite the main application site, the site of the approved dwelling, and the applicant should clarify they have permission from the other users/owners of the land to construct such an arrangement. Also, details of paving and the drainage run off should be provided to ensure it doesn't impact neighbouring properties.

- 6.14 The owners of the land, The Blakeney House Residents Association Limited, have been notified of the application and are aware of the proposal. details of the paving and drainage are included within this application and also within the supporting Surface Water Drainage Strategy Addendum by aegaea.

- 6.15 The proposal is considered to policy compliant and would not cause any adverse impacts upon the highway network.

Flooding and sustainable urban drainage

- 6.16 The site is located within Flood Zone 1. Accordingly, in accordance with the NPPF neither the sequential or exception test need to be addressed, and the proposal is compliant with Policy LP21 of the Local Plan. However, the site is in an Area Susceptible to Groundwater Flood and therefore a statement from aegaea has been provided which addresses the groundwater and surface flooding.
- 6.17 Policy LP 21 Flood Risk and Sustainable Drainage states that all developments should avoid, or minimise, contributing to all sources of flooding, including fluvial, tidal, surface water, groundwater and flooding from sewers, taking account of climate change and without increasing flood risk elsewhere.
- 6.18 In terms of Sustainable Drainage, paragraphs 6.2.21 and 6.2.22 states that:
"The borough is very susceptible to surface water flooding. Surface water flooding happens when the ground and rivers cannot absorb heavy rainfall and when man-made drainage systems have insufficient capacity to deal with the volume of rainfall. Typically this type of flooding is localised and happens very quickly, making it very difficult to predict and give warnings. With climate change predicting more frequent short-duration, high intensity rainfall and more frequent periods of long-duration rainfall, coupled with an ageing Victorian sewer system and increasing pressure from growing populations, surface water flooding is likely to be an increasing problem.
Therefore, to reduce the risk of surface water and sewer flooding, all development proposals in the borough that could lead to changes to, and have impacts on, surface water run-off are required to follow the London Plan drainage hierarchy: use infiltration techniques, such as porous surfaces in non-clay areas".

- 6.19 The proposed permeable paving under the car port of site will ensure it complies with the flooding policy to provide porous materials to ensure water can permeate through the ground to prevent flooding. The permeable block paving provides the solution in creating a suitable parking area where there are no environmental consequences to the environment in times of heavy rainfall, compliant with policy LP21.

Fire Safety

- 6.20 London Plan policy D12 requires the submission of a Fire Safety Statement on all planning applications. A Fire Safety Statement is submitted with this application. The scheme is considered to be consistent with this Policy D12 of the London Plan.

7.0 OVERALL CONCLUSION

- 7.1 The National Planning Policy Framework (NPPF) states that the creation of high quality and beautiful buildings and places is fundamental to what the planning and development process should achieve.
- 7.2 The significance of the heritage assets of the Teddington Lock Conservation Area and High Street, Teddington Conservation Area is in the appearance of the buildings which front the main roads. In this instance, due to the small scale structure set back behind Manor Road the proposed development is considered to preserve the character and appearance of the adjacent conservation areas. There would also be no harmful impacts on the neighbouring property, no.28 Manor Road.
- 7.3 The proposal would include permeable paving to ensure there is no harm to flood risk. The impact on neighbouring trees can be addressed via conditions added to a permission to ensure the tree roots are protected and there would be no fire safety risks.
- 7.4 Overall, the proposal is considered to comply with the NPPF and policies LP1, LP3, LP4, LP 8, LP 15, LP 16, LP 21 and LP 45 of the Richmond upon Thames Local Plan 2018, and to the NPPF.

7.5 Overall, the scheme is considered to be fully policy compliant, and we respectfully request that permission is granted.